

# **CITY OF PORTAGE PLANNING COMMISSION**

## **AGENDA**

**Thursday, May 16, 2024  
7:00 PM**

**Portage City Hall Council Chambers**

### **CALL TO ORDER**

### **PLEDGE OF ALLEGIANCE**

### **ROLL CALL**

### **APPROVAL OF MINUTES:**

1. Minutes dated May 2, 2024

### **PUBLIC HEARINGS**

- \* 1. A Special Land Use Permit and Parking Reduction Request to operate a Group Child Care Home at 3609 W. Milham Avenue.
- \* 2. Special Land Use Permit and Site Plan to operate a Contractor's Business at 9651 Portage Road.
- \* 3. Lakefront Property Housing Code Ordinance Amendment #23/24-5: Ordinance to amend City of Portage Chapter 24 Article 5 Safety, Sanitation, and Health; and Chapter 42 Land Development Regulations relating to the Lakeshore Housing Project.

### **SITE/FINAL PLANS:**

- \* 1. A Final PD Site Plan for 'Oakland Commons' a proposed 58-units residential development at 9581 Oakland Drive.

### **OLD BUSINESS:**

- \* 1. Draft Portage Forward Together 2045 Master Plan

### **NEW BUSINESS:**

### **STATEMENT OF COMMISSIONERS AND CITIZENS:**

### **ADJOURNMENT:**

Star (\*) indicates printed material within the agenda packet.

# CITY OF PORTAGE PLANNING COMMISSION

Thursday, May 2, 2024

7:00 PM Portage

Portage City Hall Council Chamber

The City of Portage Planning Commission meeting of May 2, 2024, was called to order by Chair Corradini at 7:00 p.m.

## **IN ATTENDANCE**

- Peter Dame, City Chief Development Officer
- Eric Feldt, Senior City Planner
- Alex Johnson, City Planner/Project Manager
- Catherine Kaufman, City Attorney
- Pat McGinnis, City Manager
- Biqi Zhao, Deputy Director of Planning and Zoning

## **ROLL CALL**

Mr. Feldt called the roll: Chairman Corradini (yes); Vice Chair Baldwin (yes); Secretary Freiman (yes), Adams (yes); Pezzoli (yes); Joshi (no); Fries (yes); Youngs (yes); and Longjohn (no).  
7-Present; 2-Absent.

Motion by Vice Chair Baldwin, seconded by Commissioner Fries to approve the excused commissioners.  
Motion carried 7-0.

## **APPROVAL OF MINUTES**

1. Minutes dated April 18, 2024.

Motion by Commissioner Youngs, seconded by Vice Chair Baldwin to approve the April 18, 2024, amended meeting minutes, as submitted. Motion carried 7-0.

## **STATEMENT OF CITIZENS**

1. Vanessa Messenger; 9220 East Shore Drive  
She presented a petition asking the commission to maintain single-family zoning in various parts of Portage due to concerns about traffic, environment, neighborhood character, and public safety. Further, she believes that rezoning those areas negatively impacts property values, increases overdevelopment, and adversely affects residents who purchase property under existing zoning.
2. David Savoy; 3520 East Shore Drive  
He read a letter on behalf of Michael Cartier, resides at 1127 Lakeview Drive, in regards to the proposed changes of the Public Participation Plan stating support of requiring developers to hold neighborhood meetings and opposing the proposed changing to 'recommended' a neighborhood meeting.
3. Susan Sandmeyer; 3520 East Shore Drive  
She asked the Commission to think as Portage citizens, not the commission, and how changing the language could negatively impact themselves in the future in regards to the proposed changes of the Public Participation Plan.

## **PUBLIC HEARINGS**

None.

### **SITE/FINAL PLANS**

None.

### **OLD BUSINESS**

#### **Proposed Revisions to Portage Public Participation Plan**

City Manager, Pat McGinnis, presented proposed changes to the Public Participation Plan based on feedback from the Planning Commission during the April 4, 2024 Commission meeting. Changes included: reorder stakeholder list to remove any biasness, ensure Planning Commissioners receive notification a new project, and replace requiring with recommending future developers to meet with adjacent neighborhoods. His presentation concluded with seeking Planning Commission's motion of recommendation on the proposed changes for a future City Council vote.

#### **Discussion**

Secretary Freiman found the proposed changes may be too ambiguous for the Chief Development Officer when deciding if a neighborhood meeting should be required.

Commissioner Youngs supports the changes as it protects developers building on their own land and agrees with the Chief Development Officer making the of whether or not to require a neighborhood meeting.

Commissioner Pezzoli finds the term 'others' too vague and suggests imposing a maximum buffer radius for neighborhood meeting notification.

City Manager McGinnis stated support to include other entities such as utility companies.

Commissioner Fries asked about the evaluation process for determining neighborhood meeting success, and suggests city staff should meet with the developer to decide if a meeting is necessary.

City Manager McGinnis stated it will be evaluated periodically, the City will encourage developers to hold neighborhood meetings, and not support City staff attend the meetings. He indicated that staff can facilitate future meetings.

Vice Chair Baldwin supports reinstating the 300' requirement for neighborhood meeting adjacent property invite area.

Commissioner Adams supports requiring a meeting at the discretion of the Chief Development Officer since the new master plan lessen projects requiring Planning Commission approvals, and including adding that the City will help facilitate those meetings.

Secretary Freiman supports requiring the development to hold a neighborhood meeting.

Commissioner Fries advocates supports the changes and defining the city's role in facilitating the meetings.

Chair Corradini indicated that neighborhood meetings occurring before project plan submittal can negatively impact both developers and citizens, as they may not provide enough details and may not be worth the time. He suggested the City should facilitate meetings, but not attend, and provide a summary

of the issues discussed, including the developer's response to the comments. He also recommended a few citizens in certain neighborhood meetings be the collective voice.

City Manager McGinnis emphasized the informal nature of the meetings, and that the intention is to inform the developer of citizen concerns and inform the citizens about new developments.

City Attorney Kaufman informed the Commission about concerns about due process from citizens if a distance is not specified for notification range.

Commissioner Fries motioned, supported by Commissioner Youngs, to recommend the proposed Public Participation language for change #3, with the insertion of a 300' distance as to who would need be notified, for approval to the city council. Vice Chair Baldwin (yes); Secretary Freiman (no); Pezzoli (no); Fries (yes); Youngs (yes); Adams (no) Chairman Corradini (yes). Prior to vote of motion, Secretary Freiman asked for a friendly amendment to require a neighborhood meeting; Commissioner Fries opposed the amendment. Motion passes 4-3.

Commissioner Fries motioned, supported by Commissioner Frieman, to recommend approval to city council for the proposed Public Participation changes #1 and #2; alphabetizing the list of stakeholders and having the Commission be notified the same time as the public. Vice Chair Baldwin (yes); Secretary Freiman (yes); Pezzoli (yes); Fries (yes); Youngs (yes); Adams (yes) Chairman Corradini (yes): Motion passes 7-0.

## **NEW BUSINESS**

### Draft Portage Forward Together 2045 Master Plan

Mr. Feldt updated the Commission of the draft Master Plan process, upcoming steps, stated the new online format for the draft Plan, and introduction Houseal Lavigne staff to present the new draft Master Plan. He concluded his presentation with staff's recommendation to the Commission to provide a motion of support to Council for the draft Maste Plan public distribution.

Jack Carso and Sujana Das Shrestha, Houseal Lavigne associates, presented the draft Master Plan website format, a brief history of the process so far, a list of goals for the plan, and a summary of each chapter.

Several Commissioners stated their support of the new draft Master Plan and online format.

Commissioner Adams did not have the ability to access the entire draft master plan online.

Mr. Feldt informed the Commission that the technical problems with viewing the draft master plan will be fixed.

Several Commissioners support hard copies of the draft master plan for review for those without access to internet, maybe a library check out system to cut down on printing costs.

Mr. Feldt stated that the primary purpose of this agenda item is not for questions and answers, but to showcase the new format, outline, summary of the chapters, and to recommend to City Council in order to initiate the 63-day public review period, so feedback from the public can be received.

Commissioner Adams motioned, supported by Vice Chair Baldwin, to table the recommendation to Council for the draft Master Plan public distribution to the start of the draft master plan 63-day

notification period until next meeting. Vice Chair Baldwin (yes); Secretary Freiman (yes); Pezzoli (yes); Fries (yes); Youngs (yes); Adams (yes) Chairman Corradini (yes): Motion passes 7-0.

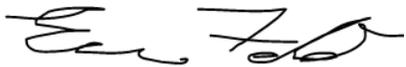
**STATEMENT OF COMMISSIONERS/ STAFF**

None.

**ADJOURNMENT**

There being no further business to come before the Commission, Chair Corradini adjourned the meeting at 9:05 p.m.

Respectfully submitted,



Eric Feldt  
Senior City Planner

DRAFT

**TO:** Planning Commission

**DATE:** May 16, 2024

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** A Special Land Use Permit and Parking Reduction Request to operate a Group Child Care Home at 3609 W. Milham Avenue.

**I. INTRODUCTION:**

The applicant and property owner, Madison Schmidt, has submitted a Special Land Use Permit and Parking Reduction Request to operate a Group Child Care Home at 3609 W. Milham Avenue located in the northwestern portion of Portage approximately 1/3 mile east of US-131 overpass. The applicant is currently operating a State-licensed child home day care at the subject site for up to six children. By seeking approval of a Group Child Care Home pursuant to Sec 42-182(I) and gaining an additional State license, the applicant would care for up to 12 children.

Concurrent with the filed Special Land Use Permit, the applicant is seeking approval for a Parking Reduction Request pursuant to Sec 42-520(O) to reduce the parking requirement by one space as a result of cost-prohibitive driveway expansions and unique parking demand from a home-based day care operation. This parking reduction request is also addressed in the subject report.

The project consists of using a single-family home for a home-based day care operation for up to 12 children. As noted in the attached project description, the existing 'Madison's Childcare' child day care operation allows morning drop-off hours between 6am and 8:30am and afternoon pick-up hours between 4pm and 5:30pm. Parents/Guardians will use the existing driveway. Day care activities will continue to occur inside the home and in the fenced backyard (see attached floor plans and site image). The applicant is currently pursuing a State license for the proposed Group Child Care Home, which first requires local (City of Portage) approval.

Portage offers a variety of child day care operations, including home day care operations such as Melissa Loghry Daycare & Preschool, commercial day care centers such as Little Tykes Learning Center, religious-based centers such as Christ for Kids Day Care at St. Michael Lutheran Church), and public school-based centers such as Curious Kids Childcare/ Preschool at Portage Community Education Center. The subject proposal is similar in nature and operation to other home day care operations where parts of the house and yard are dedicated to the functions of the child care operation.

The State licenses, regulates, and inspects day care operations in homes and commercial facilities. Also, the City of Portage regulates child day care homes and centers through zoning and permitting standards, except for home day cares for up to six children, which is generally exempt by the State.

Despite the variety of child day care options in Portage, there is child day care shortages and affordability issues across the State and nation which have become a significant concern for many families<sup>1,2</sup>. This is true in the greater Kalamazoo region as well<sup>3</sup>. Many operations closed or struggled to

stay open during the COVID period, with some closing afterwards and are strained by staffing issues<sup>4</sup>. The State has created a child care program aiming to increase child care affordability. The proposal to increase child care for up to 12 children helps to lessen local child care shortage issues.

Per the City’s Public Participation Plan, the applicant held a neighborhood meeting on Wednesday, March 27th, 2024 at the City of Portage Westfield Park at 4500 W. Milham Avenue in Portage with no attendees.

**II. BACKGROUND INFORMATION:**

The subject site is owned by the applicant since 2022. Sometime in 2023, the owner cleared part of the backyard and installed fencing. The current day care operations of caring for up to six children uses the fenced-in area.

The following table summarizes **Existing Conditions** in this general area.

Existing Land Use/ Zoning	<ul style="list-style-type: none"> <li>• The subject site consists of a single-family house and licensed day care home, and zoned: R-1B One Family Residential</li> <li>• <u>North (across W. Milham Ave)</u>: New Apostolic Church of Portage; 3600 W. Milham Ave; Zoned R-1B One Family Residential</li> <li>• <u>South</u>: Single Family Homes; 3628, 3658 Westcove Dr; Zoned R-1B One Family Residential</li> <li>• <u>West</u>: Single Family Home; 3629 W. Milham Ave; Zoned R-1B One Family Residential</li> <li>• <u>East</u>: Single Family Home; 3549 W. Milham Ave; Zoned R-1B One Family Residential</li> </ul>
Public Streets	2022 W. Milham Avenue (near W. Milham Ave & Angling Rd intersection) recorded 14,141 AADT total trip counts (AADT - Annual Average Daily Trips) Recorded by Kalamazoo Area Transportation Study (KATS).
Public Utilities	City sanitary sewer and water are available along W. Milham Ave. and are connected to the subject house.
Environment	The site is occupied by single family detached house with generally maintained lawn and natural vegetation.
Unique Conditions	No mapped wetlands or flood hazards exist on the site. No mapped or identified historical structures/ buildings exist.

**Applicable Zoning Regulations**

The following sections of Chapter 42. Division 5. Subdivision 1. - Special Land Uses and Subdivision 2 - Site Plan Review and Division 6 Subdivision 1- Off-Street Parking & Loading, Reduction in Parking Requirements procedures and regulations apply and addressed further in this report:

- Special Land Uses: Sec. 42-460 thru – 462
- Parking Reduction Request: Sec 42-520 (O)

### III. ANALYSIS:

#### Analysis

##### Special Land Use Permit procedures

Pursuant to the Special Land Use Permit Sec 42-461 *Application and review procedures*, the applicant has submitted a complete Special Land Use Permit application, site drawings, and fee. The Planning Commission shall consider the subject permit during a public hearing and may attach conditions if deemed necessary for the general welfare, neighborhood harmony, and other measure to ensure the intent and object of the zoning ordinance is observed.

##### Group Child Care Homes Standards

Proposed Group Child Care Homes in the R-1A thru -D One Family Residential districts must meet specific standards pursuant to Sec 42-182 (I), detailed as the following:

#### I. *Group child care homes.*

- 1. The lot containing the group child care home shall not be located closer than 1,500 feet to any lot occupied by any of the following uses. This distance shall be measured as a traveled distance along public streets, but not including an alley. Nor shall the lot containing the group child care home be located less than 500 feet to any lot occupied by any of the following uses, as measured form the nearest property lines of each use.*
  - a. Another licensed group child care home.*
  - b. An adult foster care small group home or large group home.*
  - c. A facility offering substance abuse treatment and rehabilitation service.*
  - d. A community correction center, resident home, halfway house or similar jurisdiction administered by the Michigan Department of Corrections or other state or federal agency.*
- 2. If the lot occupied by the group child care home is located between the distances noted in [subsection] 1, above, the planning commission shall make a finding that the proximity of the uses will not result in excessive noise, traffic or other disturbances which may adversely affect abutting or nearby residential uses.*
- 3. The planning commission may require up to a six-foot high screening fence completely enclosing the area where children play or congregate in order to mitigate and/or avoid possible adverse impacts on surrounding property and to improve safety.*
- 4. The group child care home shall be maintained in a manner visibly characteristic of the surrounding neighborhood. No signs advertising the child care home are permitted.*

5. *No more than one nonresident employee, whether compensated or not, in addition to the operator, shall be permitted for a group child care home, unless required by the state as part of the approval of the license.*
6. *Off-street parking shall be provided for the group child care operator and nonresident employee(s). Two client parking spaces shall also be provided.*
7. *A revocation or refusal to renew a license or certificate of registration of a group child care home shall cause the revocation of the special land use permit.*

The subject site is primarily used as a residence and secondary as a home-based child day care operation licensed by the State. According to the applicant's submitted narrative, floor plans, and site image, the majority of the house and fenced backyard will continue to be used for general child care activities. Staff notes that under the subject Special Land Use Permit review, professional drawings are not required. No changes to the site are proposed. Access is along W. Milham Avenue. Increasing the number of children cared for from six to 12 will result in a slight increase in parking demand for the site. The house and fenced backyard area are located downhill from the road and screened from adjacent residences. Parking for the homeowner and child care parents/ guardians drop-offs and pick-ups for the child care operations will continue to occur on the existing driveway and parking area. The proposal met the above requirements except #6, off-street parking. Further details about parking are described later under 'Parking Reduction Request'.

The applicant has attached a State license form for the City to complete if overall approval is gained by the City. It is expected that if the subject Special Land Use Permit and Parking Reduction Request are approved, the City will complete the State form to allow the applicant to proceed with the required State licensing procedures and inspections.

#### Sec 42-520 (O) Parking Reduction Request

Pursuant to Sec 42-520, the application shall meet standards addressing reduction in parking, impacts on neighbors and adjacent streets. All requests shall be voted by the Planning Commission.

Pursuant to Sec 42-182 (I)6 the parking requirement for a Group Child Care Home consists of one space for the operator, one space for each non-resident employee, and two spaces for parents/ guardians of client children. The applicant has indicated the need for one non-resident employee for the proposed Group Child Care Home per State requirement. Therefore, the proposal requires three spaces. Additionally, two spaces are required for the single-family house. Since the operator is also the homeowner, an overall total of five spaces is required for the property. The property can only accommodate a total of four spaces, however. Two spaces inside the garage plus two spaces in the driveway. See attached site pictures. City staff asked the owner about options to expand the driveway to accommodate all five required spaces. The applicant indicated that the received contractor estimates for expansions were cost prohibitive due to challenging grades and the overall price of work. Therefore, the applicant submitted the subject requested parking reduction request to permit four spaces instead of five for the proposed Group Child Care Home. The applicant has submitted a narrative addressing the parking reduction request code section and indicates the unique parking demands of the existing and proposed child day care operations. Additionally, the applicant has submitted a parking agreement

outlining general parking areas and prohibitive areas which will be acknowledged and signed by parents/guardians. Staff notes that the property's only access is off of W. Milham Avenue, which is a heavily-traveled, five-lane road and does not allow on-street parking.

Staff notes that child day care operations of a home-based or commercial business tend to have limited parking demands which generally occur at peak times during typical morning and afternoon commuting hours. Further, child day care operations generally do not have customers or shoppers, nor regularly scheduled appointments more commonly found in other service-oriented businesses. Also, operators of child day care businesses tend to have greater control and limited times of expected parking demand. This often results in very low parking demands outside of the drop-off, pick-up times. Lastly, home-based operators can better inform where and how parents/guardians can park on their private driveway.

Staff finds that the applicant's narrative and parking agreement meets Sec 42-182(I)(6) and ensures that the forecasted increase in parking demand will be contained within the driveway, and that no parking or staging will occur on W. Milham Avenue. But, if approval is gained and parking or staging does occur on W. Milham Avenue, staff recommends a condition of approval requiring the applicant to file a permit to provide one additional on-site parking space.

#### Comprehensive Plan

The Comprehensive Plan indicates that Special Land Use Permit applications should be consistent with the Plan's Future Land Use Map, appropriateness of future uses within the surrounding neighborhood, impacts on adjacent streets and utilities, and consistent with guidelines within the Plan.

The existing Future Land Use Map classifies the subject property as 'Low Density Residential' which encourages single family residences at densities of 1-4 units per acre. This classification covers the majority of the greater neighborhood. This classification corresponds to the R-1B thru R-1E zoning districts. The underlying zoning district is R-1B. The proposed Future Land Use Map in the draft Master Plan classifies the subject property as 'Single Family Residential' which has similar characteristics as the 'Low Density Residential'. The proposed Group Day Care Home is not expected to result in negative impacts on the neighborhood due to the large lot size, fenced outdoor area, indoor operations, and the submitted parking agreement. Therefore, staff finds the proposal consistent with the existing and proposed land use classification.

#### **Public Noticing Requirement**

Public notice was published in the Kalamazoo Gazette and a notice was sent to all property owners/occupants within 300 feet of the subject property on April 30, 2024. As of the date of this report (Wednesday, May 9), no public comments have been received.

#### **IV. RECOMMENDATION:**

Based on the above analysis and subject to any additional information brought before the Planning Commission during the public hearing, staff recommends the Special Land Use Permit and Parking Reduction Request for a Group Day Care Home at 3609 W. Milham Avenue, be approved with the following condition:

1. If parking or staging occurs on W. Milham Avenue, the applicant shall file a permit to provide one additional on-site parking space.

### **Sources**

1 Bolan, Molly. “How One State Is Tackling Child Care Challenges.” *Route Fifty*. 30 May, 2023, <https://www.route-fifty.com/management/2023/05/one-states-plan-address-child-care-issues/386903/>

2 Miller, Cain Claire. “With Pandemic Money Gone, Child Care Is an Industry on the Brink.” *The Upshot New York Times*. 25 February, 2024. <https://www.nytimes.com/2024/02/25/upshot/child-care-centers-struggling.html>

3 Harthorn, Jessica. “Michigan child care crisis: over 1,200 centers at risk of closure due to federal funding cuts, report finds.” *News Channel 3*. 23 August 2023 <https://wwmt.com/news/local/michigan-child-care-crisis-over-1200-centers-at-risk-of-closure-due-to-federal-funding-cuts-arpa-childcare-closing-working-parents-ymca-montessori-school-kresa-head-start-kids-daycare>

4 French, Ron. “Childcare is a critical job, with a critical worker shortage in Michigan”; *Bridge Michigan*. 25 August 2023. <https://www.bridgemi.com/talent-education/childcare-critical-job-critical-worker-shortage-michigan#:~:text=As%20of%202022%2C%20there%20were,children%20to%20go%20to%20work>.

5 ‘MI Tri-Share’. State of Michigan, Department of Lifelong Education, Advancement, and Potential. <https://www.michigan.gov/mileap/early-childhood-education/mi-tri-share-child-care>

### **Attachments:**

1. Special Land Use Permit Application
2. Vicinity map
3. Site Map
4. Group Day Care Home Floor Plans
5. Narrative for Daycare
6. Applicant's Response to Special Land Use Criteria
7. Parking, Driveway Agreement
8. State License Application
9. Neighborhood Meeting Invite
10. Staffs Site Pictures
11. 5.14.24 Communication from Christine Lemons
12. 5.15.24 Communication from Michelle Voss



Department of Community Development

**APPLICANT INFORMATION**

Name Madison Schmidt		Telephone Number 269-203-5683	
Address 3609 W milham ave	City Portage	State MI	Zip code 49024

**OWNER INFORMATION (if different)**

Name		Telephone Number	
Address	City	State	Zip code

**PROPERTY INFORMATION**

Address of property 3609 W milham ave Portage, MI 49024	Zoning District	Land Area (acres)
Legal Description (or attach separate page)		

**PROPOSED USE**

Description of proposed Special Land Use (attach additional page(s), if necessary)

I currently run a in home daycare. I am asking for this permit so I can expand my daycare into a group child care home. childcare is in high demand right now and I am having to put people on a waitlist.

**OWNER CERTIFICATION**

I hereby certify that I am presently the legal owner for the above-described property and all of the above information is true and accurate. I further acknowledge that approval of this Special Land Use Permit constitutes an agreement with the City of Portage and all conditions or limitations imposed shall be fulfilled.

Maelyn Keller  
Signature

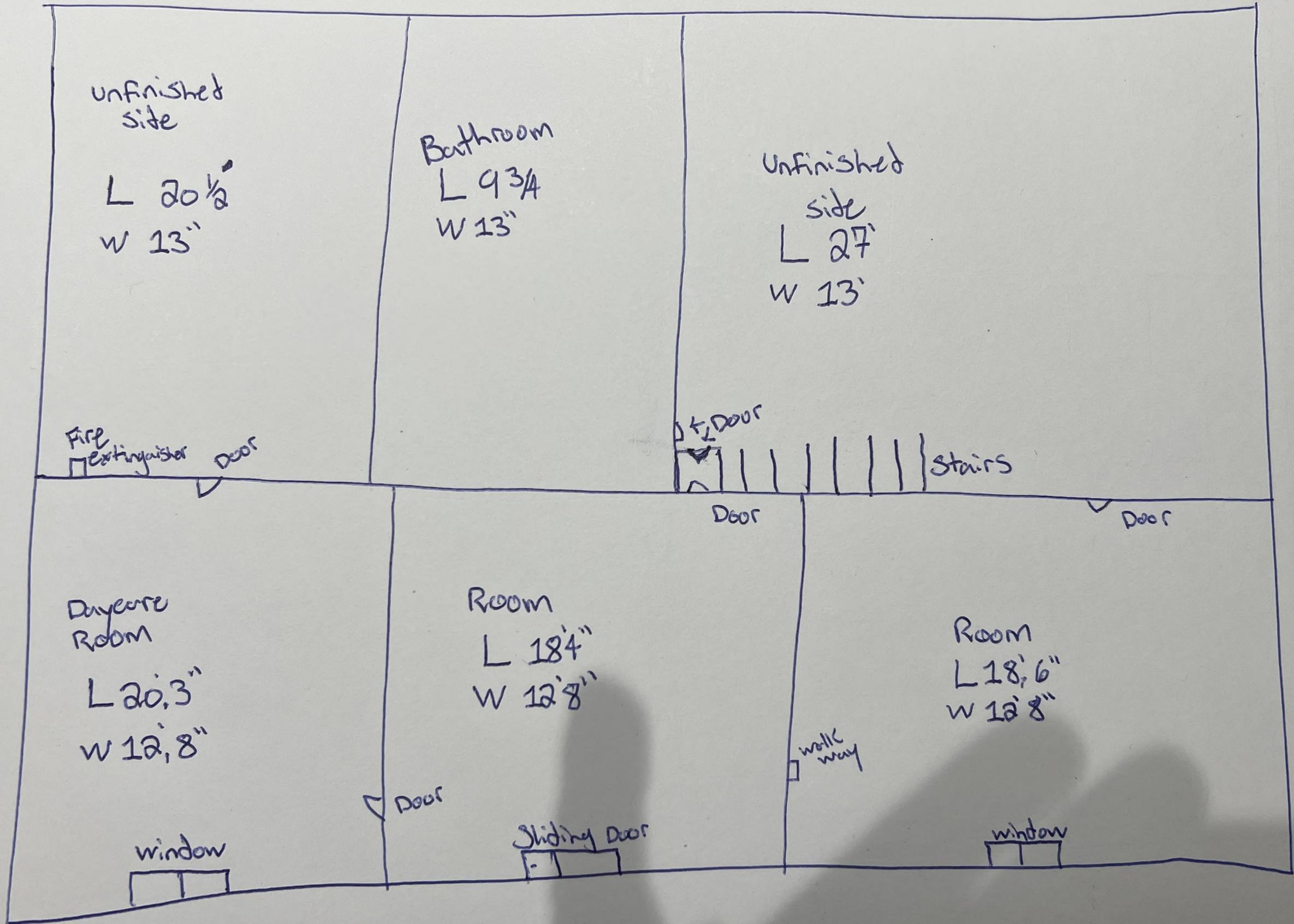
3-28-2024  
Date



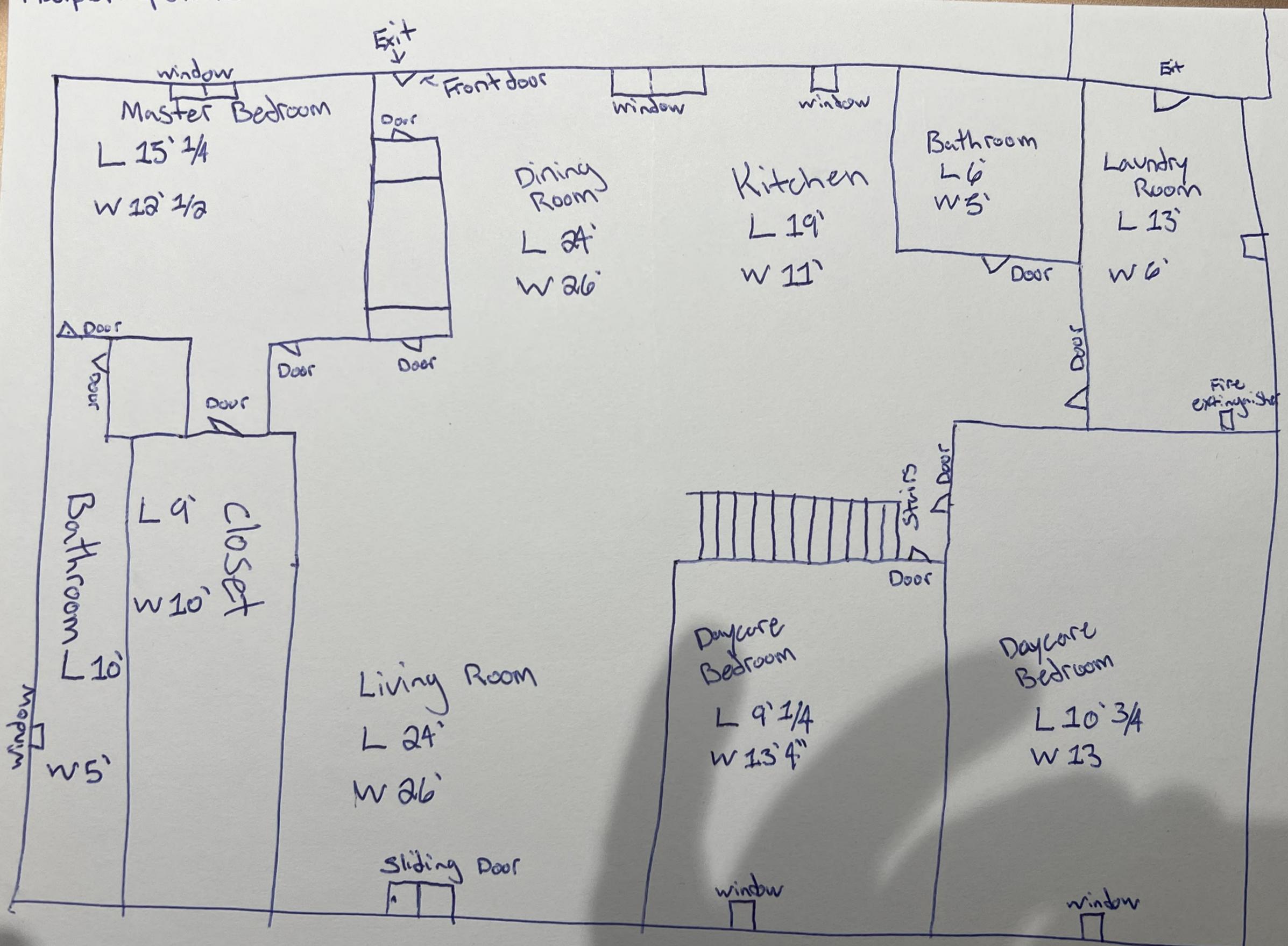
# Site Map



# Floorplan Downstairs



Floorplan upstairs



1A: I own a licensed in home daycare I am trying to expand. I have overall four parking spaces in my driveway. I only have had 2 or at most 3 parents picking up at the exact same time. With that being said, the space I have is plenty to accommodate the parents during pick up and drop off times.

1b: Drop off times vary from 6:00am-9:00am. None of my kids currently get dropped off at the same time. Pick up times vary from 3:00pm-5:30. Also none of my kids currently get picked up at the exact same time.

1c: The sidewalk to enter the house will never be driven/parked on or blocked and will always have access for people to enter the home.

1d: N/A

1E: All of my current daycare parents access the driveway while dropping and picking up with no issues.

2. This is not a temporary situation. I will be licensed for 6 more kids and then no more after that in a group child care home.

3. There will be absolutely no parking, waiting and staging on W Milham Ave. My new daycare parents will be signing a form that they understand.

4. N/A

5. There is not really a "peak parking demand" all of my children get picked up at different times due to their parents schedules.

6. I also have a special land use permit I requested.

7. I will attend the planning commission meeting for this and my special land use permit

8. My daycare parents will be aware parking and waiting on W Milham Ave is not allowed and it will not affect the traffic circulation. There will be a form that parents will have to sign acknowledging this.

## **Project Description**

I currently run a family in home daycare “Madison’s Childcare” at 3609 W. Milhave Ave Portage, MI 49024. I am open M-F 6:00am – 5:30pm. I currently care for 6 children and with this approved I will be able to care for 12. Drop off times vary from 6:00am-8:30am. Pick up times vary from 4:00pm-5:30pm. Cars will pull in the driveway to drop off and then turn around and pull out to leave. Cars that stay here will be parked in garage and in driveway or front yard.

### **Sec. 42-462. - General standards for review of special land uses.**

The planning commission shall take the following standards into consideration in its review of any special land use application:

- A. Promote the intent and purpose of this article;
  - Applicant: My reason for writing this is because I currently own a family in home daycare and I am applying to be licensed for a group day care. I currently have 6 kids and I am wanting to expand to 12. I currently have people on a waitlist and childcare is needed very badly in our community.
- B. Be compatible with adjacent uses of land and the natural environment;
  - Applicant: I will be running the daycare out of my home as it. I will NOT be building or adding on.
- C. Not unduly affect the capacities of public services or facilities;
  - Applicant: I will not be using more than necessary of my public services. I will not disrupt the facilities and homes around me.
- D. Be consistent with the public health, safety and welfare;
  - Applicant: I will be consistent with these by all of the children in my care being vaccinated and having safety plans for any abrupt situations.
- E. Be harmonious with and in accordance with the general objectives or with any specific objective of the comprehensive plan; and
  - Applicant: I understand the rules and ensure that they are met.
- F. Be planned and designed to ensure that the nature and intensity of the principal use and all accessory uses, and the site layout and its relation to the streets giving access to it, shall not be hazardous or otherwise detrimental to the area or unduly conflict with normal traffic to and from the use.
  - Applicant: The cars picking up and dropping off will not affect the safety of the traffic. They will pull into the driveway for drop off and pickup. They will NEVER be parking on the road.

# Madison's Childcare Parking/Driveway Agreement

During pick up and drop offs you will enter the driveway and go down and park at the end. Once your child is dropped off or picked up, you will then back up in the turn around area and drive out of the driveway.

NO parking, waiting or staging will be taking place on W Milham Ave.

Parent/ Guardian:

Daycare Owner:

**ZONING APPROVAL FOR GROUP CHILD CARE HOMES**  
Michigan Department of Licensing and Regulatory Affairs  
Bureau of Community and Health Systems

Licensee Name: Madison Schmidt  
Licensee Address: 3609 W Milham Ave Portage, MI, 49024

License Type: DG – Group Child Care Home (capacity 7-12 children)

Zoning Authority:

According to the Michigan Zoning Enabling Act, 2006 PA 110, a group child care home located in a county or township shall be issued a special use permit, conditional use permit, or other similar permit if the group child care home meet specific standards. A group child care home located in a city or village may be issued a special use permit, conditional use permit, or other similar permit.

Please complete the lower portion of this form and return this completed form to the licensee/applicant.

If you have any questions or concerns, please contact the Michigan Department of Licensing and Regulatory Affairs, Bureau of Community and Health Systems, at 517-284-9730.

Thank you.

- Location is APPROVED by the local zoning authority.
- Location is DISAPPROVED by the local zoning authority.
- City, township or county is unzoned.

\_\_\_\_\_  
Signature of Zoning Authority or  
City, Township, County Manager for Unzoned Communities

\_\_\_\_\_  
Date

\_\_\_\_\_  
Telephone Number

\_\_\_\_\_  
Printed Name of Zoning Authority or  
City, Township, County Manager for Unzoned Communities

\_\_\_\_\_  
Jurisdiction (City, Township)

Authority: 1973 PA 116  
Completion: Required  
Penalty: Applicant cannot be licensed/registered

LARA is an equal opportunity employer/program.

# Madison's Childcare

Hi everyone! My name is Madison Schmidt and I own a in home daycare located at 3609 w milham ave called "Madison's Childcare"

I am currently applying to expand my daycare which means I need zoning approval through the City of Portage!

If you would like to join me on Wednesday March 27th, 2024 join me at Westfield Park from 5:45pm-6:15pm to discuss the expansion and I can answer any questions you may have!

Thank you all for your support!

## Staff's Pictures April 8, 2024



Figure 1. Looking south across W. Milham Avenue toward subject residence (3609 W. Milham Avenue).

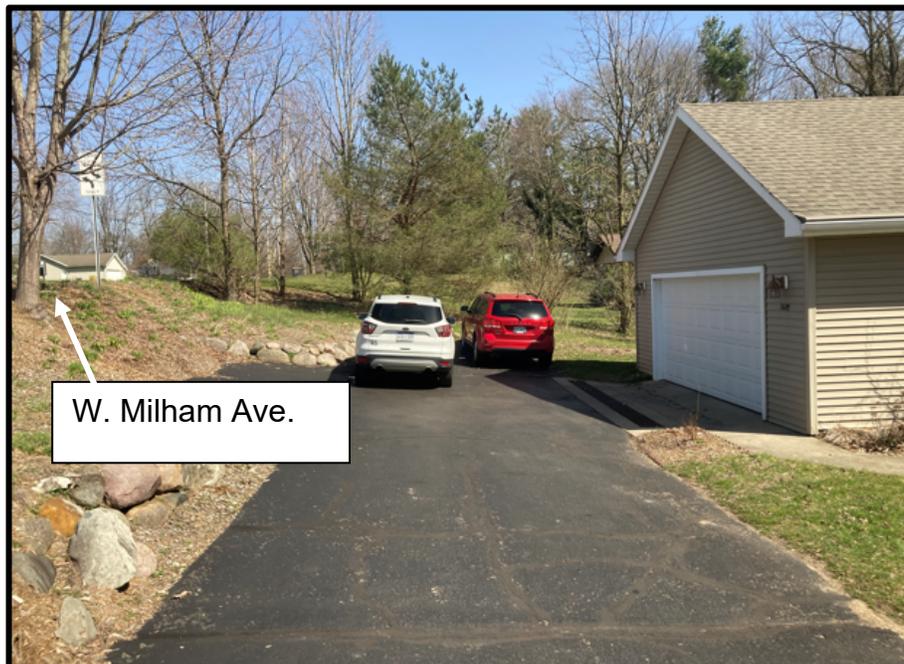


Figure 2. Looking east at parking area for homeowners and day care drop-offs/ pick-ups located in front yard. W. Milham Avenue located to the left, uphill of driveway.



Figure 3. Looking north toward driveway entrance at W. Milham Avenue, located slightly uphill from house.



Figure 4. Looking south at fenced backyard used for current and proposed day care operations. Right fence side is west property line (approx.).



Figure 5. Looking south at backyard area. Left fence side is east property line (approx.).

## Eric Feldt

---

**From:** Christine M. Lemons <christine.lemons@gmail.com>  
**Sent:** Tuesday, May 14, 2024 7:16 PM  
**To:** Eric Feldt  
**Cc:** Don Lemons  
**Subject:** Concerns Regarding Proposed Zoning Change for Group Daycare Home at 3609 W Milham Ave

**CAUTION: THIS EMAIL IS FROM AN EXTERNAL SENDER**

Do not click on links or open attachments unless this is from a sender you know and trust.

Dear Mr. Feldt,

We are writing to express our concerns regarding the proposed zoning change for the home located at 3609 W Milham Ave, behind our home, to operate a Group Daycare Home-Childcare Facility. The recent developments, including the removal of many trees, the erection of a white fence (that doesn't provide a barrier) and numerous visible children's play structures, have raised several issues that greatly impact our neighborhood.

Firstly, the lack of notification and transparency prior to these significant developments is concerning. Neither we nor several of our neighbors received any communication or invitation to discuss these developments prior to their implementation. This oversight has left us feeling blindsided and excluded from important decisions impacting our neighborhood.

Moreover, the current setup with the steep hill and the white fence provides little to no privacy for both the children in daycare and us residents. We are now directly looking at their playground area, which not only compromises the children's privacy, but also disrupts the peaceful ambiance of our private backyard. We believe that installing taller privacy measures, such as evergreen trees or a more substantial fence, would mitigate these privacy concerns.

Furthermore, we are apprehensive about the potential impact of this zoning change on the residential character of our neighborhood and our property values. While we understand and support the operation of small home daycares, the prospect of a larger-scale childcare facility operating in a residential area raises valid concerns about increased noise, traffic, parking issues and potential further expansion. We fear that this could set a precedent for further commercialization of our neighborhood, ultimately diminishing the appeal and value of our homes.

In light of these concerns, we urge you to carefully consider the implications of this proposed zoning change and take into account the interests and well-being of all affected residents. Preserving the residential character of our neighborhood and safeguarding our property values should be paramount in any decision-making process regarding zoning changes.

We respectfully request keeping it as an in-home childcare and not changing the zoning.

Thank you for your attention to this matter.

Sincerely,

Don & Christine Lemons

3628 Westcove Drive, Portage, MI 49024

D- 269.568.0688

C- 269.568.0686

"Knowledge speaks, Wisdom listens, Hearts write."

~ CML

## Eric Feldt

---

**From:** Michelle Voss <michellemarievoss@gmail.com>  
**Sent:** Wednesday, May 15, 2024 8:55 PM  
**To:** Eric Feldt  
**Subject:** Public Comment on Special Land Use Permit May 16

**CAUTION: THIS EMAIL IS FROM AN EXTERNAL SENDER**

Do not click on links or open attachments unless this is from a sender you know and trust.

Dear Mr. Feldt,

This comment is in response to the Special Land Use Permit request by Madison Schmidt to operate a Group Child Care Home at 3609 W. Milham Avenue. We are neighboring property owners at 6041 McGillicuddy Ln in the Homestead neighborhood. We received notification in the mail of this request on Thursday May 9th. We do not recall receiving notice of a neighborhood meeting held by the applicant on March 27th.

We purchased our home in July 2022 and have since witnessed the removal of several trees and the installation of a screening fence on the property on W. Milham Ave. Unfortunately, as this property and backyard are located downhill from the road and from our property off of McGillicuddy Ln. the fence does little to screen the backyard, including play structures, from our view. This is most noticeable in the fall, winter and spring months when there is little foliage on the trees. Therefore, we do not believe the proposed Group Child Care Home is being maintained in a manner that is visibly characteristic of the surrounding neighborhood and find the screening fence does not avoid possible adverse impacts on the surroundings.

We understand and appreciate the need for available and affordable child care in Kalamazoo county. However, we are also nervous about the potential negative impact to our property value and are concerned about the negative impact we have already seen to our surroundings. If the City of Portage plans to approve the permit for a Group Day Care Home at 3609 W. Milham Avenue, we propose the follow condition be added:

1. Additional privacy screening will be added on the West and South ends of the property. For example, this may be accomplished by adding vegetation such as evergreen trees to avoid adverse impacts on surrounding property.

Thank you for your consideration.

Sincerely,

Michelle Voss & Jacob Spreitzer  
6041 McGillicuddy Ln, Portage, MI

**TO:** Planning Commission

**DATE:** May 16, 2024

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** Special Land Use Permit and Site Plan to operate a Contractor’s Business at 9651 Portage Road.

**I. INTRODUCTION:**

The applicant, KCH Ventures LLC, has submitted a Special Land Use Permit and Site Plan to operate a contractor’s business at 9651 Portage Road located south of City’s Lakeview Park. The business is Otis Elevator and will occupy one of the three existing commercial tenant spaces with new offices and business materials. This operation is considered a ‘contractor’s business’ within the underlying B-3 General Business District under Sec 42-262.C.6 and, therefore, requires an approved Special Land Use Permit and Site Plan by the Planning Commission.

The applicant has submitted a floor plan showing proposed conditions, project narrative, and the original approved site plan. No exterior changes are proposed, no fabricating of materials will take place on the site, and no exterior storage of equipment or materials or supplies will be on the site. Business operations are Monday-Friday only and between 7:30am – 4pm. Two office staff will provide part-time hours/ days of week; and ten field employees will provide off-site technical repairs, inspections, and maintenance of various elevators in the area. Daily and weekly deliveries of business materials and goods are expected. The existing parking lot will be used for employees and deliveries. Public walk-ins are not expected.

Per the City’s Public Participation Plan, the applicant held a neighborhood meeting for adjacent property owners on Saturday, April 6 at the subject site. Two adjacent owners attended and indicated no issues with the project.

**II. BACKGROUND INFORMATION:**

The site was purchased by KCH Ventures LLC, in August 2023 and consists of a three-tenant two-story commercial building, parking lot, and dog park. The building is used by two tenants: 1) Hounds & Co., and 2) Barks & Bubbles; and a third tenant space is currently vacant but is being sought by the subject business. The dog park is private and associated with the existing tenants. The building and parking lot gained an approved Site Plan in 2002. The dog park was later approved in 2018 with a Special Land Use Permit by the Planning Commission. Both approvals were under a prior owner.

The following table summarizes **Existing Conditions** in this general area.

Existing Land Use/ Zoning	<ul style="list-style-type: none"> <li>The subject site consists of a commercial development and zoned B-3 General Business District</li> </ul>
------------------------------	---

	<ul style="list-style-type: none"> <li>• <u>North</u>: Vacant – privately owned; 9599 Portage Rd; Zoned B-3 General Business District</li> <li>• <u>South</u>: Direct Services Inc (trucking company); 9711 Portage Road; Zoned I-1 Light Industrial</li> <li>• <u>West (across Portage Rd)</u>: Autism Treatment &amp; Research; 9616 Portage Road; Zoned OS-1 Service Office; Macks Fire Protection; 9650 Portage Rd; Zoned I-1 Light Industrial _</li> <li>• <u>East</u>: Vacant – City owned; 9617 Portage Rd; Zoned R-1B One Family Residential _</li> </ul>
Public Streets	2015 Portage Road near subject site recorded 15,478 AADT total trip counts (AADT - Annual Average Daily Trips). 2023 Portage Road near S. Shore recorded 17,340 AADT. Recorded by Kalamazoo Area Transportation Study (KATS).
Public Utilities	City sanitary sewer and water are available along Portage Road.
Environment	The site is developed with a commercial building and parking lot. No mapped environmental items on property.
Unique Conditions	No mapped wetlands or flood hazards exist on the site. No mapped or identified historical structures/ buildings exist.

**Applicable Zoning Regulations**

The following sections of [Chapter 42. Division 5](#). Subdivision 1. - Special Land Uses and Subdivision 2 - Site Plan Review procedures and regulations apply and addressed further in this report:

- Special Land Uses: Sec. 42-460 thru 462
- Site Plan: Sec. 42-482 thru 484

**III. ANALYSIS:**

Permitting Procedures

Pursuant to the Special Land Use Permit Sec 42-461 Application and review procedures and Site Plan Sec 42-482 the applicant has submitted a complete application, site drawings, and fee. The Planning Commission shall consider the subject permit during a public hearing and may attach conditions if deemed necessary for the general welfare, neighborhood harmony, and other measures to ensure the intent and object of the zoning ordinance is observed.

Sec 42-462 General standards for review of special land uses

Pursuant to Sec 42-462 for Special Land Use requests, the applicant shall meet standards addressing neighborhood harmony, impacts on the natural environment, impacts to public services/ utilities, and general consistency with the zoning ordinance.

Sec 42-483 Site Plan Review Standards

Pursuant to Sec 42-483 for Site Plan requests, the applicant shall meet various site standards such as building setbacks, parking lot dimensional requirements, lighting, signage, landscaping, overall circulation (vehicular and non-vehicular), utility standards, and more.

As mentioned above, the previously approved Site Plan, Special Land Use Permit, and subsequent building permits have permitted today's existing two-story, three-tenant commercial building, associated parking lot, dog park, and driveway access off of Portage Road. The proposed Site Plan and Special Land Use Permit for a new contractor's business is described in the attachments only consists of interior reconfiguration of the vacant tenant space. Based on the business operation narrative, the operations are expected to result in little traffic and parking demand beyond what currently exists on site and along Portage Road. No exterior renovations to the building nor site are proposed nor required for the proposed business. Therefore, a new Site Plan drawing is not required.

Based on the 2002 approved Site Plan for the review of the subject proposal, staff determined no additional site improvements are required. However, during a recent site visit, several landscape items required under the approved Site Plan were missing. Examining past aerial photographs of the site, the missing landscape had been installed at the time. No approved changes to the 2002 Site Plan landscaping were found in staff's records. Therefore, landscaping was removed sometime in the past without approval and needs to be replanted.

Comprehensive Plan

The 2014 Comprehensive Plan indicates that Special Land Use Permit applications should be consistent with the Plan's Future Land Use Map, appropriateness of future uses within the surrounding neighborhood, impacts on adjacent streets and utilities, and consistent with guidelines within the Plan. The Future Land Use Map classifies the subject property as 'Industrial' which encourages general manufacturing, processing of goods and materials, pharmaceutical and medical equipment development, trucking and logistics, etc. Staff finds the proposal consistent with that classification. Many commercial businesses located near and south of the subject site are industrial in nature.

Evaluations from the Development Review Team (DRT)

This project was reviewed by the Development Review Team (DRT) who ensures that the proposed development is consistent with the City's infrastructure, engineering, and other requirements including streets, sewers, water mains, storm water, parking, landscaping, and others. The DRT is comprised of representatives from the Department of Transportation and Utilities, Fire Department, Department of Parks and Recreation, Office of the City Assessor, and Department of Community Development. DRT's review found the conditions below: the proposal meets Sec 42-462 and Sec 42-483:

- Plant bushes and trees consistent with the 2002 approved Site Plan.

**Public Noticing Requirement**

Public notice was published in the Kalamazoo Gazette and a notice was sent to all property owners/occupants within 300 feet of the subject property on April 30, 2024. As of the date of this report (Thursday, May 9), no public comments have been received.

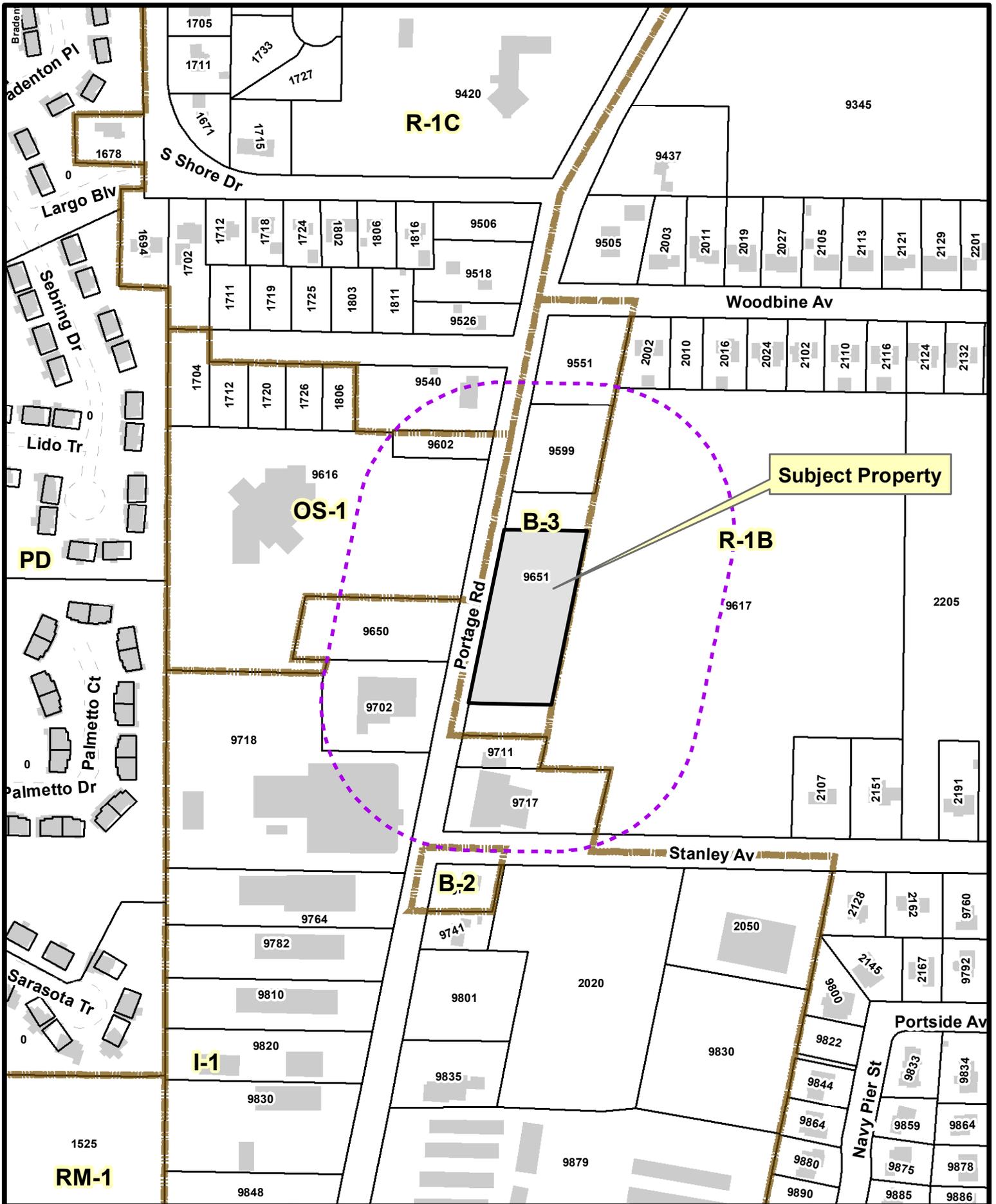
**IV. RECOMMENDATION:**

Based on the above analysis and subject to any additional information brought before the Planning Commission during the public hearing, staff recommends the Special Land Use Permit and Site Plan to operate a Contractor's Business at 9651 Portage Road, be approved with the following condition:

1. Plant bushes and trees consistent with the 2002 approved Site Plan.

**Attachments:**

1. Vicinity Map
2. Special Land Use Permit Application
3. Original Plans, Site Drawing
4. Proposed Floor Plans
5. Otis Business Summary - Portage MI 4.11.2024
6. Applicant's Neighborhood Mtg letter invite
7. Neighborhood Meeting Email
8. Staffs Site Pictures



-  300' Notification Area
-  Subject Property
-  Zoning Boundary

## Special Land Use Permit 9651 Portage Road



1 inch = 250 feet  
(32)

## **SPECIAL LAND USE PERMIT INFORMATION**

### INTRODUCTION:

The purpose of this information is to explain a Special Land Use and the process of obtaining approval to conduct a Special Land Use. While certain land uses are “permitted by right” in each zoning district in Portage, a Special Land Use is allowed, when conditions specified in the zoning district are met and the use is approved by the Planning Commission.

### IMPORTANT MATERIALS THAT MUST BE SUBMITTED TO THE PLANNING COMMISSION:

Applicants are responsible for providing materials for review by the Planning Commission that describe and graphically illustrate the request to obtain approval to conduct a special land use. The following information explains the responsibilities of applicants. IF SUCH INFORMATION IS NOT PROVIDED, THE COMMISSION MAY NOT BE ABLE TO ACT ON A PARTICULAR APPLICATION FOR LACK OF INFORMATION. The following is required to be submitted to the Department of Community Development at least ten working days prior to the Planning Commission meeting date:

1. A DESCRIPTION OF THE SPECIAL LAND USE TO BE CARRIED OUT on the property, including specific details about the special land use that will be on the property and which may dictate the size, shape or configuration of the building or structure to be used.
2. SKETCHES OF THE BUILDING OR STRUCTURE including floor plan, building or structure elevations and proposed location on the property, if pertinent to the application.
3. A SKETCH OF THE PROPERTY OR SITE MAP including adjoining properties and/or buildings, if pertinent to the application. Dimensions in feet of the property and building or structure are to be indicated. Topography, vegetation and other physical features are also to be shown on the sketch or site map.
4. A SPECIAL LAND USE PERMIT APPLICATION (attached) must be completed and signed by the property owner. Unless applying by mail, the Department of Community Development will prepare a computerized application with the applicant at time of submittal.
5. APPLICANT OR THEIR REPRESENTATIVE MUST ATTEND the Planning Commission meeting at which his/her application will be considered. Also, the Planning Commission will conduct a public hearing and input from the public will be accepted.
6. SPECIAL LAND USE PERMIT FEE: \$230 per group day care home application  
\$340 for all other applications

FOR FURTHER INFORMATION ON SPECIAL LAND USE PERMITS: Please feel free to contact the Department of Community Development at 329-4477. Department staff will be glad to assist you.

**APPLICANT INFORMATION**

Name Eric Eldred		Telephone Number 269 998 0970	
Address 8134 Black Forest DR	City Portage	State MI	Zip code 49002

**OWNER INFORMATION (if different)**

Name		Telephone Number	
Address	City	State	Zip code

**PROPERTY INFORMATION**

Address of property 9651 Portage Rd	Zoning District B3	Land Area (acres) 1.4
--	-----------------------	--------------------------

Legal Description (or attach separate page)  
SECTION 26-3-11 COM AT INT OF ELY LI PORTAGE RD WITH N LI OF STANLEY AVE, TH NLY ALG E LI OF PORTAGE RD 265.39 FT FOR P.O.B., TH CONT ALG SD E LI 369.2 FT, TH E 165 FT, TH SWLY PAR WITH E LI OF PORTAGE RD 369.2 FT, TH W 165 FT TO P.O.B.

**PROPOSED USE**

Description of proposed Special Land Use (attach additional page(s), if necessary)  
We are applying for a special use permit for the OTIS elevator company to occupy the eastern most unit at 9651 Portage Road. The Tenant will use the premises for storage of product, parts and equipment which are used in conjunction with their business activities. They will also have a handful of employees on site using the office space for various business activities including but not limited to: Accounting, HR, Sales, Scheduling, Service, ETC.

**OWNER CERTIFICATION**

I hereby certify that I am presently the legal owner for the above-described property and all of the above information is true and accurate. I further acknowledge that approval of this Special Land Use Permit constitutes an agreement with the City of Portage and all conditions or limitations imposed shall be fulfilled.

Eric C Eldred	4/9/2024
Signature	Date

eSigned via SeamlessDocs.com  
*Eric Eldred*

Key: 4ad1d8f03b0a5b47496b7a8f104a4ec4

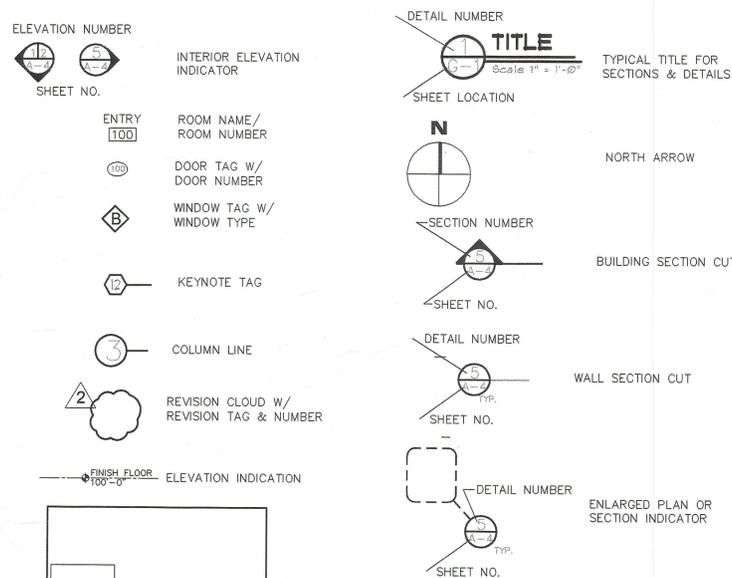
# NEW BUILDING PRECISION BUILDERS

## 9651 PORTAGE ROAD PORTAGE MICHIGAN

ARCHITECTURE  
ONE

ARCHITECTURE ONE, INC.  
450 W. CENTRE  
PORTAGE, MICHIGAN 49024  
PHONE: 269.329.1575  
FAX: 269.329.1580  
EMAIL: RCARCH1@ISERV.NET

### ARCH. SYMBOLS & PLAN INDICATORS



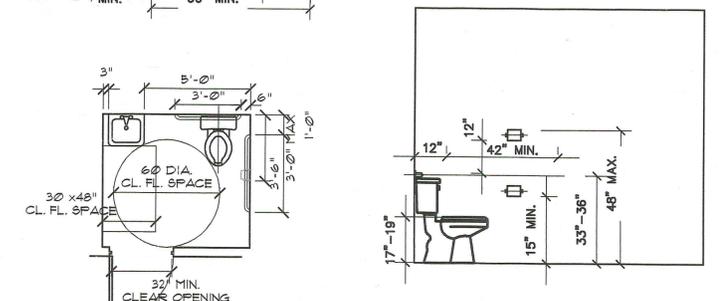
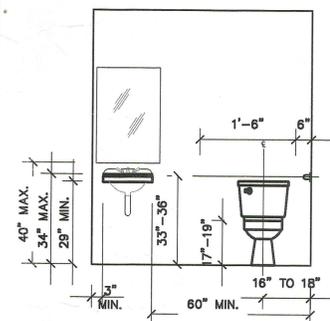
### SHEET INDEX

- O-1 COVER SHEET
- 1 SITE PLAN
- SP-1 OUTLINE SPECIFICATIONS
- S-1 FOUNDATION PLAN
- A-1 FLOOR PLAN
- A-2 PLAN OF 2ND FLOOR AND 2nd FLOOR FRAMING
- A-3 ELEVATIONS AND EXTERIOR SECTION
- A-4 WALL SECTIONS
- A-5 WALL SECTIONS AND DETAILS
- P-1 PLUMBING PLAN
- M-1 MECHANICAL PLAN
- E-1 ELECTRICAL PLAN

### CODE REVIEW

**OCCUPANCY - B**  
**TYPE OF CONSTRUCTION TYPE III - 3**  
 ALLOWABLE AREA PER TABLE 503 15,000 SF  
 4 FLOORS  
**SPRINKLER SYSTEM - NO**

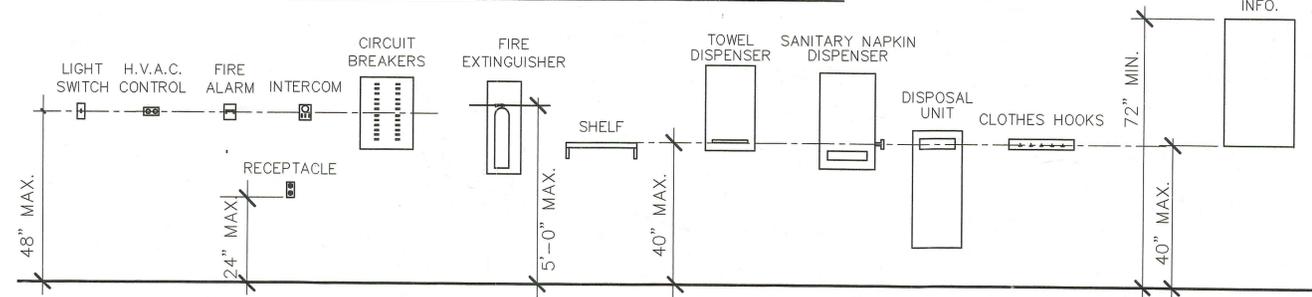
**NEW BUILDING AREA**  
**FIRST FLOOR 1,000 SF**  
**SECOND FLOOR 2,231 SF**  
**TOTAL BUILDING AREA 3,231 SF**



STANDARDS FOR TYPICAL MICH. BLDG. CODE



### GENERAL ITEMS MOUNTING HEIGHTS



NEW BUILDING FOR  
PRECISION BUILDERS  
9651 PORTAGE ROAD  
PORTAGE MICHIGAN

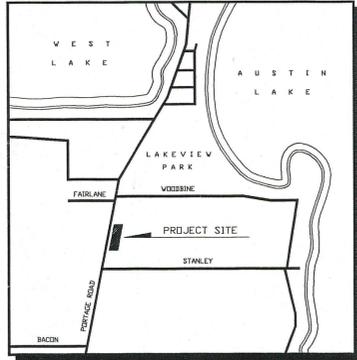
DATE  
2-22-02

REVISIONS  
FINAL CONST.  
SET  
3/25/03

SHEET TITLE

PROJECT NUMBER  
02011

SHEET NUMBER  
O-1



LOCATION MAP

DESCRIPTION OF PROPERTY:

LOCATED IN THE CITY OF PORTAGE, KALAMAZOD COUNTY, STATE OF MICHIGAN

COMMENCING AT THE INTERSECTION OF THE EASTERLY LINE OF PORTAGE ROAD WITH THE NORTH LINE OF STANLEY AVENUE (SAID POINT BEING 2 RODS NORTH OF THE EAST AND WEST ONE-EIGHTH LINE OF THE SOUTHWEST QUARTER OF SECTION 26, TOWN 3 SOUTH, RANGE 11 WEST); RUNNING THENCE NORTHEASTERLY ALONG THE EASTERLY LINE OF PORTAGE ROAD, 265.39 FEET FOR THE PLACE OF BEGINNING OF THE LAND HEREINAFTER DESCRIBED (SAID POINT BEING 1039.5 FEET SOUTH OF THE EAST AND WEST QUARTER LINE OF SAID SECTION); AND CONTINUING THENCE NORTHEASTERLY ALONG THE EASTERLY LINE OF SAID PORTAGE ROAD, 369.2 FEET TO A POINT 676.5 FEET SOUTH OF SAID EAST AND WEST QUARTER LINE; THENCE EAST PARALLEL TO SAID EAST AND WEST QUARTER LINE 165 FEET; THENCE SOUTHWESTERLY PARALLEL TO THE EASTERLY LINE OF SAID PORTAGE ROAD, 369.2 FEET; THENCE WEST 165 FEET TO THE PLACE OF BEGINNING.

OWNER/APPLICANT - ROBERT ATKINSON  
7143 WINTERFOREST  
PORTAGE, MI 49024  
(269) 383-9800

ENGINEER - GORDON JONES ASSOCIATES, INC.  
4855 W. CENTRE AVE.  
PORTAGE, MI 49024  
(269) 375-7306

SITE PLAN STORM WATER CALCULATIONS PROVIDED SEPARATELY  
IMPERVIOUS AREA - 39768.44 sq ft

PERVIOUS AREA - 20,126.25 sq ft

TOTAL STORAGE VOLUME SEE ATTACHED COMPUTATIONS -

Northwest Retention Area Req'd = 1359 cft. Provided = 1359 cft @ 0.44 ft deep

Northeast Retention Area Req'd = 2550 cft Provided = 3300 cft @ 3' deep

Southwest Retention Area Req'd = 1374 cft Provided 1374 cft @ 0.7 ft deep

Southeast Retention Area Req'd = 3300 cft Provided 3600 cft @ 3' deep

Oil & Grit Separator Area Req'd = 2 @ 148 sft Provided = 2 @ 150 sft

INTERIOR PARKING LOT LANDSCAPING REQUIREMENTS-

INTERIOR PARKING LOT AREA = 18,816 sq ft

LANDSCAPED ISLAND AREA REQUIRED = 1,254.40 sq ft

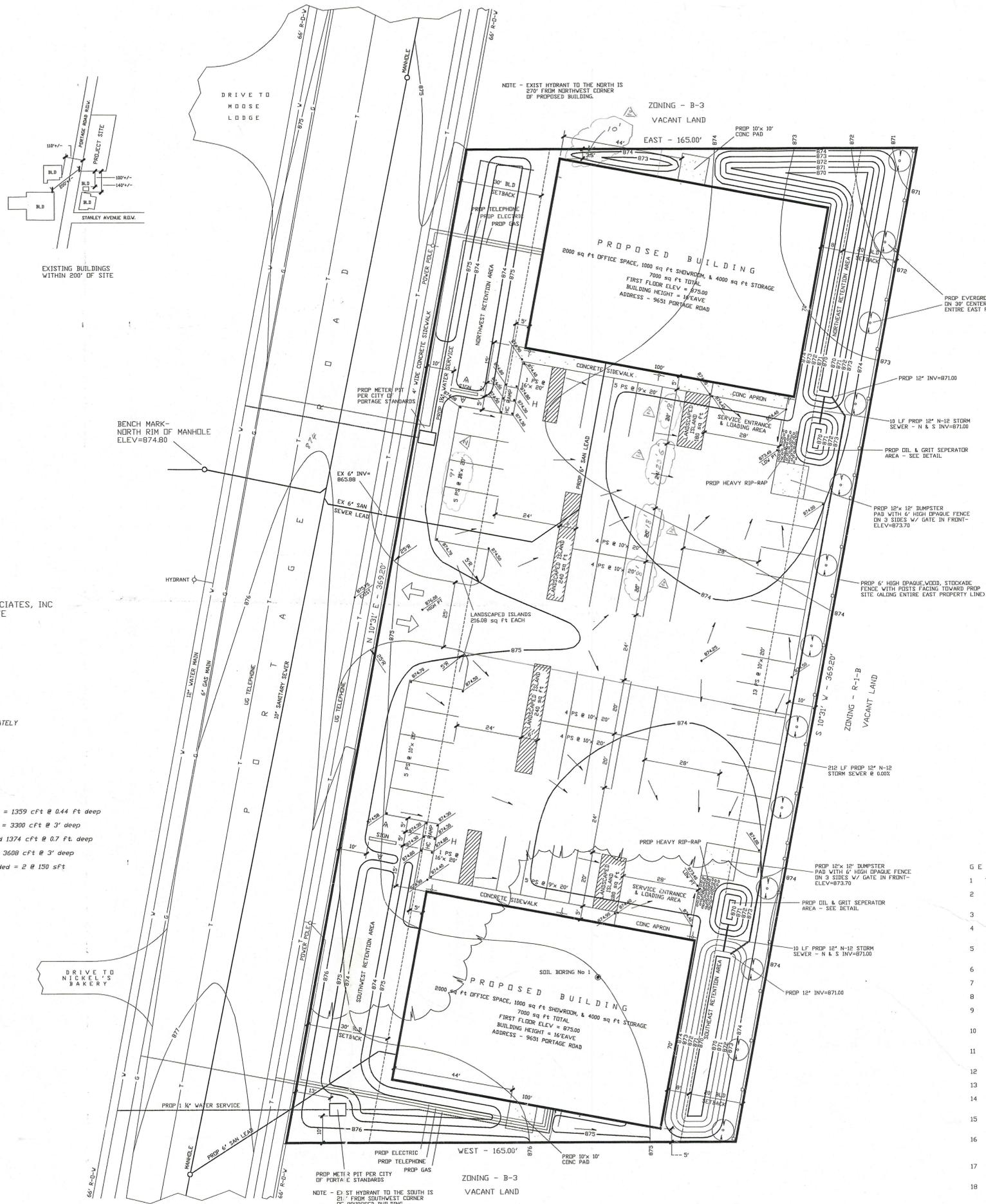
LANDSCAPED ISLAND AREA PROVIDED = 1,272.16 sq ft

PARKING REQUIREMENTS-

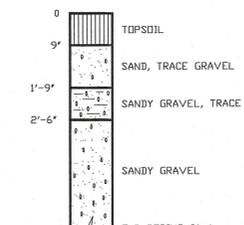
2,000 sq ft (SHOWROOM) - 1 SPACE/800 sq ft = 3 SPACES  
4,000 sq ft (OFFICE) - 1 SPACE/150 sq ft = 27 SPACES  
8,000 sq ft (STORAGE) - 0 SPACES  
TOTAL = 30 SPACES

PARKING PROVIDED-

STANDARD SPACES (9'x20')(10'x20') = 49 SPACES  
HANDICAPPED SPACES (16'x20') = 2 SPACES  
TOTAL = 51 SPACES

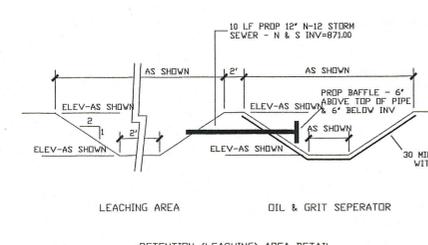
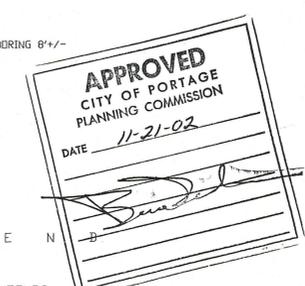


SOIL BORING No 1



LEGEND

- 876 PROPOSED CONTOURS
- 876 EXISTING CONTOURS
- PROPOSED SPOT ELEVATIONS
- PROPOSED SURFACE DRAINAGE
- PROPOSED SECURITY LIGHTING
- PROPOSED FLOODLIGHTING (SHARP CUT-OFF)



GENERAL NOTES -

1. TYPE OF BUILDING CONSTRUCTION IS *see 3B mod*
2. SITE LIGHTING SHALL NOT GLARE DIRECTLY ONTO PUBLIC RIGHT-OF-WAY OR ADJACENT PROPERTIES AND SHALL NOT EXCEED 0.1 FOOT CANDLES AT ABUTTING RESIDENTIALLY ZONED/USED PROPERTIES.
3. THE PROPOSED PORTAGE ROAD DRIVE WILL SERVE AS THE ONLY ACCESS FOR THE ENTIRE 1.4 ACRE PARCEL.
4. ANY SIDEWALK DAMAGED DURING THE DEVELOPMENT OF THE SITE SHALL BE REPAIRED PURSUANT TO THE CITY OF PORTAGE SPECIFICATIONS.
5. PREMIUM JOINTS SHALL BE USED FOR SOLID WALL PIPE, AND RESILIENT CONNECTORS SHALL BE USED AT CONNECTIONS TO MANHOLES AND CATCH BASINS.
6. SITE ZONING IS B-3.
7. MAINTAIN 10' GREENSPACE ALONG ENTIRE EAST & WEST PROPERTY LINES.
8. PROPOSED SANITARY MANHOLE PER CITY OF PORTAGE STANDARDS.
9. THE SIDE WALLS OF THE PROPOSED BUILDINGS SHALL NOT CONTAIN ANY WINDOWS OR OTHER OPENINGS.
10. A VEGETATIVE SCREEN (MIN 6 FEET IN HEIGHT) SHALL BE PLANTED WITHIN EACH OF THE LANDSCAPE AREAS SITUATED ALONG THE WEST SIDES OF THE TWO DESIGNATED LOADING/UNLOADING AREAS.
11. THE PROPOSED ACCESS FROM PORTAGE ROAD SHALL BE CONSTRUCTED PURSUANT TO CITY OF PORTAGE STANDARD.
12. INGRESS LANE OFF OF PORTAGE ROAD SHALL BE STRIPED 14 FEET WIDE, & EGRESS LANE 11' WIDE.
13. INSTALL HANDICAPPED PARKING STALLS & SIGNS PER CITY OF PORTAGE STANDARD SPECIFICATIONS.
14. ALL PAVEMENT MARKINGS MUST CONFORM TO THE STANDARDS SET FORTH IN THE CURRENT EDITION OF THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
15. PLACE 8\"/>

STATE OF MICHIGAN  
MONTELL STERNMAN  
ENGINEER  
No. 43818  
PROFESSIONAL ENGINEER

DATE: 9-4-02  
1. PER CITY OF PORTAGE: 7-22-03  
2. *Corrections SETBACK*

GORDON JONES ASSOC., INC.  
CONSULTING ENGINEER  
KALAMAZOO, MICHIGAN

SCALE: DRAWN BY: EGADY DATE: 6-06-02  
CHECKED BY: DATE:

PRECISION BUILDERS

PROPOSED SITE PLAN

1 OF 1

SPECIFICATIONS

Division A - Special Conditions and Bidding

- 1. All Bids shall be on the basis of a guaranteed sum. The subcontractors shall visit the site to be fully aware of all existing conditions prior to submitting a bid.
2. It is the General Contractor's responsibility to coordinate and review the subcontractor's bids to assure the Owner that all items herein specified or shown on the drawings, unless indicated as "not included in Contract" (NIC), are included in the guaranteed total.
3. All permits, fees, and taxes shall be included in the Contractor's proposals.
4. Contractors shall be required to provide their own temporary facilities for tools and equipment.
5. The Contractor shall be required to pay for all temporary utilities and temporary heat as may be required to complete the work.
6. The Contractor and his Subs shall be required to carry Contractor's Liability and Property Damage Insurance and Contingent and protections Insurance in amounts sufficient to protect the Owner and himself against any and all claims arising from operations in connection with this project.
7. Owner shall provide standard Builder's Risk Insurance.
8. All work shall conform to current State and local Codes and Ordinances, and shall be performed by experienced tradesmen using the highest quality accepted standard construction practices, in order to provide the Owner with a structurally sound, weather-tight building.
9. All construction shall conform to all MIOSHA rules for the construction process.

Division 1 - General Requirements

- 1. The General Requirements for the work shall be A.I.A. Documents A201 "General Conditions of the Contract for Construction". A copy can be obtained from Architecture One, Inc. or Cornerstone C.M.
2. Any and all changes to the Contract shall be made in writing only. Verbal change orders from either party will not be authorized unless followed by written verification.
3. Substitutions will be permitted only after receiving written consent from Architecture One, Inc., or Cornerstone C.M.
4. The Contractors shall maintain the job site in a clean and orderly manner at all times, and the Owner reserves the right to clean up the job and deduct the cost from the Contractor if he fails to keep the job site clean.
5. The General Contractor shall maintain his work in good condition and shall repair, at his own expense, any work or materials which prove to be defective within one (1) year from the date of final acceptance by Owner, not necessarily from the date of installation.
6. It shall be the responsibility of the General Contractor to convey to the Subcontractors the requirements contained herein.

Division 2 - Sitework

- 1. The Contractor shall be responsible for the final siting and grading of the building, by staking out to verify all dimensions and elevations shown on the site plan, and after consulting with the Architect in the field.
2. All footings to rest on undisturbed soils or engineered fill at 95% compaction. All slabs to rest on 95% minimum compacted sand fill. Provide compaction tests for all areas required compaction.
3. Site topsoil is to be stripped and stockpiled for later re-spreading by the Contractor.
4. Any trees or shrubs to be removed shall be removed by this Contractor and legally disposed of off the site.
5. No building materials shall be used as backfill, and nowhere on the site shall any building materials be buried for disposal.
6. Additional fill, if needed, is the responsibility of the Sub-contractor. An allowance or guaranteed maximum unit cost for extra fill and cartage shall be included in the Subcontractor's proposal.
7. The Subcontractor is responsible for the rough grading and re-spreading of topsoil at a maximum of 4" over the entire disturbed areas. Provide new topsoil as required to meet a 4" DEPTH REQUIREMENT.
8. Provide minimum 6" thick stabilized gravel at drive and parking areas, graded within 1" of final grades.
9. Install subsurface drainage system as indicated on the site plan.
10. Provide all exterior concrete walks, aprons, stoops, and patios as shown, using 3,000 psi, air-entrained limestone mix.
11. Final landscaping to be by Owner, or others, and not part of this Contract.
12. Seed blend
a) Sunny lawn seed mix shall be 10% Perennial Rye Grass, 40% Kentucky Blue Grass, 50% Creeping Red Fescue. No Marion Bluegrass will be allowed in mix. Submit seed vendor's certified statement for each grass seed mixture required, stating botanical and common names, percentage by weight, and percentage of purity, germination, and weed seed for each species.
b) Straw Mutch: Use clean, dry straw mutch for newly seeded lawn areas, or an approved equal method.
c) Fertilizer: Granular, non-burning product composed of not less than 50 % organic, slow-acting, guaranteed analysis, professional fertilizer.
d) Water: Free of substance harmful to seed growth. Hoses or other methods of transportation furnished by Contractor.
13. Provide termite treatment as performed by Michigan Terminix or equal, conforming to all state and EPA requirements for use and application. Certification of Treatment and five-year Guarantee shall be forwarded to the Owner.

Division 3 - Concrete

- 1. All concrete shall be at least 3,000 psi strength (after 28 days). All concrete shall comply with ACI 318, building code requirements for reinforced concrete, ACI 301, ACI 347, ACI 306, and ASTM C150.
2. Reinforce all footings as indicated, or as required by Code.
3. Reinforcing steel shall conform to ASTM 615 Grade 60 deformed bars. Welded wire fabric: ASTM A185.
4. The concrete Contractor shall be responsible for coordination with General Contractor for placing of all under floor items and sleeves, including those required for telephone, plumbing, electrical, and mechanical trades.
5. Provide saw-cut control joints where shown, and use keyed joints between non-continuous pours.
6. All footings shall extend a minimum of 42: below final grades and as shown on the drawings.
7. Provide concrete pads for all mechanical and electrical pad mounted equipment.
8. Unless noted otherwise on drawings, anchor bolts shall be placed a maximum of 6'-0" apart, 1/2"x15" long minimum size. (1/2"x8" long is acceptable at concrete walls, except where there are brick ledges or recessed slab areas.) Where the top of wall is less than 8" wide, anchor bolts shall extend a minimum of 6" into the 8" or larger width below. All bolts to be grouted-in solid.
9. Gout: Use non-shrink, non-stain type, 6,000 psi (28-day); ASTM C109.
10. Floor Finishes: Interior: trowled, per ACI 301, 11.7.3. Exterior: broom, per ACI 301, 11.7.4.

Division 4 - Masonry

- 1. Masonry Contractor to be responsible for all miscellaneous galvanized wall lintels, flashings, cut stone sills, etc..
2. Block Units shall be ASTM C90, grade N, Type-1 concrete block units, load-bearing. Note: All exposed corners shall be bullnose block.
3. Mortar shall be Type N (ASTM C270).
4. Grout shall be ASTM C409.
5. Provide control joints where shown or required by common practice.
6. Contractor shall grout fill all blocks under lintel bearing and for 1'-6" each side.

Division 5 - Metals

- 1. Any structural steel, lintels, etc., shall conform to specifications of ASTM for A-36 steel, and shall be shop coated.
2. High-strength bolts: ASTM A325; Structural steel tubing: ASTM A500, Grade C; Machine bolts: ASTM A307 for bolts set in concrete.
3. Fabricated structural members in accordance with AISC - Specification for Design, Fabrication, and Erection of Structural Steel Buildings.

Division 6 - Carpentry

- 1. Standards: American Institute of Timber Construction Standard 104 "Typical Construction Details"; American Plywood Association "Design/Construction Guide - Residential and Commercial"; National Forest Products Association "National Design Specifications for Wood Construction".
2. All foundation plates, sills, and sleepers shall be of AWPA rated treated wood, and shall be securely fastened to the foundations by tightened anchor bolts as described above. In addition, for any wood resting on concrete slabs or block, the bottom plate shall be treated wood.
3. All wood structural framing (2" to 4" thick, 5" and wider) shall be Spruce-Pine-Fir Construction grade or better. Nonbearing studs may be Hem-Fir, stud grade.
4. Parallel strand lumber (PSL): ASTM D 2559; fb = 2900 psi for 12 inch depth members; e = 2,000,000 psi.
5. Laminated veneer lumber (LVL): ASTM D 2559; Fb = 2900 psi for 12 inch depth members; E = 2,000,000 psi.
6. Roof sheathing shall be 5/8" exterior grade plywood, installed in accordance with A.P.A. recommendations. OSB sheeting (5/8") may be substituted.
7. Firestop all walls at top with 2" thick (nominal) solid wood, and at all dropped bulkheads or soffits, to prevent spread of fire to adjacent concealed spaces. Holes cut for passage of mechanical or electrical components shall be sealed tight with rockwool or fireproof sealant made specifically for such use.
8. Refer to drawings for special draft stopping conditions and provide blocking as required for such.

Division 6A - Finish Carpentry, Interior Doors and Hardware

- 1. Interior Doors:
a) Where indicated as wood, provide 5-ply flush, solid core type, 1 3/4" thick, of surface veneer and finish as selected by Owner.
b) Provide label doors and frames where indicated or required by code.
2. Hardware: Refer to schedule attached to end of this specification.
3. Any miscellaneous interior trim to be Custom Grade Poplar, kiln-dried, if to be painted or Premium Grade plain sawn oak wood if stained.
4. Exposed millwork to be painted to be constructed of AOA MD Overlay, Group I Exterior type. Stained casework to use Type III, interior, moisture resistant 5-ply softwood, species on drawings.
5. Provide custom plastic laminate faced cabinetry where indicated on the drawings. Wherever possible, provide 1000 Series semi-custom units as manufactured by Handom, Grand Rapids
a) Units to have exterior high pressure laminate faces over 5/8" 45# density particleboard, melamine interiors, plain door design, with PVC solid plastic edges on doors and drawers, metal wire pulls.
6. Casework Hardware: Epo 402 Series door and drawer pulls; Hafele Duomatch 329 concealed cabinet hinges; SOSS invisible hinges at mortised concealed hinges; Accuride 3837 full extension drawer glides; Timberline Supply Co. door/drawer locks.
7. All plastic laminated countertops are to be of 3/4" thick exterior grade plywood or industrial grade particle board. Plastic laminate to be by Formica, Pionite, Nevamar, WilsonArt or equal. Colors by Owner.

Division 7 - Thermal & Moisture Protection

- 1. Perimeter insulation is to be 2" thick Styrofoam brand rigid insulation or equal. Extend full height at interior side of foundation wall with no gaps from top of floor slab. Install prior to backfill, at perimeter of conditioned space.
2. Trussed ceiling spaces to receive R-48 fiberglass insulation.
3. Provide 6 mil Visqueen below all slabs in conditioned spaces and 4 mil Visqueen at all 2nd floor ceilings.
4. Furnish and install storm-proof metal louvers and grilles where shown: Airoilite Company or equal, Kynar coating, color by Owner. Include bird screen and insect screen, blank off panels as required.
13. Sealants
a) Plumbing fixtures, top and edges of backsplashes at countertops, and other areas requiring NSF or USDA approvals: One-part silicone; Dow Corning #786 Mildew Resistant or equal.
b) Exterior and interior vertical expansion and control joints, exterior and interior door and window frames, exterior joints in and adjacent to EIFS system: Two-part, non-sag polyurethane sealant; Sonneborn Sonolastic NP-2 or equal.
c) Perimeter and penetrations in sound rated partitions, unexposed: Acoustical sealant; Tremco or equal.
d) Non-working cosmetic joints between similar and dissimilar materials: Acrylic latex caulk; Bostick Chem-Calk 600 or equal, or Butyl caulk; Tremco Butyl Sealant or equal.
e) Apply sealants according to manufacturer's recommendations for exposure and conditions present. Include backers, primers, and bond breakers where required for optimum performance.

Division 8 - Exterior Doors, Windows and Glazing

- 1. Metal Doors and Frames:
a) Metal doors and frames will be standard steel type, complying with the Steel Door Institute's "Recommended Specifications, Standard Steel Doors and Frames", and as specified herein. Doors and frames made by Ceco, Pioneer, Republic, Steelcrat, or Fenestra are acceptable.
b) Doors to be Grade II - Heavy Duty type, with cutouts for glass lights as shown on plans, fabricated as per SDI 100. Frames will be 16 gauge, welded type with smooth sanded joints, prime painted. Throat dimension to match thickness of walls. Provide and install fire rated doors and frames where indicated, complete with affixed UL labels. Install all frames using at least 3 wall anchors per jamb, level and true. Pack all frames with fiberglass or foam insulation before drywall installation.
c) Provide all hardware cutouts, reinforcing, edge channels, etc., as required for a complete job. Shop apply rust-inhibitive prime coat compatible with field finishing requirements to all metal surfaces after cutting, welding and prep is complete.
d) Provide shop drawings for approval prior to fabrication.
2. Entry Doors, Frames, Sidelights and Vestibule Doors: Frames and Sidelights shall be similar to Tubelite Monumental Doors and Frames. Frames to be 2"x4 1/2. Color to match existing building. Manufacturer is to provide and install all hardware, including thresholds, hinges, locks, push-pulls, closers, frames. Doors shall have 1" insulated, solar-gray, glass panels.
4. Glazing:
a) Thermal insulating units at all exterior openings: 1" thick, solar-gray, low E type. Tempered where required by code. Minimum 10-year warranty.
b) Clear glass: 1/4" float minimum: Kind HT, Type I, Class 3, Quality Q3; tempered where required by code.
c) Wire glass: 1/4" thickness; diamond or misco mesh, Type II, Class I, Form I, Quality Q8; UL listed.
d) Install all glazing in accordance with GANA, Glazing Manual, requirements, all applicable standards, and manufacturer's instructions.

Division 9 - Finishes

- 1. Gypsum Board Assemblies:
a) All walls and ceilings to be U.S.G. 5/8" wallboard system with metal corner beads and machine taping of joints, unless noted otherwise.
b) Use fire code type wallboard where required by referenced fire rated assembly. Installed as described in referenced fire assembly.
c) Metal studs, when used, shall be ASTM C 645, 20 gauge minimum, 3-5/8" deep. Installed at 16" o.c.
d) Walls and ceilings to be screwed, and both to be taped and sanded smooth, unless indicated otherwise on the drawings.
e) Use water-resistant wallboard at all wet areas, toilet rooms.
f) Install drywall draftstops in attic as described on the drawings. Include self-closing door panels for access to all adjacent attic areas, unless noted otherwise on drawings.
2. Painting and Wallcovering:
a) In general, painting to be a three coat job: (1) coat primer, sprayed/rolled and (2) finish coats paint. Acceptable manufacturers include Pratt & Lambert, Inc., Pittsburgh Paint, O'Brien and Sherwin/Williams. Color by Owner.
b) Any visible roof or wall projections, vents, stack, louvers, etc., on the outside of the building to be painted.
c) Include allowance of \$10.0 per square yard for wallcovering material only.
d) Wallcovering to have Class A rating: Flame spread 0-25; smoke developed 0-450; 54 inches wide.
e) Any drywall to receive wallcovering to be sealed with wall sizing or one (1) coat of oil based primer.
3. Lay-in acoustical ceilings where indicated to be Armstrong, Fissured, Tegular edge, 24"x 48 or 24" x 24", square edges, 5/8" thick, with standard grid. Verify with Owner the type of tile.

Division 10 - Specialties

- 1. Toilet Accessories: Provide the following toilet accessories where indicated (All numbers are Bobrick unless otherwise noted. Verify all mounting heights and locations with Owner.)
a) Paper Towel Dispenser: Install (1) by each sink and waste receptacle.
b) Toilet Paper Holder: Standard, surface type, polished chrome, B-667.
c) Soap Dispensers: B-155. Install (1) in each Toilet Room.
d) Mirrors: B-165 Series. See elevations for mirrors at Toilet Rooms.
e) Grab Bars: Standard, B-6206 Series, stainless steel, as shown on plans.
f) Exposed pipe wrap: Brocar Trap Wrap with flip top, standard white.
2. Signage: Provide A.D.A. approved sign at toilet room doors and stairway doors.
3. Fire Extinguishers:

Division 11 - Equipment

- 1. Residential Appliances: Microwave and Refrigerator with Icemaker to be provided by Owner. Attach icemaker to water source at refrigerator in kitchen.

Division 12 - Furnishings

Hardware Schedule

Acceptable Manufacturers:

- Hinges: McKinney, Hager, Stanley
Locksets: Sargent, Schlage, Falcon
Exit Devices: Sargent, Von Duprin, Precision
Closers: Sargent, LCN, Corbin, Norton
Wall Stop, Kick Plates: Rockwood, Ives, Glynn, Johnson
Thresholds, w/Strip, Sweeps: National Guard, Pemko, Reese
Automatic Operators: Dor-O-Matic, Horton, LCN

\\Czandria c\A-Project-02102011\precibid\sp-1.dwg, 3/26/2003 8:24:40 AM, Copyrighted architecture one, Copyrighted architecture one

ARCHITECTURE ONE, INC.
450 W. CENTRE
PORTAGE, MICHIGAN 49024
PHONE: 269.329.1575
FAX: 269.329.1580
EMAIL: RCARCH1@ISERV.NET



NEW BUILDING FOR
PRECISION BUILDERS
9651 PORTAGE ROAD
PORTAGE MICHIGAN

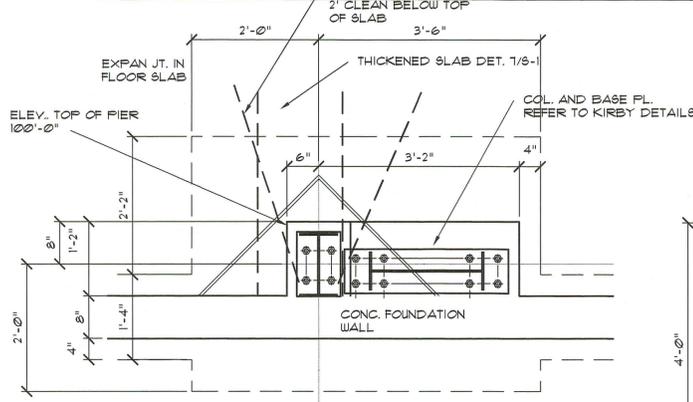
DATE
2-22-02
REVISIONS
FINAL CONST.
SET
3/25/03

SHEET TITLE

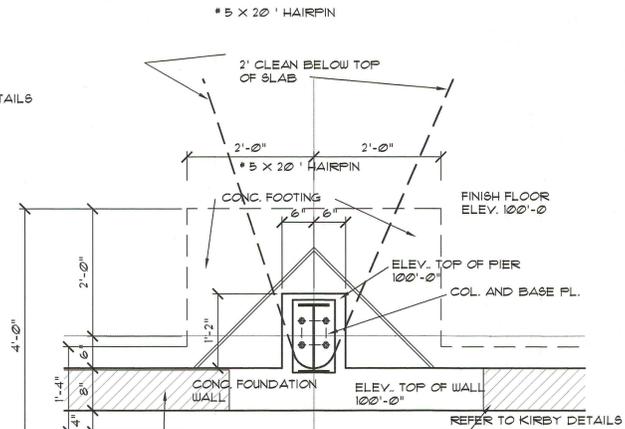
PROJECT NUMBER
02011
SHEET NUMBER

SP-1

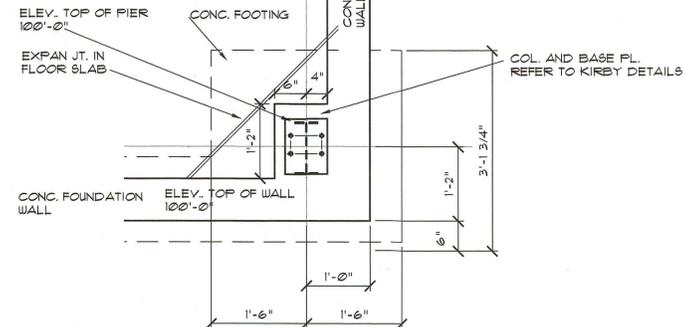
MARK	DIMENSION L (FT) X W (FT) X D (IN) TOP	REINFORCEMENT		PIER MARK	ELEVATION TOP OF		REMARKS
		TOP	BOTTOM		PIER (ETP)	FOOTING (ETF)	
F-2	2'-0" X 2'-0" X 12"	-	3 - #4 EW		100'-0"	99'-6"	
F-3	3'-0" X 3'-0" X 12"	-	4 - #5 EW		100'-0"	97'-0"	
F-4	4'-0" X 4'-0" X 12"	-	5 - #5 EW		100'-0"	97'-0"	
F-5	5'-6" X 4'-0" X 12"	-	5 - #5 EW		100'-0"	97'-0"	



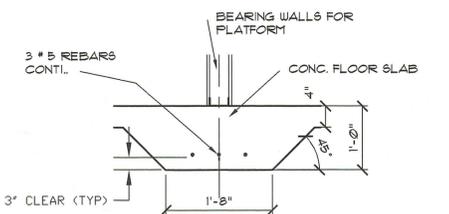
**8 FOOTING DETAIL**  
Scale 3/4" = 1'-0"



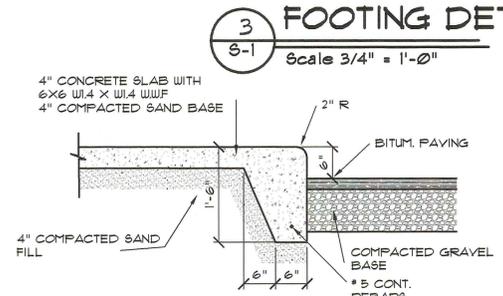
**2 FOOTING DETAIL**  
Scale 3/4" = 1'-0"



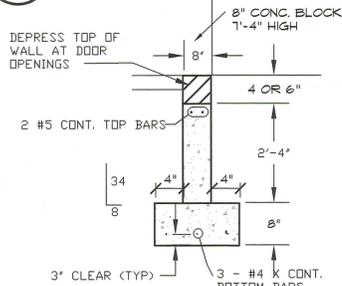
**1 FOOTING DETAIL**  
Scale 3/4" = 1'-0"



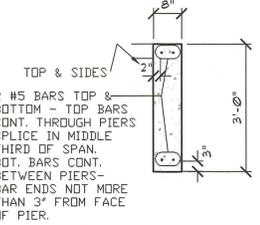
**7 THICKENED SLAB DET.**  
Scale 3/4" = 1'-0"



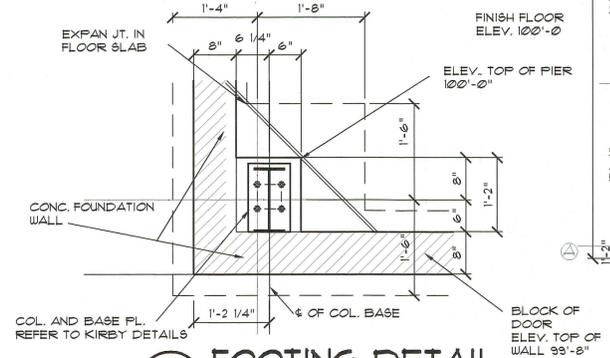
**10 CURB DETAIL**  
Scale 3/4" = 1'-0"



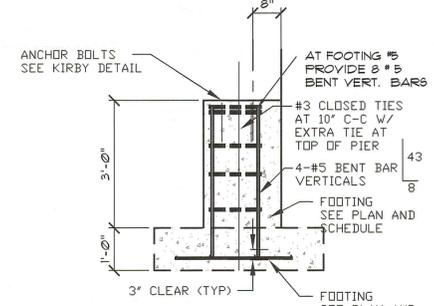
**5 DETAIL**  
Scale 1/2" = 1'-0"



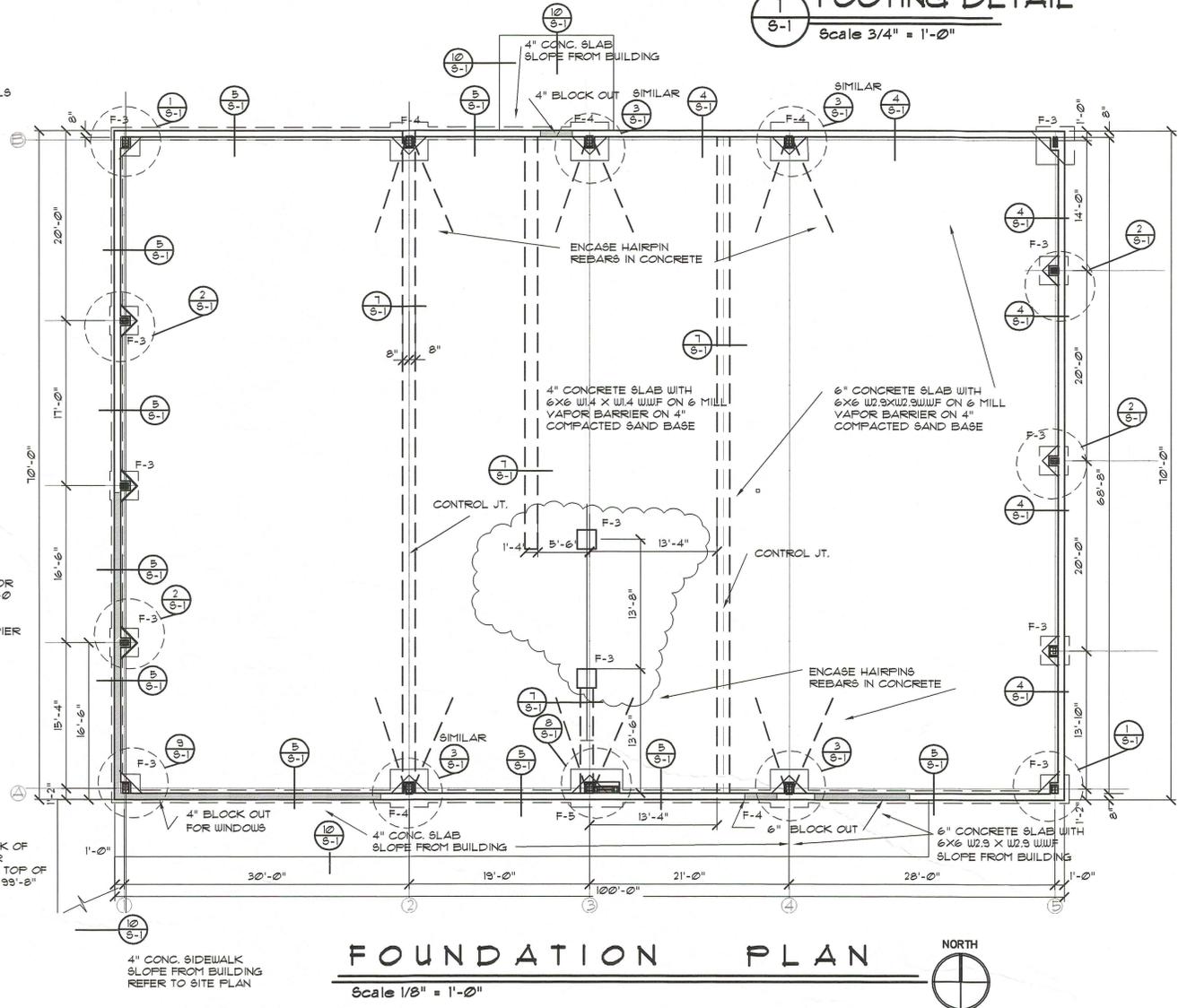
**4 DETAIL**  
Scale 1/2" = 1'-0"



**9 FOOTING DETAIL**  
Scale 3/4" = 1'-0"



**6 DETAIL**  
Scale 3/4" = 1'-0"



**FOUNDATION PLAN**  
Scale 1/8" = 1'-0"

**FOUNDATION NOTES:**  
 1. DESIGN LOADS FOR THE MANUFACTURED BUILDING PORTION OF THE STRUCTURE ARE BASED ON DESIGN INFORMATION PROVIDED BY KIRBY BUILDING SYSTEMS, AN ABS COMPANY AS INDICATED ON THEIR DRAWING DATED 2-17-03 JOB # 529310.  
 2. DESIGN LOADS FOR EQUIPMENT FOUNDATIONS ARE BASED ON INFORMATION PROVIDED BY THE MANUFACTURERS. REFERENCE DOCUMENTS ARE (NOT AVAILABLE AT THIS TIME).  
 3. FOUNDATION DESIGN BASED ON 3000 PSF ALLOWABLE NET SOIL BEARING CAPACITY. TESTING AND WRITTEN CONFIRMATION OF SOIL BEARING CAPACITY AT FOOTING DEPTH BY A QUALIFIED GEOTECHNICAL ENGINEER IS RECOMMENDED. COPY PROJECT ARCHITECT WITH ALL DOCUMENTATION.

4. REMOVE ALL EXISTING TOPSOIL AND ORGANIC MATERIAL WITHIN THE CONSTRUCTION AREA OF THE FOOTINGS AND CONCRETE SLABS.  
 5. CONTRACTOR SHALL ARRANGE FOR OBSERVATION OF THE SUBGRADE PREPARATION AND RECOMMENDATIONS REGARDING ADDITIONAL SOIL REMOVAL BY A QUALIFIED, LICENSED GEOTECHNICAL ENGINEER TO INSURE ADEQUATE BEARING CAPACITY OF THE FOUNDATION AND ANY FILL PLACED WITHIN THE STRUCTURE LIMITS. COPY ARCHITECT WITH ALL REPORTS.  
 6. ALL CONCRETE WORK SHALL BE IN ACCORDANCE WITH THE SPECIFICATION OF STRUCTURAL CONCRETE FOR BUILDINGS, ACI 301.

7. CONCRETE SHALL BE NORMAL WEIGHT, ASTM C33 AGGREGATE, TYPE 1 PORTLAND CEMENT 4 INCH SLUMP WITH 28 DAY CYLINDER COMPRESSIVE STRENGTHS AS FOLLOWS:  
 FOOTINGS / FOUNDATION WALLS / GRADE BEAMS / PIERS - 3500 PSI  
 FLATWORK - INTERIOR - 4000 PSI  
 - EXTERIOR - 4000 PSI W/ 6% +/- 1% AIR  
 NO ADMIXTURES TO BE INCLUDED WITHOUT PRIOR WRITTEN AUTHORIZATION OF THE ENGINEER.  
 8. MILD STEEL REINFORCEMENT:  
 DEFORMED BARS - ASTM A615, GR 60  
 9. PROVIDE BENT BAR DOVELS OR HOOKED BARS AT ALL FOOTING TO FOUNDATION WALL INTERSECTIONS AT SPACING INDICATED.  
 10. PROVIDE 24 INCH BY 24 INCH CORNER BARS AT ALL FOOTING AND FOUNDATION WALL INTERSECTIONS, MATCHING SIZE AND SPACING OF ALL CONTINUOUS HORIZONTAL REINFORCING.

ARCHITECTURE ONE, INC.  
 450 W. CENTRE  
 PORTAGE, MICHIGAN 49024  
 PHONE: 269.329.1575  
 FAX: 269.329.1580  
 EMAIL: RCARCH1@ISERV.NET

NEW BUILDING FOR  
**PRECISION BUILDERS**  
 9651 PORTAGE ROAD  
 PORTAGE MICHIGAN

DATE  
**2-22-02**  
 REVISIONS  
**FINAL CONST.**  
**SET**  
**04/18/03**  
 Revision  
**4-18-03**

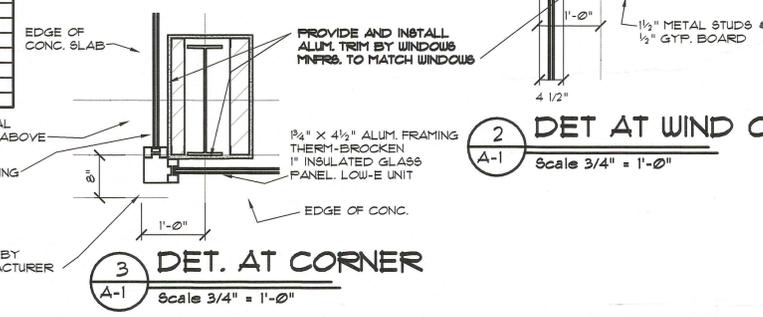
SHEET TITLE  
 PROJECT NUMBER  
**02011**  
 SHEET NUMBER

**S-1**

Zcandra c:\A-Project-02011\p\cbidns-1.dwg, 4/17/2003 7:17:52 PM, Copyrighted architecture one. Copyrighted architecture one

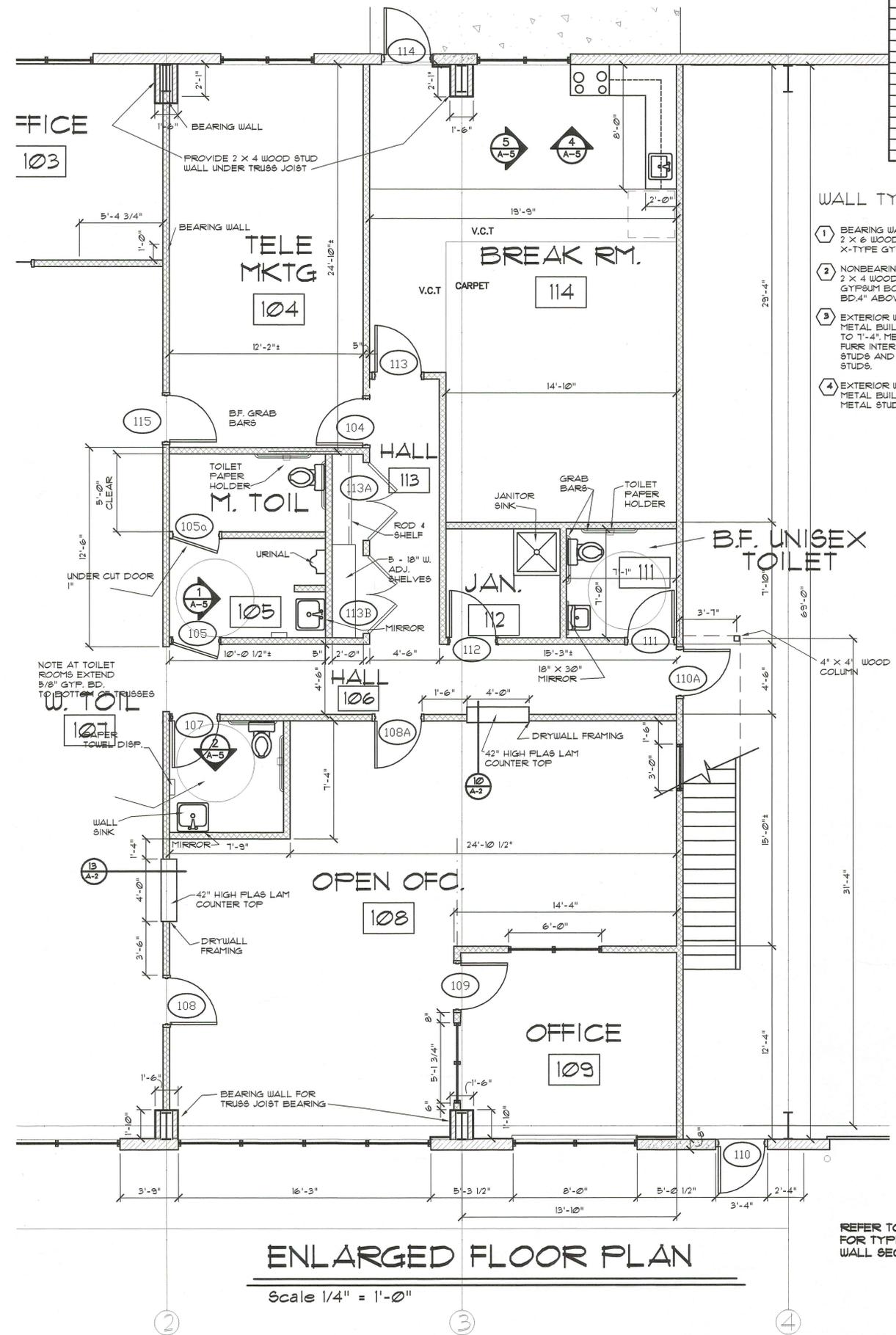
ROOM FINISH SCHEDULE									
NO.	ROOM NAME	FLOOR	BASE	WALLS			CEILING		REMARKS
				N	E	S	W	FINISH	
100	SHOW ROOM	VCT/CAR	4V	P	P	P	P	AC	15'
101	OFFICE	CARPET	4V	P	P	P	P	AC	10'
102	OFFICE	CARPET	4V	P	P	P	P	AC	10'
103	OFFICE	CARPET	4V	P	P	P	P	AC	8'
104	TELE MKTG	CARPET	4V	P	P	P	P	AC	8'
105	TOILET	V.C.T	6V	P	P	P	P	AC	8'
106	HALL	VCT	4V	P	P	P	P	AC	8'
107	TOILET	V.C.T	6V	P	P	P	P	AC	8'
108	OFFICE	CARPET	4V	P	P	P	P	AC	8'
109	OFFICE	CARPET	4V	P	P	P	P	AC	8'
110	STORAGE	CONC.	-	-	-	-	-	-	-
111	TOILET	V.C.T	6V	P	P	P	P	AC	8'
112	JAN	V.C.T	6V	P	P	P	P	AC	8'
113	HALL	VCT	4V	P	P	P	P	AC	8'
115	BREAK RM.	CAR/VCT	4V	P	P	P	P	AC	8'

LEGEND  
 WD = WOOD  
 V.F. = VINYL WALL FABRIC  
 V. = 4" VINYL BASE  
 P = PAINTED SURFACES  
 CAR = CARPET  
 C.T. = CERAMIC TILE  
 S.C. = SEALED CONCRETE SLAB  
 1 - 12" X 12" CERAMIC TILE REFER TO SPEC. 9310

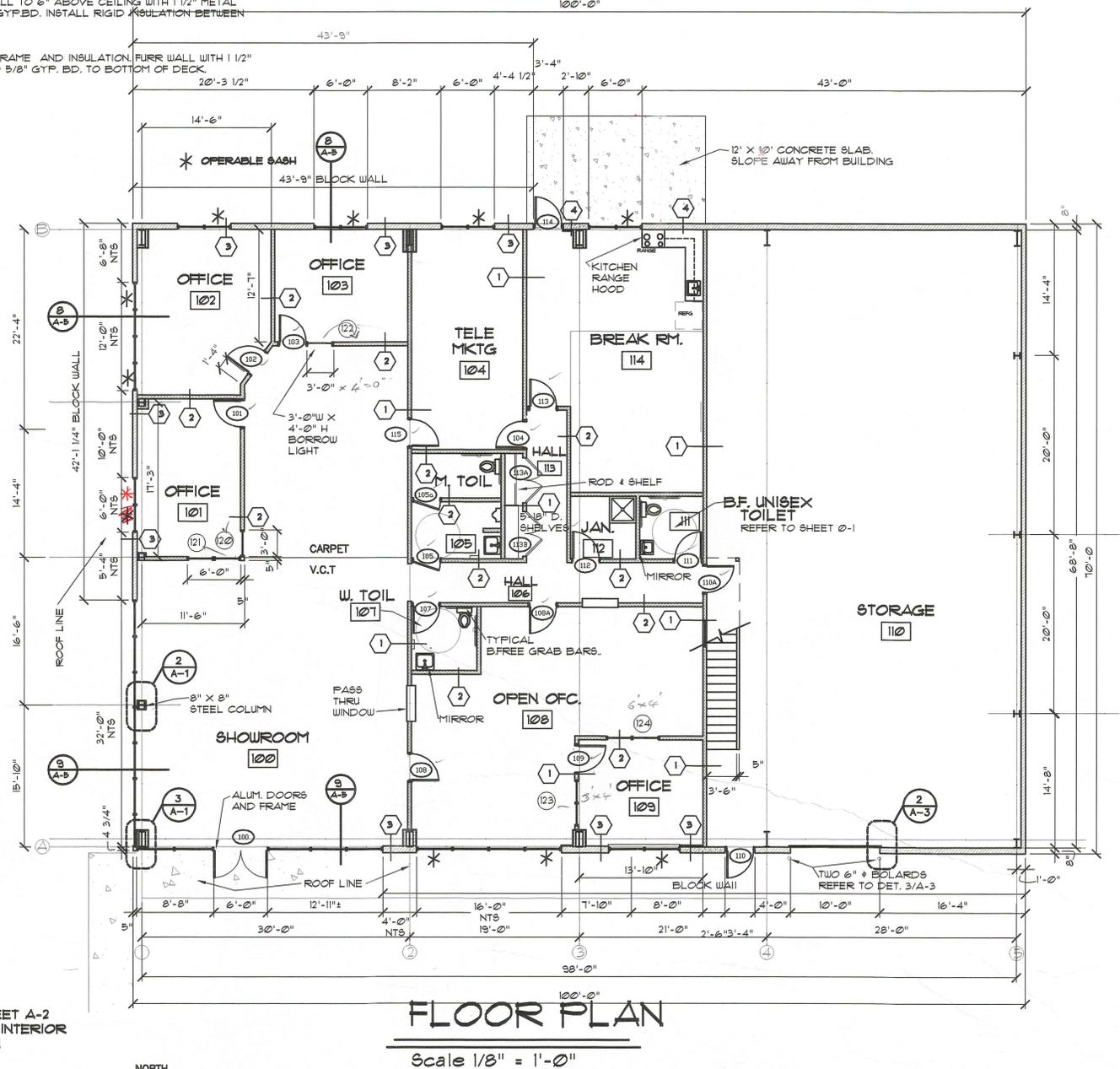


WALL TYPES

- BEARING WALL FOR 2ND FLOOR  
2 X 6 WOOD STUDS @ 16" O.C. WITH 5/8" X-TYPE GYPSUM BOARD EACH SIDE
- NONBEARING INTERIOR WALL  
2 X 4 WOOD STUDS AT 16" O.C. WITH 5/8" GYPSUM BOARD EACH SIDE. STOP GYP. 8D.4" ABOVE CEILING FOR RETURN AIR FLENUM.
- EXTERIOR WALL  
METAL BUILDING FRAME, 8" SPLIT FACE BLOCK UP TO 1'-4". METAL BUILDING SIDING AND 6" BUILDING INSULATION FURR INTERIOR WALL TO 6" ABOVE CEILING WITH 1/2" METAL STUDS AND 5/8" GYP. BD. INSTALL RIGID INSULATION BETWEEN STUDS.
- EXTERIOR WALL  
METAL BUILDING FRAME AND INSULATION. FURR WALL WITH 1/2" METAL STUDS AND 5/8" GYP. BD. TO BOTTOM OF DECK.



ENLARGED FLOOR PLAN  
 Scale 1/4" = 1'-0"



FLOOR PLAN  
 Scale 1/8" = 1'-0"

ARCHITECTURE ONE, INC.  
 450 W. CENTRE  
 PORTAGE, MICHIGAN 49024  
 PHONE: 269.329.11575  
 FAX: 269.329.11580  
 EMAIL: RCARCH1@ISERV.NET

NEW BUILDING FOR  
**PRECISION BUILDERS**  
 9651 PORTAGE ROAD  
 PORTAGE MICHIGAN

DATE  
 2-22-02  
 REVISIONS  
 FINAL CONST.  
 SET  
 04/18/03  
 Revision  
 4-18-03

SHEET TITLE  
 PROJECT NUMBER  
 02011  
 SHEET NUMBER

A-1

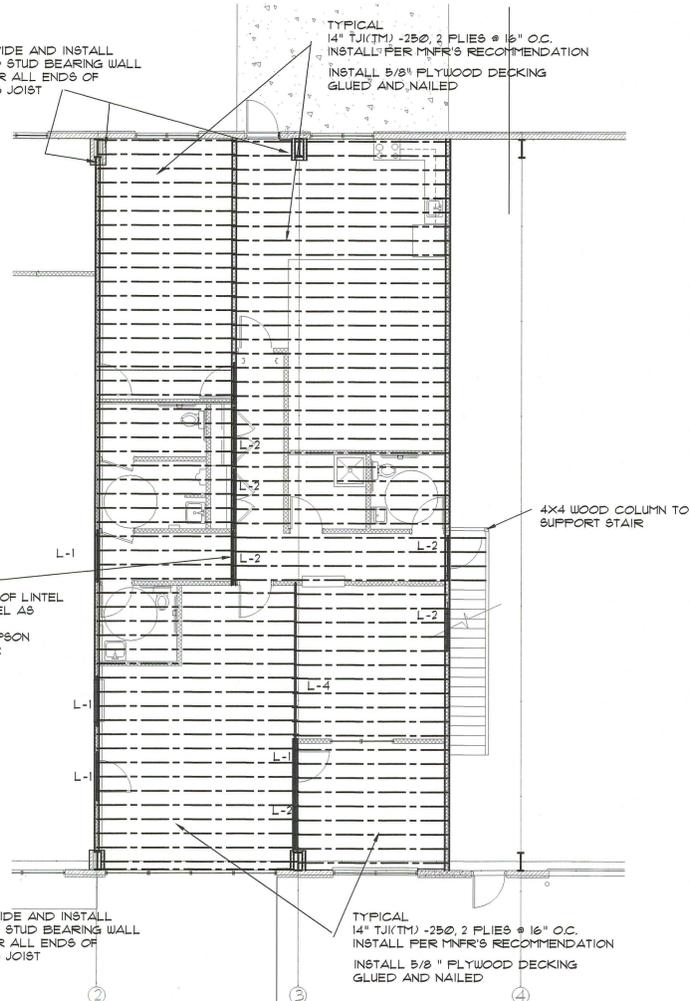
U:\zandura c\A\Project\0202011\precidba-A-1 floor plan.dwg, 4/17/2003 7:25:47 PM, Copyrighted architecture one, Copyrighted architecture one

LINTEL SCHEDULE		
L-1	3-2 X	MIN 4" BEARING EACH END
L-2	3 1/2" X 14" GLUE LAM BEAM	MIN 4" BEARING EACH END
L-3	5/4" X 14" GLUE LAM BEAM	MIN 4" BEARING EACH END
L-4	1" X 18" GLUE LAM BEAM	MIN 4" BEARING EACH END

NOTE MINIMUM END BEARING 4" TYPICAL.  
BOTTOM OF L-2 AND L-3 TO BE AT SAME ELEVATION AS TJI JOIST.

PROVIDE AND INSTALL WOOD STUD BEARING WALL UNDER ALL ENDS OF TRUSS JOIST

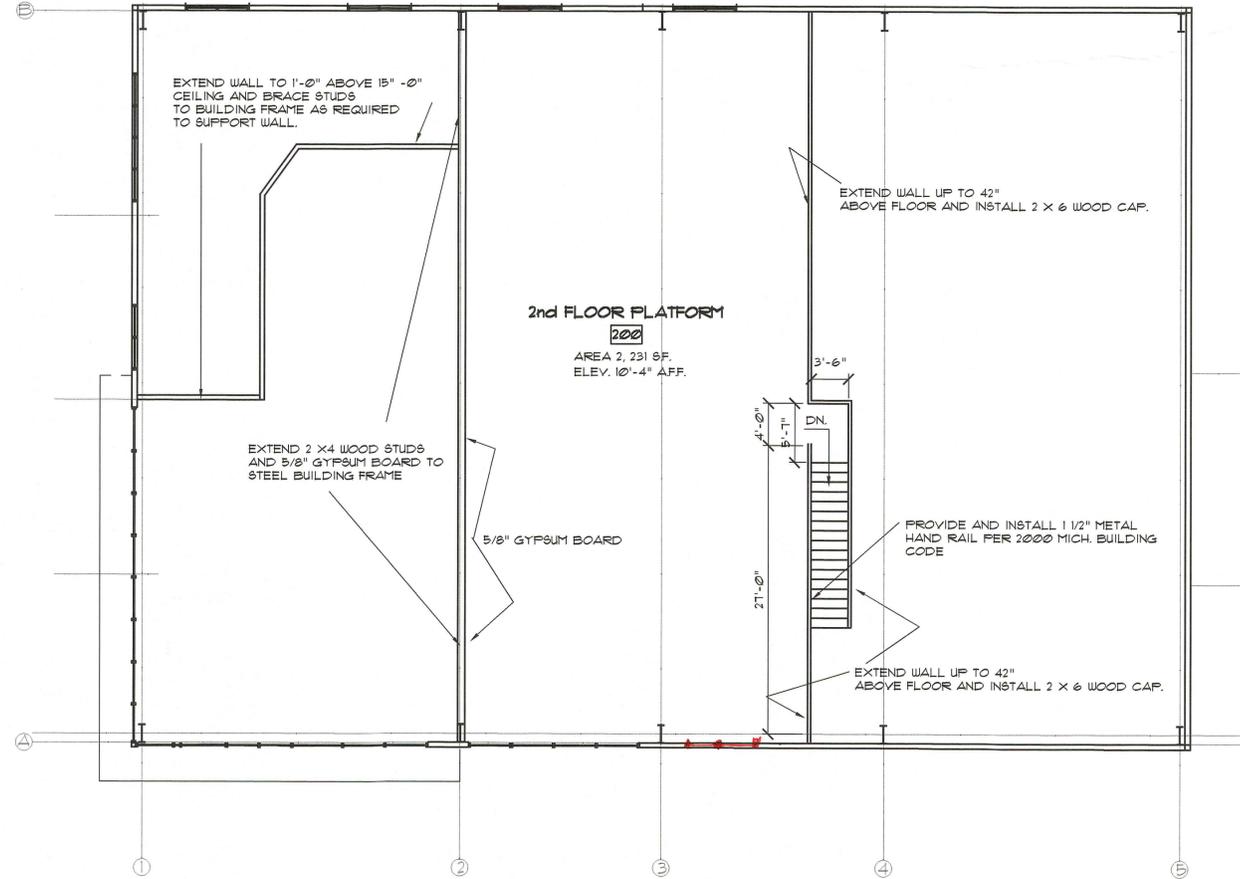
TYPICAL 14" TJI(TM) -250, 2 PLYES @ 16" O.C. INSTALL PER MFR'S RECOMMENDATION  
INSTALL 5/8" PLYWOOD DECKING GLUED AND NAILED



TJI NOTES:  
FLOOR JOISTS: TRUSS JOIST TJI SERIES TO SUPPORT THE LOADING SHOW WITH A TOTAL DEFLECTION OF LESS THAN 1/360.  
LOADING BASIC: 15 P&F DEAD LOAD, 125 P&F LIVE LOAD,

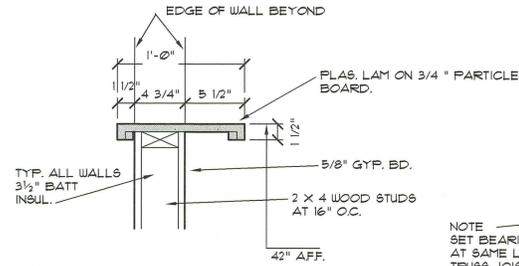
### PLATFORM FRAMING

Scale 1/8" = 1'-0"



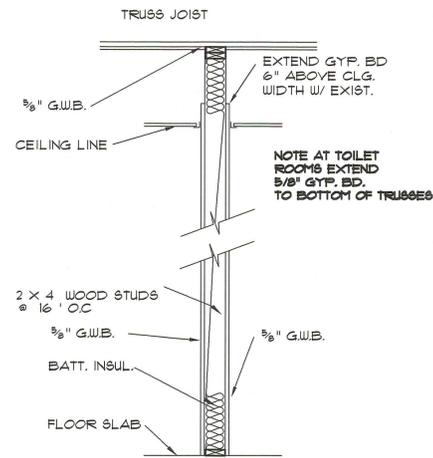
### PLATFORM PLAN

Scale 1/8" = 1'-0"



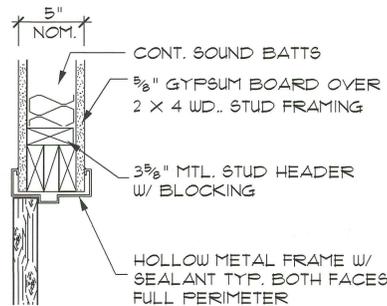
### SHELF DET.

Scale 1/2" = 1'-0"



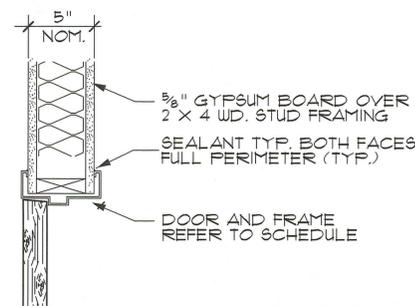
### TYPICAL INTERIOR WALL

NO SCALE



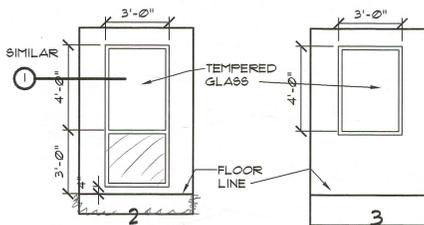
### TYP. DOOR HEAD

Scale 1/2" = 1'-0"



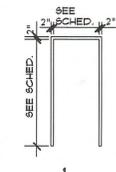
### TYP. DOOR JAMB

Scale 1/2" = 1'-0"



### DOOR TYPES

No Scale



### FRAME TYPES

No Scale

### DOOR SCHEDULE

DOOR No.	DOOR SIZE	DOOR		FRAME		DETAILS			RATING (MIN.)	H'WARE	REMARKS
		TYPE	MATERIAL	TYPE	MATERIAL	HEAD	JAMB	SILL			
100	PR 3'-0" X 7'-0" X 1 3/4"	---	ALUM	---	ALUM	---	---	---	---	---	---
101	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
102	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
103	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
104	3'-0" X 7'-0" X 1 3/4"	B	WOOD	1	H.M.	2	1	---	---	---	---
105	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
105a	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	UNDERCUT DOOR 1 INCH
107	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
108	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
108A	3'-0" X 7'-0" X 1 3/4"	B	WOOD	1	H.M.	2	1	---	---	---	---
109	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
110	3'-0" X 7'-0" X 1 3/4"	A	METAL	1	H.M.	2	1	---	---	---	---
110A	3'-0" X 7'-0" X 1 3/4"	B	WOOD	1	H.M.	2	1	---	---	---	---
111	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
112	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
113	3'-0" X 7'-0" X 1 3/4"	B	WOOD	1	H.M.	2	1	---	---	---	---
113A	PR 2'-6" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
113B	PR 2'-6" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
114	3'-0" X 7'-0" X 1 3/4"	A	METAL	1	H.M.	2	1	---	---	---	---
115	3'-0" X 7'-0" X 1 3/4"	A	WOOD	1	H.M.	2	1	---	---	---	---
120	---	---	---	2	H.M.	2	1	---	---	---	REFER TO ELEV. 1/A-5
121	---	---	---	2	H.M.	2	1	---	---	---	REFER TO ELEV. 1/A-5
122	---	---	---	3	H.M.	2	1	---	---	---	REFER TO ELEV. 1/A-5
123	---	---	---	2	H.M.	2	1	---	---	---	REFER TO ELEV. 1/A-5
124	---	---	---	2	H.M.	2	1	---	---	---	REFER TO ELEV. 1/A-5

ARCHITECTURE ONE, INC.  
450 W. CENTRE  
PORTAGE, MICHIGAN 49024  
PHONE: 269.329.1575  
FAX: 269.329.1580  
EMAIL: RCARBH1@SERV.NET

NEW BUILDING FOR  
**PRECISION BUILDERS**  
9651 PORTAGE ROAD  
PORTAGE MICHIGAN

DATE  
**2-22-02**  
REVISIONS  
**FINAL CONST.**  
**SET**  
**04/18/03**  
Revision  
**4-18-03**

PROJECT NUMBER  
**02011**  
SHEET NUMBER

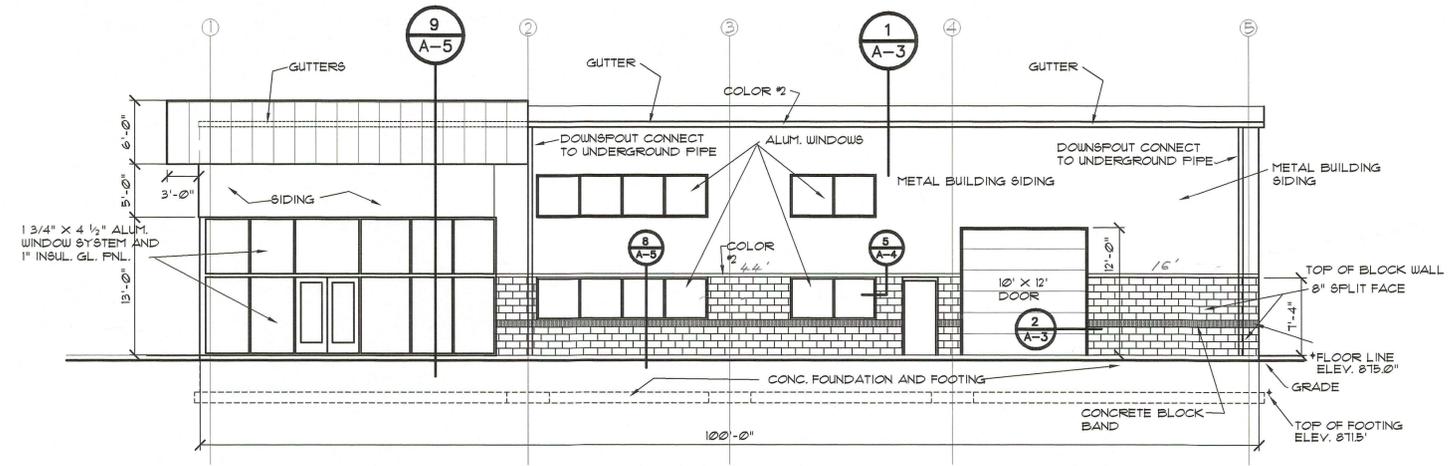
**A-2**

V:\Czandra cA-Project-020201\precidA-2 PLATFORM.dwg, 4/17/2003 7:27:10 PM, Copyrighted architecture one, Copyrighted architecture one



**WEST ELEVATION**

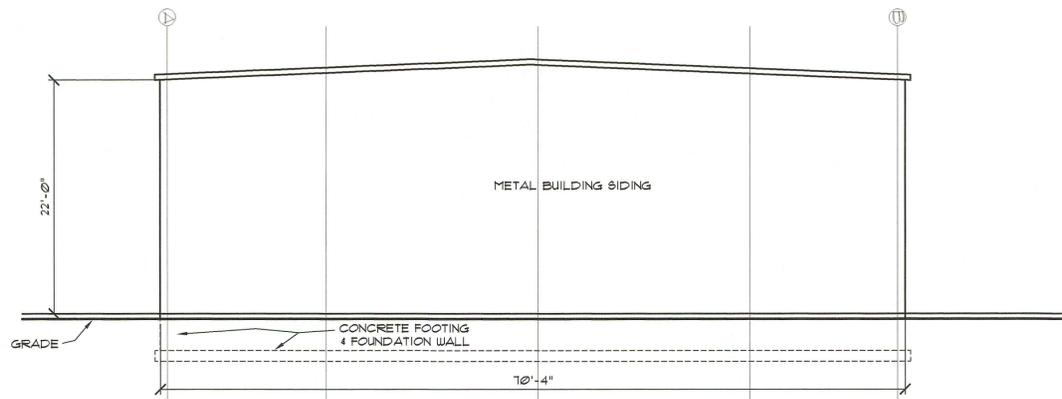
Scale 1/8" = 1'-0"



**SOUTH ELEVATION**

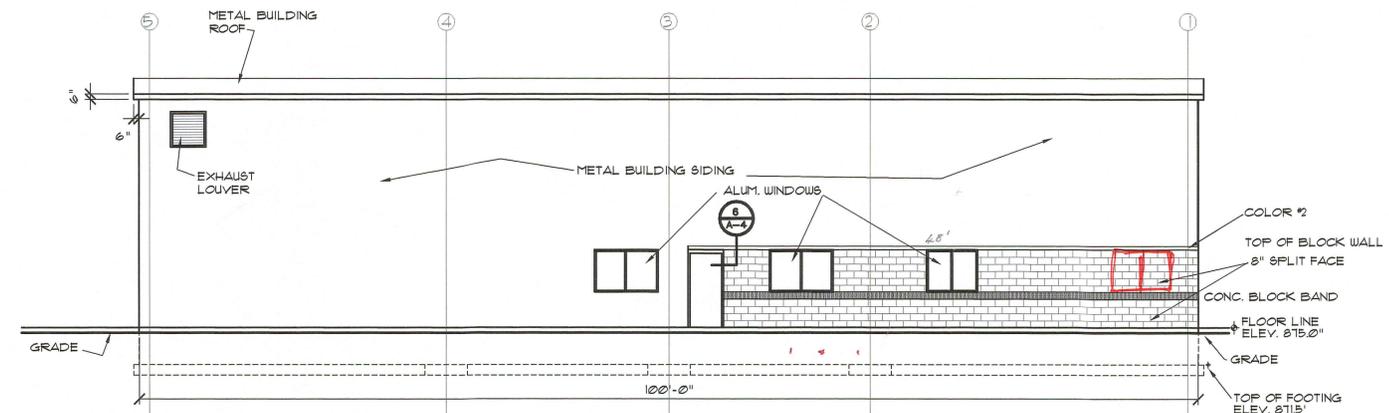
Scale 1/8" = 1'-0"

NOTE:  
INSTALL INSULATION IN ALL BLOCK VOIDS



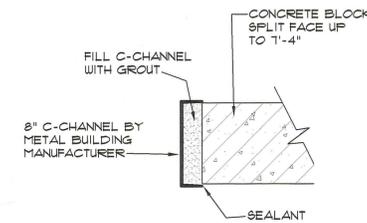
**EAST ELEVATION**

Scale 1/8" = 1'-0"



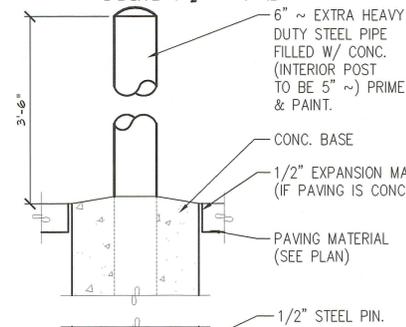
**NORTH ELEVATION**

Scale 1/8" = 1'-0"



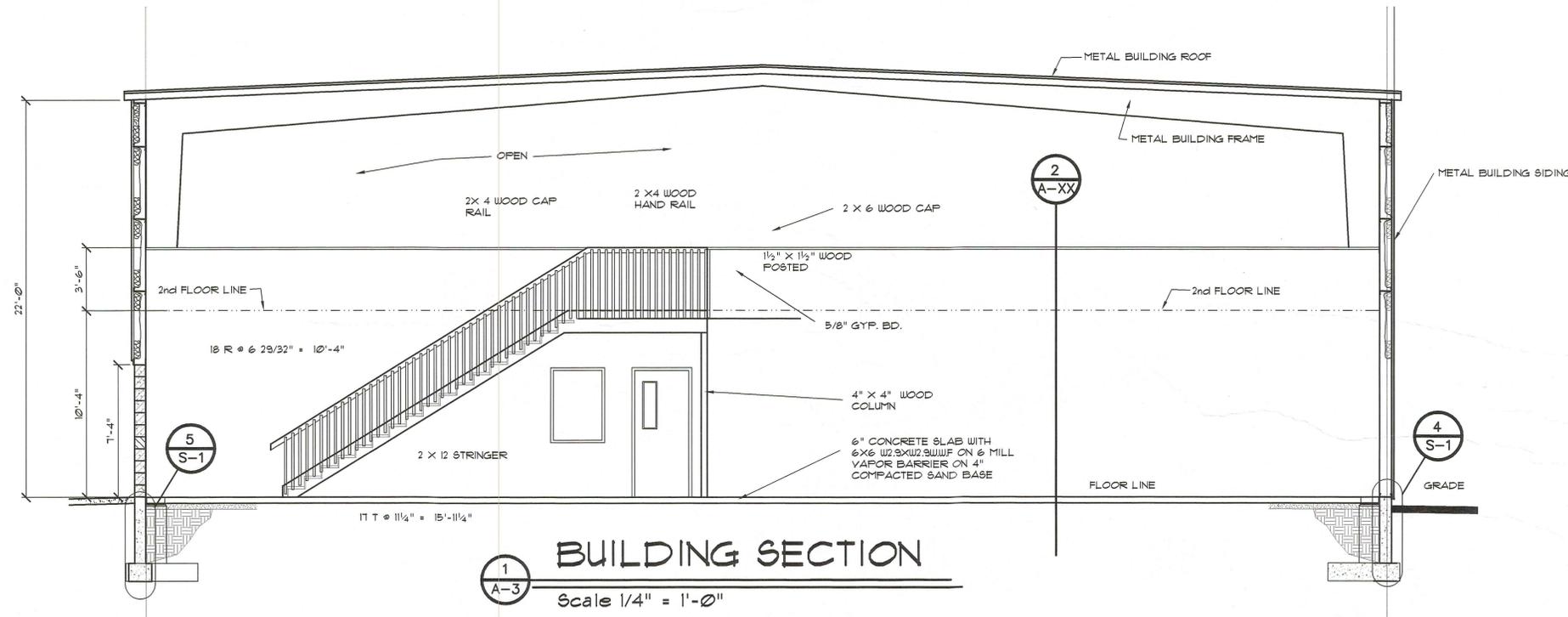
**FRAME DET.**

Scale 1 1/2" = 1'-0"



**BOLLARD DETAIL**

Scale 1" = 1'-0"



**BUILDING SECTION**

Scale 1/4" = 1'-0"

ARCHITECTURE ONE, INC.  
450 W. CENTRE  
PORTAGE, MICHIGAN 49024  
PHONE: 269.329.1575  
FAX: 269.329.1580  
EMAIL: RDARCH1@ISERV.NET

ONE  
Architecture

NEW BUILDING FOR  
**PRECISION BUILDERS**  
9651 PORTAGE ROAD  
PORTAGE MICHIGAN

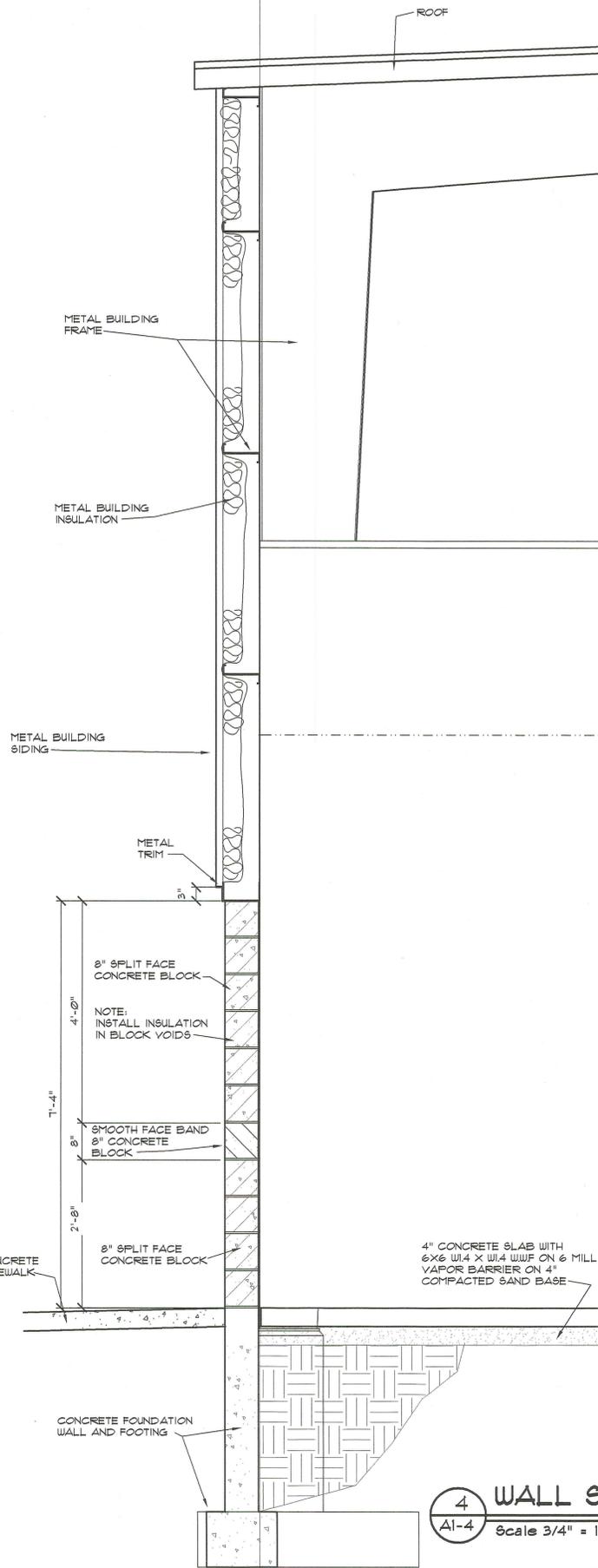
DATE  
2-22-02  
REVISIONS  
FINAL CONST.  
SET  
04/18/03  
Revision  
4-18-03

SHEET TITLE  
PROJECT NUMBER  
02011  
SHEET NUMBER

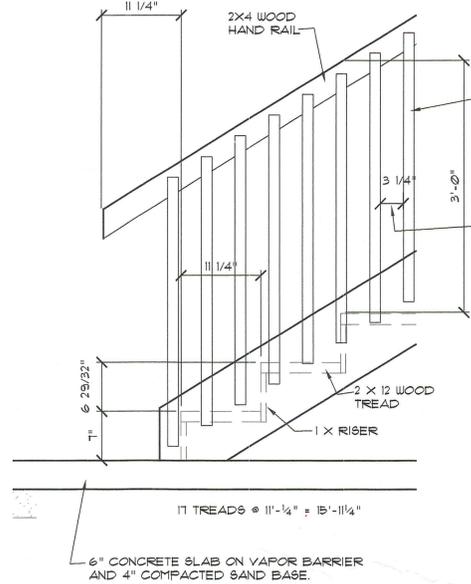
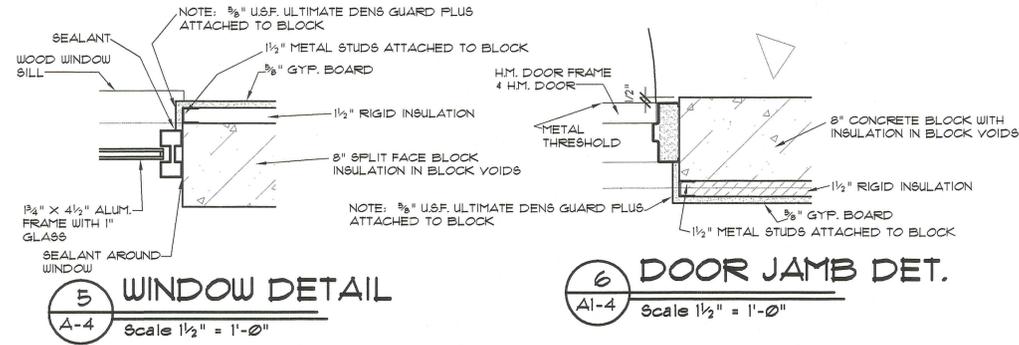
A-3

I:\cavandra c\A-Project\0202011\precidbldr-3\ELEVATIONS.dwg, 4/17/2003 7:25:54 PM, Copyrighted architecture one. Copyrighted architecture one

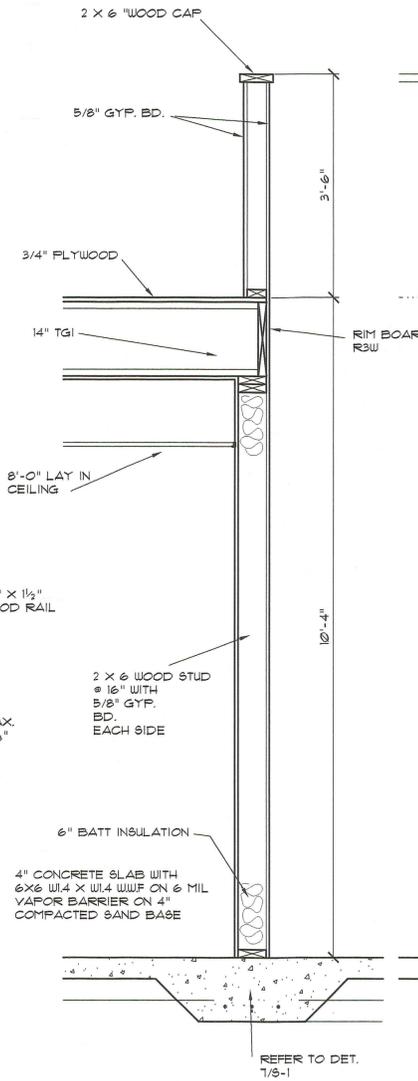
Z:\zandara cA-Project-02\02011\precision\A-4 SECTION.dwg, 3/26/2003 8:56:45 AM, Copyrighted architecture one, Copyrighted architecture one



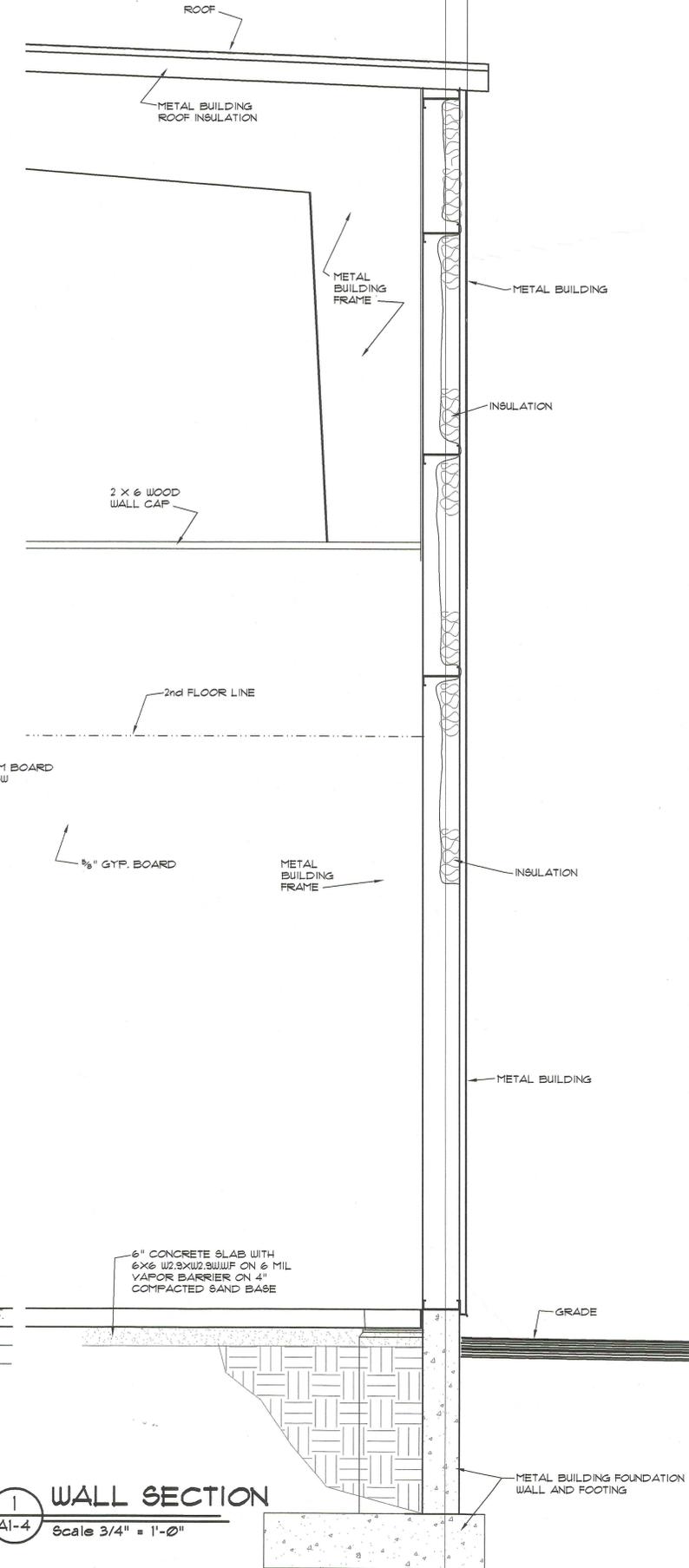
**4 WALL SECTION**  
A1-4 Scale 3/4" = 1'-0"



**3 STAIR DETAIL**  
A1-4 Scale 1" = 1'-0"



**2 WALL SECTION**  
A1-4 Scale 3/4" = 1'-0"



**1 WALL SECTION**  
A1-4 Scale 3/4" = 1'-0"

ARCHITECTURE ONE, INC.  
450 W. CENTRE  
PORTAGE, MICHIGAN 49024  
PHONE: 269.329.1575  
FAX: 269.329.1580  
EMAIL: RCARCH1@ISERV.NET

ONE  
Architecture

NEW BUILDING FOR  
**PRECISION BUILDERS**  
9651 PORTAGE ROAD  
PORTAGE MICHIGAN

DATE  
**2-22-02**  
REVISIONS  
**FINAL CONST.**  
SET  
**3/25/03**

SHEET TITLE  
PROJECT NUMBER  
**02011**  
SHEET NUMBER

**A-4**



ARCHITECTURE ONE, INC.  
 450 W. CENTRE  
 PORTAGE, MICHIGAN 49024  
 PHONE: 269.329.1575  
 FAX: 269.329.1580  
 EMAIL: RCARDHI@ISERV.NET

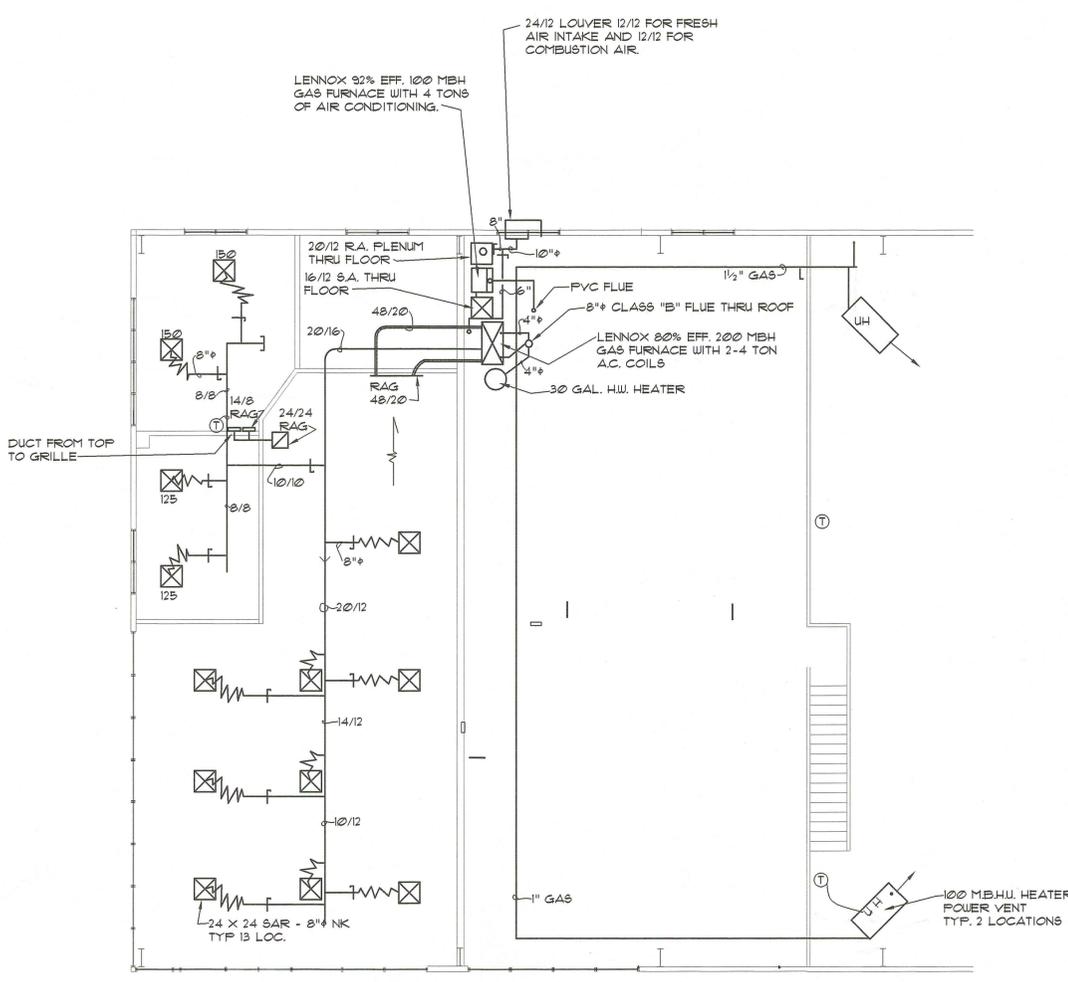


NEW BUILDING FOR  
**PRECISION BUILDERS**  
 9651 PORTAGE ROAD  
 PORTAGE MICHIGAN

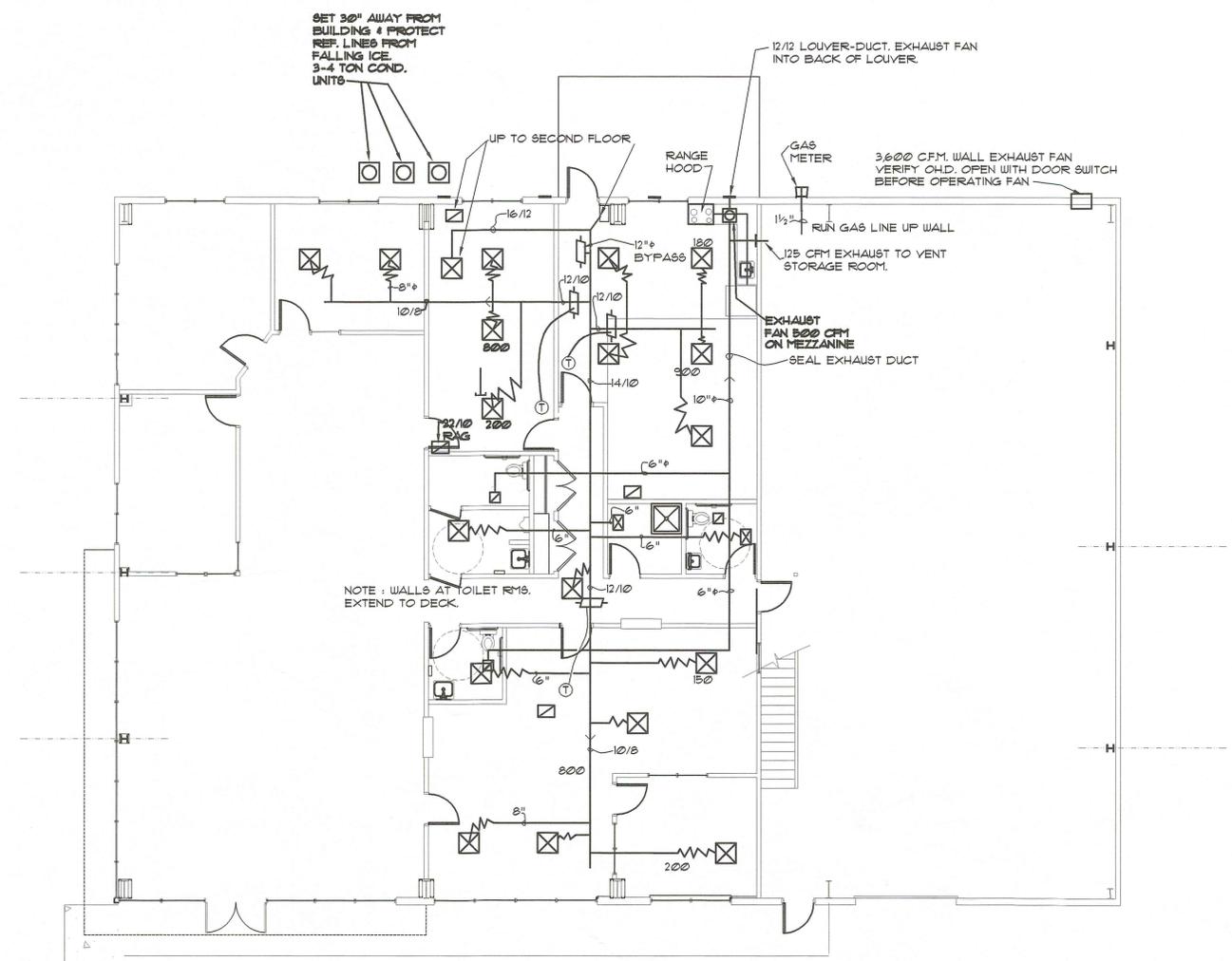
DATE  
**2-22-02**  
 REVISIONS  
**FINAL CONST. SET**  
**04/18/03**  
 Revision  
**4-18-03**

SHEET TITLE  
 PROJECT NUMBER  
**02011**  
 SHEET NUMBER

**M-1**



**MECHANICAL SECOND FLOOR PLAN**  
 Scale 1/8" = 1'-0"



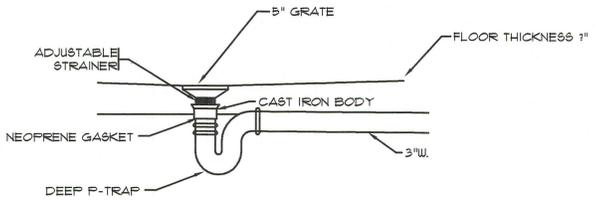
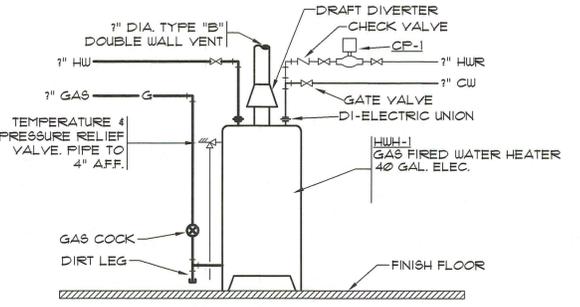
**MECHANICAL FLOOR PLAN**  
 Scale 1/8" = 1'-0"

ELECTRICAL AND MECHANICAL PLANS AND SCHEDULES ARE DRAWN BASED ON INFORMATION PROVIDED BY THE OWNER OR SUB-CONTRACTORS. ARCHITECTURE ONE PROVIDES THIS SERVICE AS A CONVENIENCE TO THE PROJECT AND ASSUMES NO LIABILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED. BEFORE INSTALLATION THE CONTRACTORS ARE TO VERIFY ALL INFORMATION WITH THE OWNER AND OTHER SUB-CONTRACTORS.

I:\c\zandra c\A-Project-0202011\precoldm-1.dwg, 4/21/2003 8:33:57 AM, Copyrighted architecture one, Copyrighted architecture one

### MECHANICAL SPECIFICATIONS

- CONTRACTOR IS RESPONSIBLE TO VISIT THE JOB SITE TO VERIFY EXISTING CONDITIONS RELATING TO WORK REQUIRED.
- CONTRACTOR IS REQUIRED TO SUBMIT SHOP DRAWINGS TO THE ENGINEER FOR REVIEW.
- CONTRACTOR SHALL PAY FOR ALL NECESSARY PERMITS, FEES, TAXES, AND APPROVALS FOR CONSTRUCTION.
- CONTRACTOR SHALL REMOVE ALL SALVAGE MATERIALS AND DEBRIS FROM THE SITE AND REPLACE OR REPAIR ALL AREAS DAMAGED OR DISTURBED BY THE CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE ALL UTILITIES.
- INSTALL ALL EQUIPMENT PER MANUFACTURER'S INSTRUCTIONS AND MAINTAIN ALL RECOMMENDED CLEARANCES FOR SERVICE AND PROPER OPERATION OF EQUIPMENT.
- ALL DUCTWORK SHALL BE INSTALLED PER SMACNA STANDARDS. DUCTS INSTALLED DIFFERENT THAN SIZES SHOWN SHALL HAVE THE SAME OR GREATER EQUIVALENT DUCT SIZE. PROVIDE DUCT TRANSITIONS AND FITTINGS AS REQUIRED. SEAL JOINTS WITH HARDCAST IRON GRIP 601, SURE GRIP 404, OR OTHER APPROVED SEALANT.
- NOT USED
- COORDINATE ALL DUCTWORK LOCATIONS WITH EXISTING STRUCTURE, OTHER DUCTWORK, PLUMBING, LIGHT FIXTURES, CONDUIT, SPRINKLERS, AND EQUIPMENT.
- DUCT DIMENSIONS SHOWN ARE INSIDE FREE AREA.
- LINE FIRST 10 FEET OF SUPPLY AND RETURN DUCTWORK WITH 3/4" THICK CLOSED CELL FLEXIBLE ELASTOMERIC DUCT LINER, AP ARMAFLEX SA OR EQUAL.
- NOT USED
- INSULATE SUPPLY, RETURN, AND OUTSIDE AIR DUCTWORK WITH 3/4 LB. 1-1/2" FIBERGLASS INSULATION WITH FSK VAPOR BARRIER TAPE AND SEAL ALL SEAMS, CUT-OUTS, AND PENETRATIONS. INSULATE THE NECKS AND TOPS OF ALL SUPPLY AIR REGISTERS AND DIFFUSERS.
- INSULATE ALL OUTDOOR DUCTWORK WITH 1 1/2 LB. 2" FIBERGLASS INSULATION WITH FSK VAPOR BARRIER TAPE AND SEAL ALL SEAMS, CUT-OUTS, AND PENETRATIONS. WRAP WITH ALUMINUM JACKET SEALED WATER TIGHT.
- ISOLATE AIR MOVING EQUIPMENT WITH SPRING OR NEOPRENE HANGING VIBRATION ISOLATORS AND WITH FLEXIBLE CANVAS CONNECTIONS TO DUCTWORK.
- NOT USED
- ALL ROUND DUCTWORK ELBOWS MUST HAVE MINIMUM CENTERLINE RADIUS OF 15 TIMES THE DUCT DIAMETER.
- PROVIDE AND INSTALL SINGLE THICKNESS TURNING VANES IN ALL RECTANGULAR 90 DEGREE ELBOWS. MAX. LENGTH FOR A SINGLE SECTION OF VANES IS 24" LONG.
- MAXIMUM 5'-0" FLEX DUCT TO DIFFUSERS.
- ALL DIFFUSERS AND GRILLES THAT ARE LAY-IN STYLE
- COORDINATE REGISTER, GRILLE, AND DIFFUSER LOCATIONS WITH THE REFLECTED CEILING PLAN.
- CENTER ALL GRILLES AND DIFFUSERS IN CEILING TILES.
- TEST AND BALANCE AIR DISTRIBUTION SYSTEM WITHIN 10% OF CFM INDICATED ON DRAWINGS. PROVIDE CERTIFIED REPORT BY INDEPENDENT, CERTIFIED TEST AND BALANCE CONTRACTOR.
- ALL GAS PIPING SHALL BE SCHEDULE 40 BLACK STEEL. SCREWED PIPE AND FITTINGS FOR 2" PIPE AND SMALLER. WELDED PIPE AND FITTINGS FOR 2-1/2" PIPE AND LARGER.
- PROVIDE INDIVIDUAL GAS SHUT-OFF VALVE AND DIRT LEG AT EACH GAS FIRED PIECE OF EQUIPMENT.
- PROVIDE 10 GAUGE SLEEVES AT MECHANICAL CHASES WHERE REQUIRED. INSTALL PER SMACNA STANDARDS.
- M.C. TO PROVIDE AND INSTALL THERMOSTATS AND MAKE FINAL ELECTRICAL CONNECTIONS BETWEEN AHUS AND THERMOSTATS. E.C. TO PROVIDE CONDUITS AND WIRING. INSTALL PER ELECTRICAL SPECIFICATIONS.
- PATCH ALL WALL, FLOOR AND CEILING PENETRATIONS TO MATCH EXISTING.
- FIRE STOP ALL WALL, FLOOR AND CEILING PENETRATIONS WHERE REQUIRED TO MAINTAIN FIRE SEPARATION INTEGRITY. USE U.L. LISTED MATERIAL.
- M.C. IS RESPONSIBLE TO PROVIDE OWNER WITH COMPLETE SET OF INSTALLATION, OPERATION, AND MAINTENANCE MANUALS FOR ALL MECHANICAL EQUIPMENT.



**FLOOR DRAIN DETAIL - FD-1**  
 NO SCALE

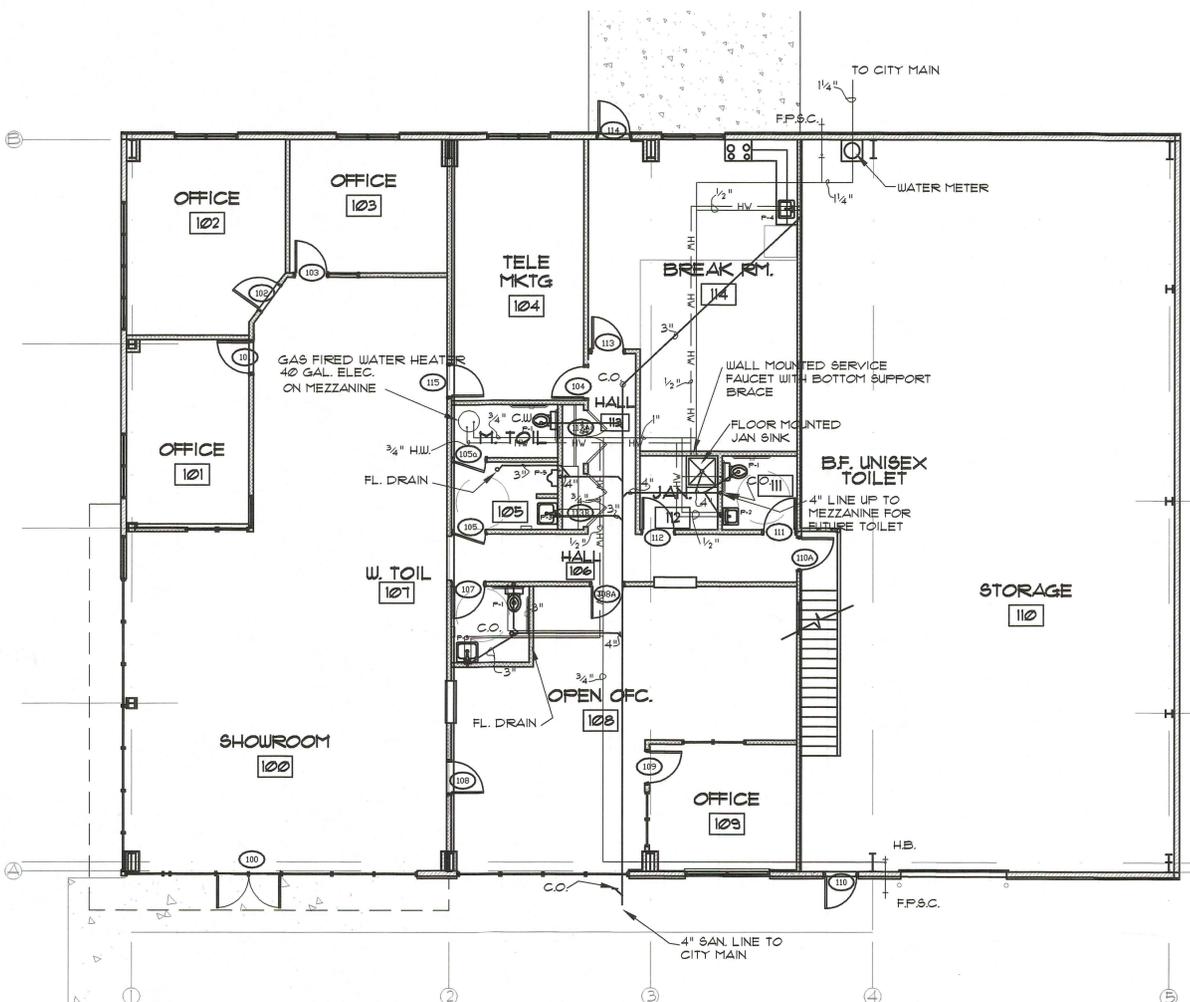
NOTE: FLOOR DRAINS NOT INSTALLED IN SLAB ON GRADE WILL REQUIRE A 24"x24" WATER PROOF MEMBRANE OR FLASHING INSTALLED UNDER FIXTURE

### PLUMBING GENERAL SPECIFICATIONS:

- CONTRACTOR IS RESPONSIBLE TO VERIFY EXISTING CONDITIONS RELATING TO WORK REQUIRED.
- CONTRACTOR SHALL PAY FOR ALL NECESSARY PERMITS, FEES, TAXES AND APPROVALS FOR CONSTRUCTION.
- CONTRACTOR'S WORK SHALL COMPLY WITH ALL LOCAL, STATE, NATIONAL CODES AND ORDINANCES THAT APPLY.
- PATCH ALL WALL, FLOOR AND CEILING PENETRATIONS TO MATCH EXISTING.
- PROVIDE HANGERS AND SUPPORTS AS REQUIRED BY CODE.
- FIRE STOP ALL WALL, FLOOR AND CEILING PENETRATIONS WHERE REQUIRED TO MAINTAIN FIRE SEPARATION INTEGRITY. USE U.L. LISTED MATERIAL.
- FOR TIE-INS TO EXISTING SYSTEM, EXISTING MUST BE EQUAL TO OR LARGER THAN NEW.
- CONTRACTOR SHALL REMOVE ALL SALVAGE MATERIALS AND DEBRIS FROM THE SITE AND REPLACE OR REPAIR ALL AREAS DAMAGED OR DISTURBED BY THE CONSTRUCTION.
- ALL EQUIPMENT, PIPING AND FIXTURES SHALL BE INSTALLED PER CODE AND LOCAL AUTHORITY HAVING JURISDICTION.
- ALL GAS PIPING SHALL BE SCHEDULE 40 BLACK STEEL A58M A53/120. SCREWED PIPE AND FITTINGS FOR 2" PIPE AND SMALLER. WELDED PIPE AND FITTINGS FOR 2 1/2" PIPE AND LARGER. CONCEAL ALL PIPE IN FINISHED AREAS PER CODE.
- PROVIDE INDIVIDUAL GAS SHUT-OFF AND DIRT LEG AT EACH GAS FIRED PIECE OF EQUIPMENT.
- DOMESTIC HOT AND COLD WATER PIPING SHALL BE COPPER TYPE 'L'. USE LEAD FREE SOLDER.
- INSULATE DOMESTIC HOT AND COLD PIPING WITH 1/2" FIBERGLASS (APPROX. 3 1/2 POUND DENSITY) WITH VAPOR BARRIER. FITTINGS AND VALVES SHALL BE COVERED WITH FIBERGLASS INSERT AND PREMOULDED PVC COVER.
- SANITARY WASTE AND VENT PIPING SHALL BE PVC, A58M D2665, EXCEPT IN RETURN AIR FLENUM, USE CAST IRON A58M A14.
- SLOPE ALL SANITARY WASTE PIPING 1/4" PER FOOT WHERE POSSIBLE AND A MINIMUM OF 1/8" PER FOOT.
- PROVIDE P-TRAPS, ANGLE STOPS AND CHROME PLATED WALL ESCUTCHEONS AT EACH PLUMBING FIXTURE.
- CAULK EACH PLUMBING FIXTURE BETWEEN WALLS AND FLOORS WITH WHITE SILICONE RUBBER SEALANT.
- SEE ARCHITECTURAL DRAWINGS FOR FIXTURE MOUNTING HEIGHTS.
- PROVIDE CHROME PLATED CLEANOUT COVER ON ALL WALL CLEANOUTS.
- PROVIDE POLISHED BRONZE CLEANOUT COVERS ON ALL FLOOR CLEANOUTS WHERE VISIBLE.
- PROVIDE SHOCK ABSORBERS ON ALL WATER SUPPLY WHERE REQUIRED. SHOCK ABSORBERS SHALL BE IN ACCORDANCE WITH FDI (PLUMBING AND DRAINAGE INSTITUTE) STANDARDS. SHOCK ABSORBERS SHALL BE SIOUX CHIEF "HYDRA-RESTER".
- PROVIDE CLEANOUTS AT THE BASE OF ALL RISERS.
- ALL NEW HOT AND COLD WATER LINES SHALL BE SANITIZED PER LOCAL WATER DEPARTMENT REQUIREMENTS. FLUSH AND TEST SYSTEM FOR LEAKS. PLUMBING CONTRACTOR IS RESPONSIBLE FOR THE PROPER AND LEAK FREE OPERATION OF THE SYSTEM.
- PROVIDE DI-ELECTRIC FITTINGS WHERE ANY DISSIMILAR METALS ARE CONNECTED.
- DISTRIBUTE HOT AND COLD WATER PIPING AS REQUIRED TO MEET MINIMUM CONNECTED SIZE REQUIREMENTS.
- PROVIDE INDIVIDUAL SHUT-OFF VALVE AT EACH INDIVIDUAL HUMIDIFIER, TO PERMIT REPAIRING A SINGLE UNIT, WHILE MAINTAINING SERVICE TO REMAINING UNITS.

**GAS FIRED WATER HEATER DETAIL**  
 NO SCALE

**FLOOR DRAIN DETAIL - FD-1**  
 NO SCALE



**PLUMBING PLAN**

Scale 1/8" = 1'-0"

ELECTRICAL AND MECHANICAL PLANS AND SCHEDULES ARE DRAWN BASED ON INFORMATION PROVIDED BY THE OWNER OR SUB-CONTRACTORS. ARCHITECTURE ONE PROVIDES THIS SERVICE AS A CONVENIENCE TO THE PROJECT AND ASSUMES NO LIABILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED. BEFORE INSTALLATION THE CONTRACTORS ARE TO VERIFY ALL INFORMATION WITH THE OWNER AND OTHER SUB-CONTRACTORS.

### PLUMBING FIXTURE SCHEDULE

MARK #	TYPE	MFGR.	MODEL #	CW	HW	WASTE	NOTES
P-1	WATER CLOSET, TANK FLR MNT.	**		1/2"	-	4"	
P-2	LAVATORY, WALL MOUNT	**		1/2"	1/2"	2"	*CHICAGO FAUCETS MODEL 802A
P-3	LAVATORY, COUNTER MOUNT	**		1/2"	1/2"	2"	
P-4	KITCHEN SINK, STAINLESS STEEL	-		1/2"	1/2"	2"	
P-5	URINAL	**		3/4"		3"	

- \* PROVIDE WITH FIXTURE.
- \*\* AMERICAN STANDARD
- W.C., URINAL AND LAVATORY COLOR BY THE ARCHITECT.
- PROVIDE CARRIERS FOR ALL WALL HUNG FIXTURES.
- PROVIDE TRUBRO "LAYGARD" INSULATION ON LAVATORY TRAP & SUPPLY PIPING.

ARCHITECTURE ONE, INC.  
 450 W. CENTRE  
 PORTAGE, MICHIGAN 49024  
 PHONE: 269.329.1575  
 FAX: 269.329.1580  
 EMAIL: RCARCH1@SERV.NET

ONE  
 Architecture

NEW BUILDING FOR  
**PRECISION BUILDERS**  
 9651 PORTAGE ROAD  
 PORTAGE MICHIGAN

DATE  
**2-22-02**

REVISIONS  
**FINAL CONST. SET**  
**04/18/03**

Revision  
**4-18-03**

SHEET TITLE

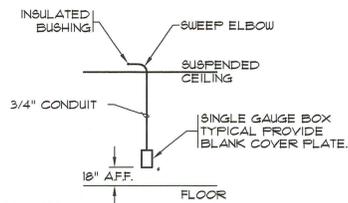
PROJECT NUMBER  
**02011**

SHEET NUMBER  
**P-1**

**ELECTRICAL SPECIFICATIONS:**

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST ADOPTED EDITION OF THE NATIONAL ELECTRICAL CODE AND ALL AUTHORITIES HAVING JURISDICTION.
2. SECURE AND PAY FOR ALL PERMITS AND FEES.
3. ELECTRICAL CONTRACTOR SHALL COORDINATE WITH ALL OTHER TRADES FOR INSTALLATION OF ALL EQUIPMENT. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS.
4. ALL WIRING SHALL BE COPPER No. 12 AWG MINIMUM SIZE, TYPE THHN/THLN INSULATION.
5. ALL WIRING SHALL BE INSTALLED IN A METALLIC CONDUIT SYSTEM UNLESS NOTED OTHERWISE ON THE DRAWING. ALL CONDUIT USE IN WET OR DAMP AREAS SHALL BE WATER TIGHT. PROVIDE FLEXIBLE METAL CONDUIT FOR CONNECTION TO ALL MOTORS. PROVIDE A SEPARATE EQUIPMENT GROUNDING CONDUCTOR WITH ALL FLEXIBLE METAL CONDUIT. MAXIMUM 2 FT. LENGTH OF FLEXIBLE METAL CONDUIT ALLOWED. RUN ALL NEW CONDUIT CONCEALED WHERE POSSIBLE. WHERE EXISTING DOES NOT PERMIT CONCEALED, RUN EXPOSED. ALL CONDUIT RUN EXPOSED SHALL BE RUN SQUARE TO THE BUILDING AND PAINTED TO MATCH SURFACE MOUNTED ON.
6. PROVIDE ELECTRICAL DEMOLITION AS REQUIRED. FIELD VERIFY EXTENT OF ELECTRICAL DEMOLITION.
7. WHERE CONDUIT RUNS ARE ABOVE EXISTING SUSPENDED CEILING, CEILING FADS AND GRID SHALL BE LEFT IN LIKE NEW CONDITION.
8. ALL RECEPTACLES SHALL BE 20AMP, 120V, SPECIFICATION GRADE, GROUNDING TYPE, IVORY COLOR WITH STAINLESS STEEL COVER PLATES. PROVIDE GFCI AND WEATHERPROOF TYPES WHERE SHOWN. VERIFY AND COORDINATE ALL POWER OUTLET TYPES AND LOCATIONS WITH OWNER PRIOR TO INSTALLATION.
9. ALL LIGHT SWITCHES SHALL BE SILENT TYPE, SPECIFICATION GRADE, 20AMP, 120V, IVORY COLOR WITH IVORY COVER PLATES. PROVIDE THREE WAY SWITCHES WHERE SHOWN.
10. ALL WIRING RUN WITHIN RETURN AIR FLENUMS SHALL BE UL LISTED FOR SUCH USE.  
  
AT 120 VAC.
12. PROVIDE UPDATED TYPED PANEL INDEXES ON ALL ELECTRICAL PANELS.
13. PROVIDE SHOP DRAWINGS TO ARCHITECT FOR APPROVAL.

LEGEND	

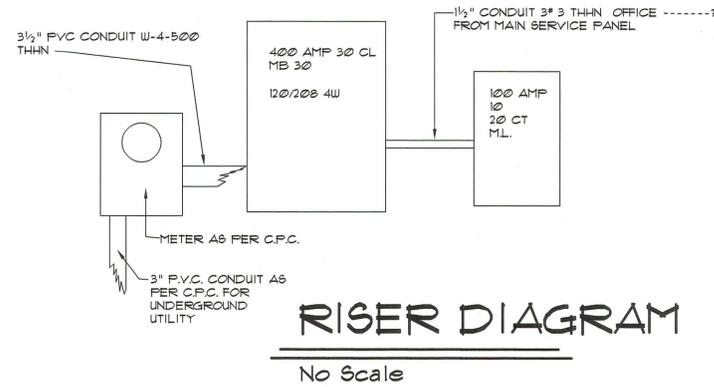


**TV/TELEPHONE/COMPUTER OUTLET DETAIL**  
SCALE: NO SCALE SYMBOLS ON DRAWINGS Δ

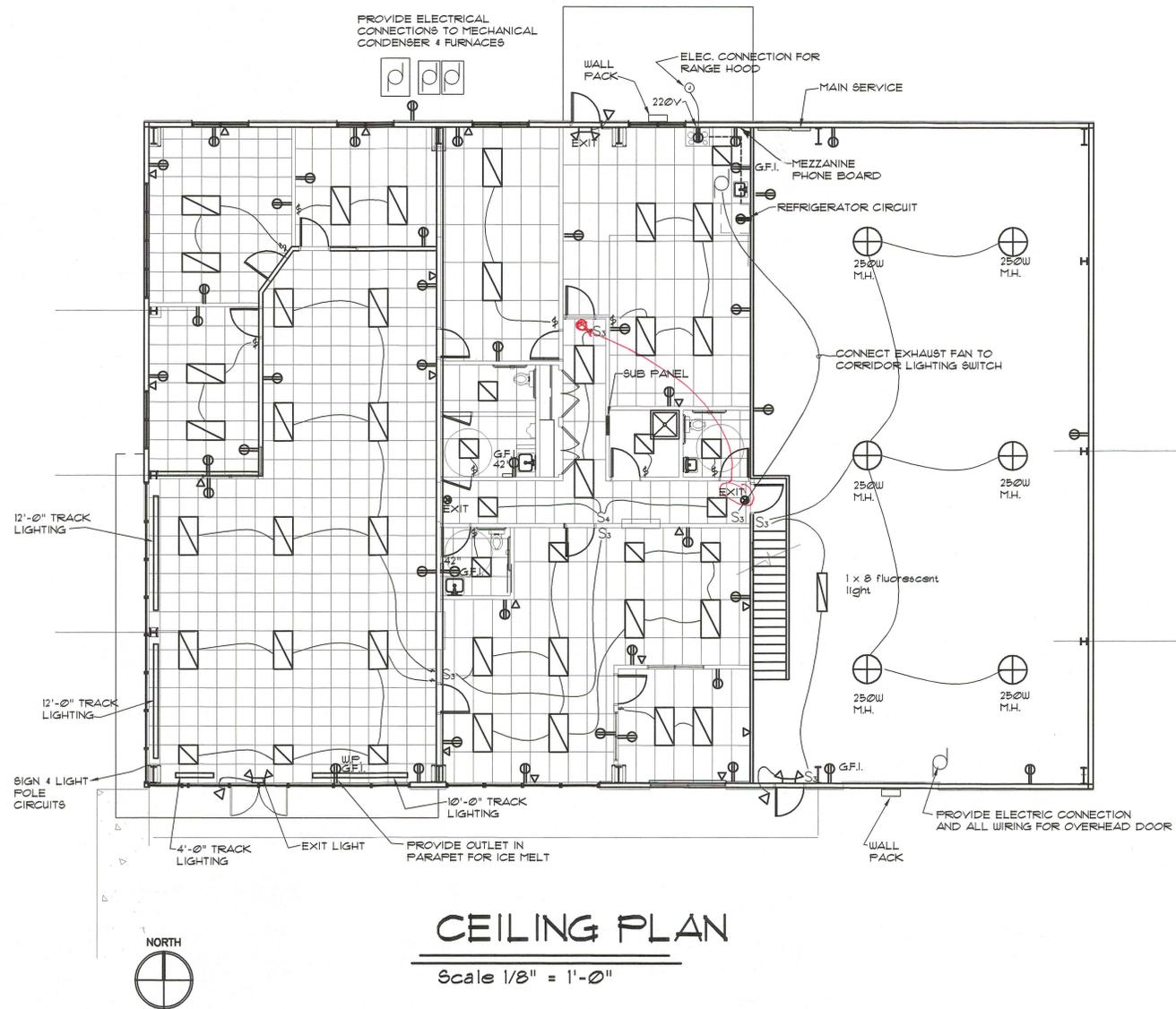
NOTE: PROVIDE EXIT LIGHTING AND EGRESS LIGHTING AS REQUIRED BY CODE

ELECTRICAL AND MECHANICAL PLANS AND SCHEDULES ARE DRAWN BASED ON INFORMATION PROVIDED BY THE OWNER OR SUB-CONTRACTORS. ARCHITECTURE ONE PROVIDES THIS SERVICE AS A CONVENIENCE TO THE PROJECT AND ASSUMES NO LIABILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED. BEFORE INSTALLATION THE CONTRACTORS ARE TO VERIFY ALL INFORMATION WITH THE OWNER AND OTHER SUB-CONTRACTORS.

**MAIN SERVICE EQUIPMENT**



**RISER DIAGRAM**  
No Scale



**CEILING PLAN**  
Scale 1/8" = 1'-0"

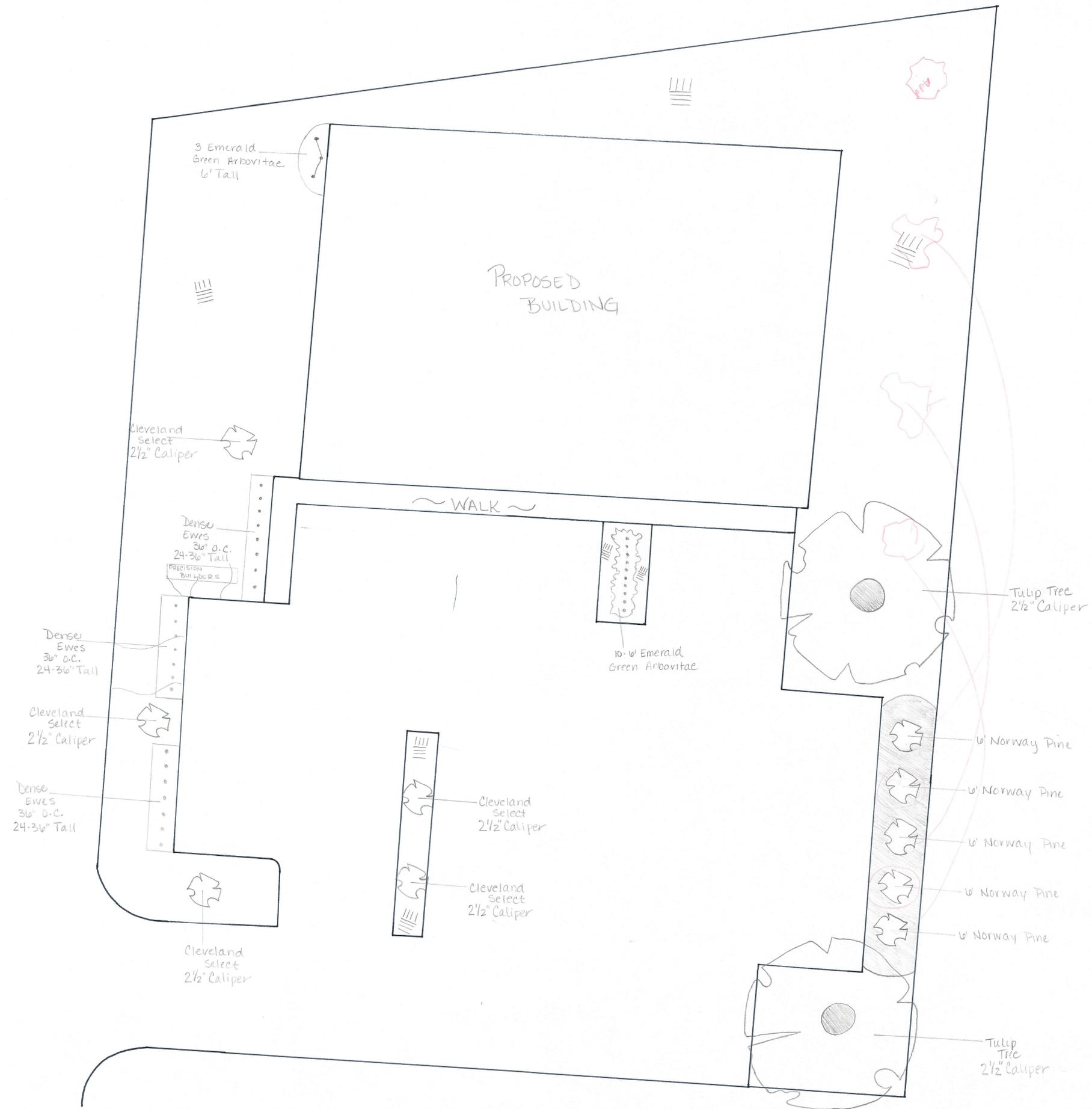
ARCHITECTURE ONE, INC.  
450 W. CENTRE  
PORTAGE, MICHIGAN 49024  
PHONE: 269.329.1575  
FAX: 269.329.1580  
EMAIL: RCARCH1@ISERV.NET

NEW BUILDING FOR  
**PRECISION BUILDERS**  
9651 PORTAGE ROAD  
PORTAGE MICHIGAN

DATE  
**2-22-02**  
REVISIONS  
**FINAL CONST. SET**  
**04/18/03**  
Revision  
**4-18-03**

SHEET TITLE  
  
PROJECT NUMBER  
**02011**  
SHEET NUMBER

**E-1**



**FLOOR PLAN**

HOWARD L. OVERBEEK  
 ARCHITECT, P.C.

25377 ISLAND VIEW LANE  
 STURGIS, MICHIGAN

PHONE (269) 377-3561

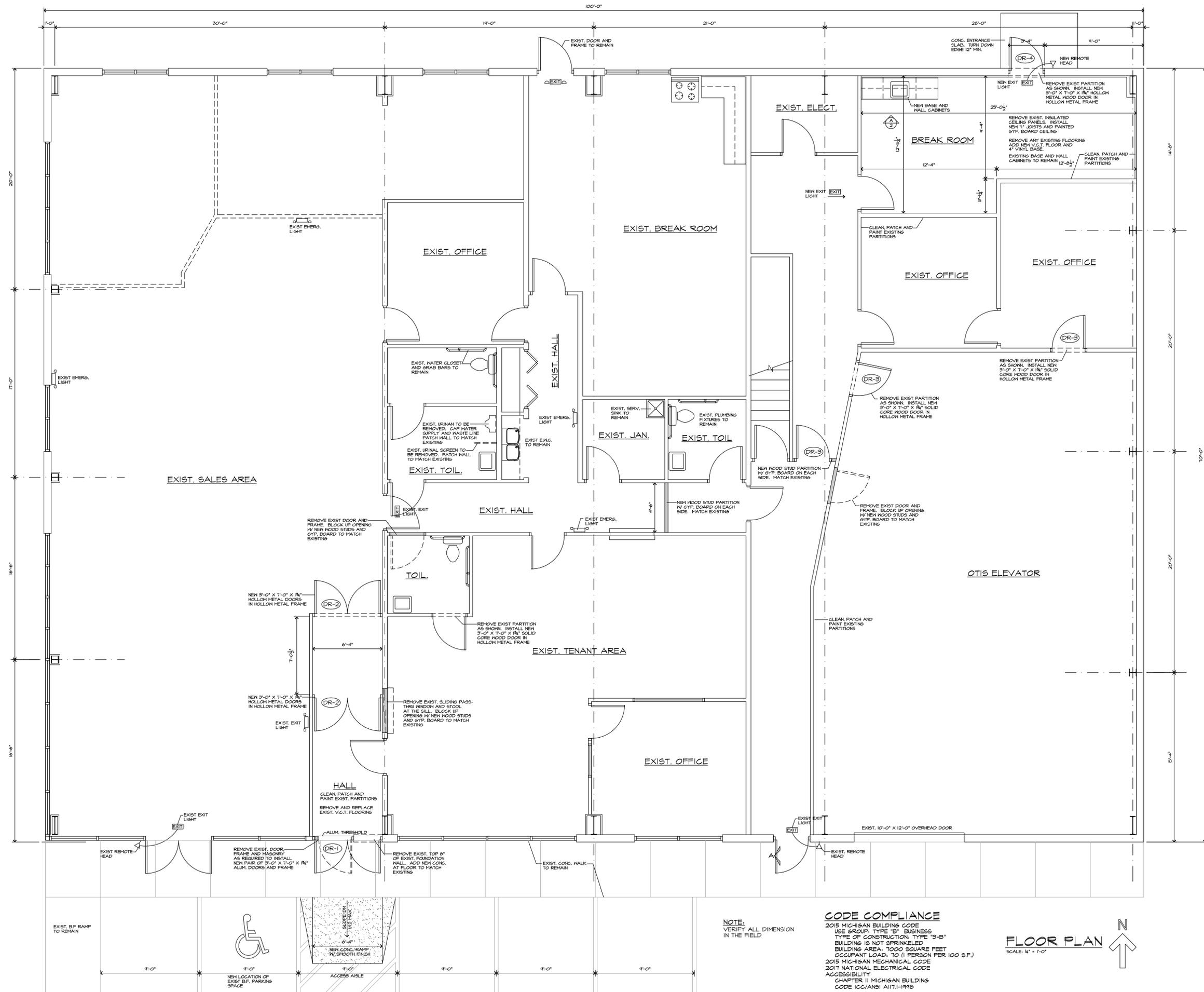


PROJECT NO.  
 2333

DATE  
 12/11/23

REVISION

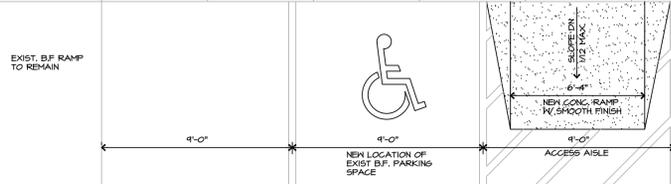
SHEET  
 1



NOTE:  
 VERIFY ALL DIMENSION  
 IN THE FIELD

**CODE COMPLIANCE**  
 2015 MICHIGAN BUILDING CODE  
 USE GROUP: TYPE "B" BUSINESS  
 TYPE OF CONSTRUCTION: TYPE "B-B"  
 BUILDING IS NOT SPRINKLED  
 BUILDING AREA: 1000 SQUARE FEET  
 OCCUPANT LOAD: 70 (1 PERSON PER 100 S.F.)  
 2015 MICHIGAN MECHANICAL CODE  
 2017 NATIONAL ELECTRICAL CODE  
 ACCESSIBILITY  
 CHAPTER 11 MICHIGAN BUILDING  
 CODE ICC/ANSI A117.1-1993

**FLOOR PLAN**  
 SCALE: 1/4" = 1'-0"



**MEZZANINE  
 PLAN**

HOWARD L. OVERBEEK  
 ARCHITECT, P.C.

25377 ISLAND VIEW LANE  
 STURGIS, MICHIGAN

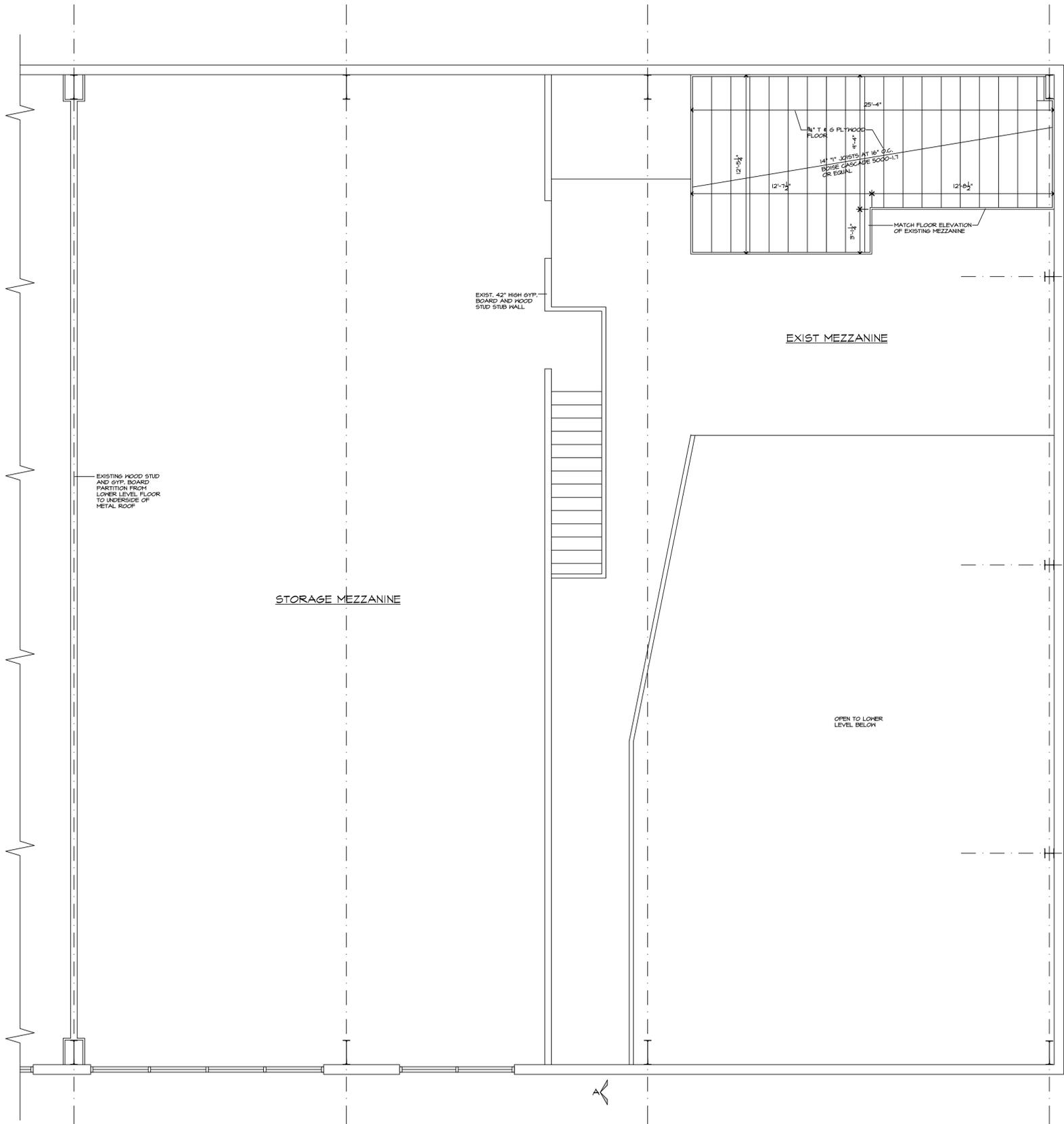
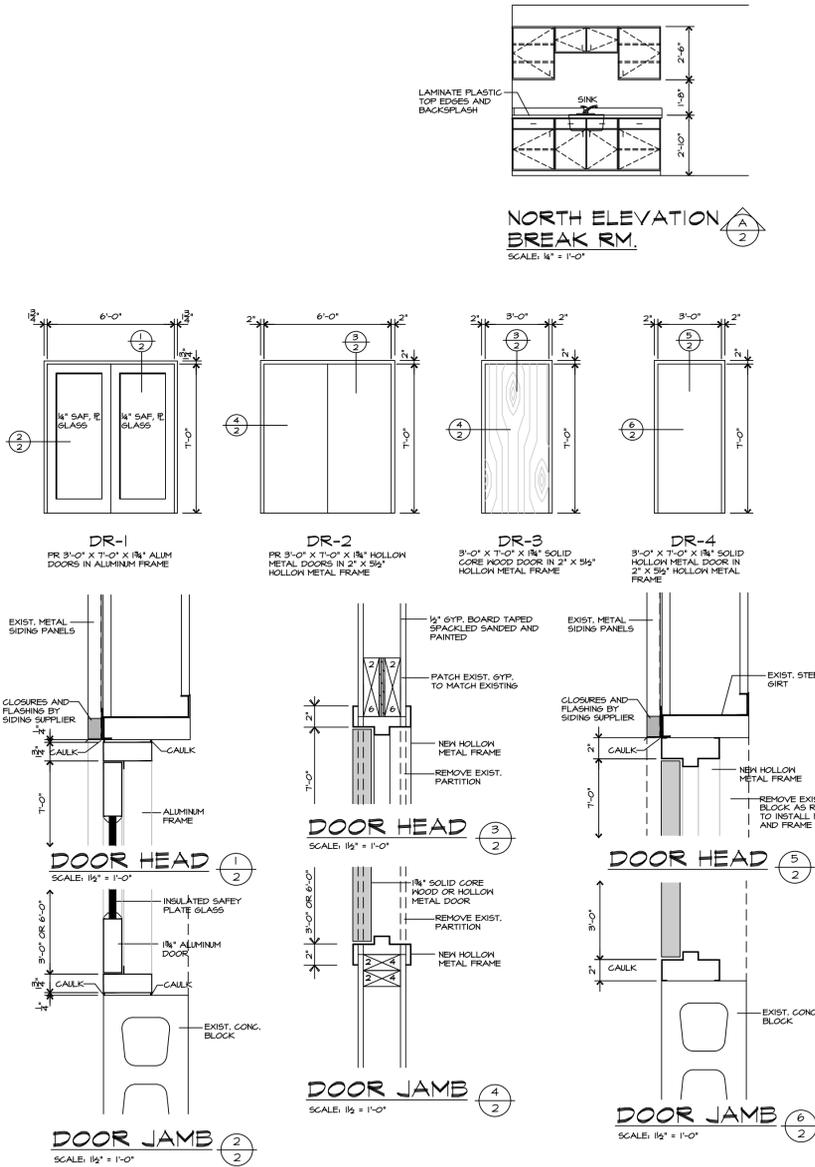
PHONE (269) 377-3561

PROJECT NO.  
 2333

DATE  
 12/12/23

REVISION

SHEET  
**2**  
 (49)



**MEZZANINE FLOOR PLAN**  
 SCALE: 1/4" = 1'-0"



NOTE:  
 VERIFY ALL DIMENSION  
 IN THE FIELD

## Otis - Portage, MI - Business Summary

### **Hours of Operation**

Monday – Friday 7:30 AM -4:00 PM

Saturday: Closed

Sunday: Closed

### **Otis Lines of Business**

- Service and Repair
  - The Otis Portage branch has 249 elevator maintenance contracts in the local area. Otis provides maintenance services, annual inspections and repair services to these customers.
- New Equipment
  - The new equipment business line installs elevators in new construction buildings in the local area.
- Modernization
  - The modernization business line replaces elevator pumps/ motors and refurbishes the elevator systems and cabs at the end of its life span.

### **Typical Business Day**

**Field Employees** (Service/ Repair, New Equipment, Modernization mechanics and their supervisors): 10 employees

A typical business day includes a delivery driver picking up parts and tooling from the storage/ warehouse at the Otis location in the morning operation hours. Most elevator mechanics will not go to the Otis location as all mechanic's vehicles are stocked with tooling. The driver will deliver parts to the job sites. Otis will have field/ safety training for their employees 1 time per month at the office location.

**Office Employees** (Sales): 2 Employees

Otis encourages a hybrid work model. The sales employees work out of the office 2-3 days per week. Regional employees will periodically work from this office 2-3 days per month. The office will be outfitted with 4 shared desks for sales managers, field supervisor, and regional management.

### **Deliveries**

This location will receive deliveries via UPS/ FEDEX daily and will receive larger cargo deliveries less frequently, estimated once or twice per week.

March 30, 2024

Eric Feldt

Dear Neighbor,

I hope this letter finds you well. My name is Charlie Eldred, and I am the owner of the building located at 9651 Portage Rd. I am writing to inform you of some upcoming changes to the property that may affect you as a neighboring property owner.

I am in the process of renting a portion of the building to the Otis Elevator Company. Their intention is to utilize the space as both an office and a storage location for parts and materials related to their business operations. However, it has come to my attention that this proposed usage does not align with the current zoning requirements set forth by the City of Portage.

To address this issue, we are taking the necessary steps to obtain a special land use permit from the City of Portage. This permit would grant permission for the intended usage by Otis Elevator Company. As part of this process, we are required to hold a meeting to provide an opportunity for residents, like yourself, to voice any questions or concerns they may have regarding this proposed change.

Therefore, I would like to invite you to attend a meeting at the property located at 9651 Portage Rd on April 6, 2024. There will be two sessions available: one from 9:00 am to 10:00 am and another from 4:00 pm to 5:00 pm. During these sessions, you will have the opportunity to meet with the property owners and address any inquiries or express any concerns you may have regarding this matter.

Should you have any questions or require further information prior to the meeting, please do not hesitate to contact me via email at [charlie.eldred@outlook.com](mailto:charlie.eldred@outlook.com) or by phone at 269-998-0970. I am available to discuss any concerns you may have and provide clarification as needed.

Thank you for your attention to this matter, and I look forward to the possibility of addressing any concerns you may have at the upcoming meeting.

Sincerely,



Charlie Eldred  
KCH Ventures LLC

## Eric Feldt

---

**From:** Charlie Eldred <charlieeldred3392@gmail.com>  
**Sent:** Friday, April 19, 2024 2:19 PM  
**To:** Eric Feldt  
**Subject:** Re: 9651 Portage Rd/OTIS Elevator

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

### CAUTION: THIS EMAIL IS FROM AN EXTERNAL SENDER

Do not click on links or open attachments unless this is from a sender you know and trust.

Thanks for the response, Eric

-The owners of 9599 portage rd came to the second session. They said they had no problems with our proposed use.

-Michael Kasten (owner of 9650 portage rd) called and said he also did not have any issues.

5/16 at 7pm is good with me.

Charlie Eldred

269 998 0970

Get [Outlook for iOS](#)

---

**From:** Eric Feldt <feldte@portagemi.gov>  
**Sent:** Friday, April 19, 2024 11:41:22 AM  
**To:** 'Charlie Eldred' <charlieeldred3392@gmail.com>  
**Subject:** RE: 9651 Portage Rd/OTIS Elevator

Charlie,

I wanted to give you an update before heading into the weekend. With the information you provided, we are anticipating taking this project to the next available Planning Commission meeting on Thursday, May 16<sup>th</sup>.

We are still waiting for the review of your documents to be completed by other City Departments. Their review will assist us in determining the Site Plan fee. Hopefully, this can be completed by Monday.

For the neighborhood meeting you held, how many people showed up and did you receive any comments?

For the May 16<sup>th</sup> meeting, it starts at 7pm and located in the Council Chambers inside City Hall. I recommend you attend this meeting, in case the Planning Commission has any questions. Does this date, time work with your schedule? If not, then next available meetings are June 6 and June 20.

Sincerely,

Eric Feldt, AICP, CFM  
City Senior Planner  
Department of Community Development  
City of Portage, MI  
Ph: (269) 329-4471  
Email: [feldte@portagemi.gov](mailto:feldte@portagemi.gov)  
[City of Portage – Zoning Code](#)

---

**From:** Charlie Eldred <[charlieeldred3392@gmail.com](mailto:charlieeldred3392@gmail.com)>  
**Sent:** Thursday, April 11, 2024 1:02 PM  
**To:** Eric Feldt <[feldte@portagemi.gov](mailto:feldte@portagemi.gov)>  
**Subject:** Re: 9651 Portage Rd/OTIS Elevator

**CAUTION: THIS EMAIL IS FROM AN EXTERNAL SENDER**  
Do not click on links or open attachments unless this is from a sender you know and trust.

Here is an image showing the proposed larger exterior door.

On Thu, Apr 11, 2024 at 8:27 AM Charlie Eldred <[charlieeldred3392@gmail.com](mailto:charlieeldred3392@gmail.com)> wrote:

UPDATES

2B- Sent to tenant for more information

2D- Uploaded this with the application yesterday but I will attach another copy here of both the original site plan from 2003 as well as the proposed changes.

2E- The only exterior change is a wider entry door into the middle unit and a concrete ramp from the parking lot up to this door. This information is on the print.

2F- Paid

3A- Attached

3B- Existing conditions shown in attached drawing. Changes shown in attached drawing

3B i- This will not be possible. The pet store is a retail business that relies HEAVILY on visibility from the street. Any obscuring the view of their store from the road will have a severe negative impact on their business.

3C- When will I know this amount?



On Thu, Apr 11, 2024 at 7:57 AM Eric Feldt <[feldte@portagemi.gov](mailto:feldte@portagemi.gov)> wrote:

Charlie,

From my 3/5/24 email, please send me the highlighted items to proceed with completing a Special Land Use Permit and Site Plan applications. The fee can be paid either [online](#), in person (at City Hall – cash, card, or check), or mailed check.

#### Next Steps

1. Hold Neighborhood Meeting. Please hold the meeting in a nearby setting and invite all adjacent owners.  
**DONE**
2. File Special Land Use Permit by submitting the following:
  - a. [Special Land Use Permit Application](#) **DONE**
  - b. **Detail business summary – Greater detail than in the email below (reference to your 3/1/24 email). State hours of operation, typical business day, types of service calls, summary of products for sale, etc.**
  - c. Floor Plan – We already have this. **DONE**
  - d. **Site Drawing. I haven't tracked down a good copy on our records. Please submit this – which is also required for the 'Site Plan' submitted indicated below.**
  - e. Show and state any proposed exterior changes.
  - f. [Fee: \\$374](#)
3. File Site Plan by submitting the following:
  - a. [Development Review Application](#)
  - b. **Existing and Proposed site conditions such as parking, access, building location, lighting, etc.**
    - i. **One site item that will be required is the need for additional trees/ bushes to be planted along Portage Rd as to be consistent with current landscaping requirements under Sec 42-572(A).**
  - c. **Fee: The fee will be based on how much changes to the site may be needed.**

Items 2 & 3 can be submitted together. Please email me those items.

Thank you,

Eric Feldt, AICP, CFM  
City Senior Planner  
Department of Community Development  
City of Portage, MI  
Ph: (269) 329-4471  
Email: [feldte@portagemi.gov](mailto:feldte@portagemi.gov)  
[City of Portage – Zoning Code](#)

---

**From:** Charlie Eldred <[charlieeldred3392@gmail.com](mailto:charlieeldred3392@gmail.com)>

**Sent:** Tuesday, April 9, 2024 3:16 PM

**To:** Eric Feldt <[feldte@portagemi.gov](mailto:feldte@portagemi.gov)>

**Subject:** 9651 Portage Rd/OTIS Elevator

**CAUTION: THIS EMAIL IS FROM AN EXTERNAL SENDER**

Do not click on links or open attachments unless this is from a sender you know and trust.

Hi Eric, the neighborhood meeting went well this weekend (missed you out there)

Special use permit is submitted.

As far as I can tell, the site plan has been submitted (by the contractor Kalleward Group) and approved by Brady.

Any chance we can hear this at an earlier meeting than 5/16/24?

--

Charlie Eldred

p:(269) 998-0970

CONFIDENTIALITY: Pursuant to the Electronic Communications Privacy Act of 1986, 18 U.S.C. Sec. 2510, et seq. (the "ECPA"), notice is given that the information or documents in this electronic message are legally privileged and confidential information, intended only for the use of the individual or entity to whom it is sent. If you are not the intended recipient, please be aware that any disclosure, distribution, use or copying of the contents of this message is prohibited. If you have received this message in error, notify the sender immediately by return mail or contact [helpdesk@portagemi.gov](mailto:helpdesk@portagemi.gov) and delete this message and any attachments from your system. Thank you.

--

Charlie Eldred

p:(269) 998-0970

--

Charlie Eldred

p:(269) 998-0970

CONFIDENTIALITY: Pursuant to the Electronic Communications Privacy Act of 1986, 18 U.S.C. Sec. 2510, et seq. (the "ECPA"), notice is given that the information or documents in this electronic message are legally privileged and confidential information, intended only for the use of the individual or entity to whom it is sent. If you are not the intended recipient, please be aware that any disclosure, distribution, use or copying of the contents of this message is prohibited. If you have received this message in error, notify the sender immediately by return mail or contact [helpdesk@portagemi.gov](mailto:helpdesk@portagemi.gov) and delete this message and any attachments from your system. Thank you.

## Staff's Pictures April 29, 2024



Figure 1. Looking east across Portage Road toward subject site 2-story commercial building.



Figure 2. Looking north along Portage Road and subject commercial building.



Figure 3. Looking north toward entrance of subject commercial building and parking lot.



Figure 4. Looking east on driveway entrance toward parking lot (left) and on-site dog park (right).



Figure 5. Looking south toward Portage Road and on-site dog park 'Lincoln Dog Park'.

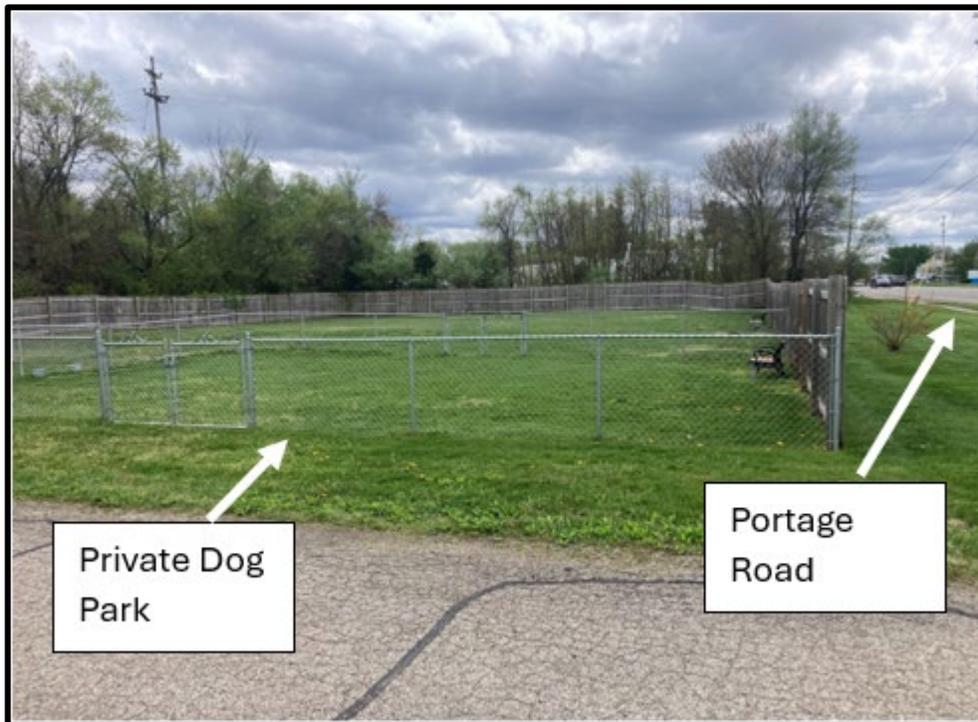


Figure 6. Looking south toward on-site dog park.

**TO:** Planning Commission

**DATE:** May 16, 2024

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** Lakefront Property Housing Code Ordinance Amendment #23/24-5: Ordinance to amend City of Portage Chapter 24 Article 5 Safety, Sanitation, and Health; and Chapter 42 Land Development Regulations relating to the Lakeshore Housing Project.

## **I. INTRODUCTION:**

### Project Update Summary

This report supplements the April 18, 2024, City report (see attachment) following the discussion of this topic during the April 18, 2024 Planning Commission meeting. During this meeting, City staff presented the Lakefront Property Housing Codes update including summarizing lakeshore development history, past zoning changes, current zoning standards, public input received from a recent survey and open house, current development challenges, and concluded with suggested changes to development regulations. The Planning Commission and general public provided various comments on the suggested changes (see attachments). These comments are summarized below.

#### Public Comment

Comments include: generally supports staff's suggestions, concerns with drainage from driveways, opposes artificial grade raising, wanting to keep existing large driveways, and concerns with stormwater impacting lake water conditions.

#### Planning Commission

Comments include: concerns about stormwater impacting lake conditions, how to address existing large driveways, and support for pervious/ alternative pavement elements.

### Project Schedule

The City has outlined a 6-month process (see attachment) to help address future development challenges on lakefront properties and lessen resulting impacts from the built environment. We are currently halfway into this process. Adoption of new development codes is expected in Summer 2024.

### Ordinance Amendment Process

The ordinance amendment process involving zoning code amendments is provided under Section 42-651 and requires a public hearing during Planning Commission and City Council meetings. The Commission will consider the proposed amendment, evaluate it for consistency with adopted City plans, impact on neighborhood conditions, and provide a recommendation to City Council. After a public hearing during a Council meeting, the Council will vote on the ordinance amendment.

### Planning Commission Consideration

Based on the four areas of suggested changes from the April 18, 2024 City report, staff has provided proposed corresponding code changes.

## **Concerns about lake viewsheds from adjacent development**

### **Current Regulations**

- Sec 42-350: House Height Maximum: 2 stories; 30' (measured to roof mid-point)
- Sec 42-350: Rear Yard (lakefront) Setback (R-1A): 40'
- Sec 42-350 Footnote (B)(2)(b): Substandard lot size reduces rear yard setback 30.'
- Sec 42-121(B)(2): Accessory building height max. in rear (lake) yard area: 8'
- No prohibition of artificially raising grades for future housing.

### **Suggested Changes**

- 1a) Remove rear yard setback reduction for substandard lot size. Keep 40' for all lots. OR,
- 1b) Create a new averaging for rear yard setback purposes. Use an average of the three closest homes along the lakeshore, AND
- 2) Reduce front yard setback from 25' to 20' for nonconforming lot size. AND
- 3) Prohibit raising of grades which artificially increases overall grade and house height.

### **Proposed Corresponding Code Changes**

- Add term Sec 42-350 (B)(2)(b) 'For non-lakefront lots...'
- Add new Sec 42-350 (B)(2)(c) 'For lakefront lots with less than the required area, setbacks shall not be less than the following:'
  - i. 40 feet to rear lot line.
  - ii. 20 feet from front lot line.
- Add to 'Building Height' definition in Sec 42-112. *Height calculations shall disregard any fill or construction which the director finds to have no significant purpose other than elevating the grade. In reaching such finding, the director shall consider only those architectural, structural, safety, aesthetic, access or other purposes claimed by the developer and supported by reasonable evidence.*

## **Desire flexible setbacks and locations for detached garages**

### **Current Regulation**

- Sec 42-350: Following setbacks apply to detached garages, sheds:
  - Front: 25' – 27' (depends on conforming lot size)
  - Sides: 5' - 8' (depends on conforming lot width)

### **Suggested Changes**

Where garage does not meet the front yard setback:

- 1) can be rebuilt located at least 20' away from the front property line (ROW) if garage doors face the street. Or
- 2) can be rebuilt located at least 15' away from the front property line (ROW) if garage doors do not face the street.

### **Proposed Corresponding Code Changes**

- Modify Sec 42-133(D) to state:
  - Notwithstanding any other standards in this section, within one year of demolition an existing residential detached garage which does not meet the front yard setback may be rebuilt if it faces the street and is located at least 20' away from front property line (ROW) or at least 15' from the front property line (ROW) if it does not face the street.

## **Desire larger driveway**

### **Current Regulation**

Sec 24-111 *Definition* – ‘Improved Driveway’ consists of uniform surface (asphalt, concrete, or gravel).

Max. average width: 24’ for lot width  $\leq$ 100’

Max. average width: 30’ for lot width  $\geq$  100’

Staff notes that very few lakefront lots are 100’ wide.

### **Suggested Changes**

Allow alternative material: pervious pavers or blocks. AND

Require submittal of drainage plan; not required engineering drawing. AND

Create new 30’ max average width regardless of lot width but not more than 50% coverage – for standard lot widths.

### **Proposed Corresponding Code Changes**

Move Sec 24-111 *Definition* – ‘Improved Driveway’ to Sec 42-112 Definitions but only state the following:

Improved driveway means a private road of ingress and egress with a uniform surface of concrete, asphalt, porous pavement or pavers, or gravel which leads from a curb cut to a garage, carport or accessory parking space. An improved driveway may include a surfaced turnabout area, a circular driveway or an accessory parking space.

Relocate the dimensional driveway standards 1 & 2 from Sec 24-111 ‘*Improved Driveway*’ definition to Sec 42-128 and create a new subsection (E) to state the following:

Driveways shall meet all the following:

Max. average width: 30’.

Does not occupy in excess of 50 percent of the area included in the front yard for standard lot width.

Between 50 and 60 percent area, professional engineer drainage plan is required.

Drainage plan to guide stormwater to a natural waterbody or an approved stormwater infrastructure.

### **Desire flexible yard setbacks**

#### **Current Regulation:**

Sec 42-350: The following setbacks apply to houses and attached garages.

Front (Street facing): 27’ (or 25’ for nonconforming lot size)

Front – also applies to detached garages.

Sides: 8’ (or 5’ for nonconforming lot widths)

Rear (lakeside): 40’ (or 30’ for nonconforming lot size)

#### **Suggested Changes**

For substandard lot size or width (either scenario):

Reduce front yard setback from 27’/ 25’ to 20’. AND

Remove 30’ rear yard reduction for nonconforming lot size; keep 40’ rear yard setback.

#### **Proposed Corresponding Code Changes**

See No. 1.

## **II. BACKGROUND INFORMATION:**

### **Public Noticing Requirement**

Public notice was published in the Kalamazoo Gazette on April 30, 2024. As of the date of this report (Thursday, May 9), no public comments have been received.

## **III. ANALYSIS:**

## **IV. RECOMMENDATION:**

If no additional public comment is requested by the Commission, and the Commission is supportive of the proposed ordinance amendment, staff advise the Planning Commission to recommend to the City Council the approval of the Lakefront Property Housing Code Ordinance Amendment #23/24-5: Ordinance to amend City of Portage Chapter 24 Article 5 Safety, Sanitation, and Health; and Chapter 42 Land Development Regulations relating to the Lakeshore Housing Project.

### **Attachments:**

1. 4.18.24 Report, Lakefront Property Housing Codes Material
2. Final Planning Commission Minutes 4.18.2024
3. Lakefront Property Project Process

**TO:** Planning Commission

**DATE:** April 18, 2024

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** Lakefront Property Housing Codes

## **I. INTRODUCTION:**

### Project Summary

The City has initiated a process to reexamine development regulations pertaining to lakefront properties around Long Lake, Austin Lake, West Lake, and Gourdneck Lake. Due to the unique character and historical construction pattern of lakefront properties, it can be difficult for property owners to develop or redevelop in a way that meets their needs while meeting current land use regulations. This memo provides a summary of historical background and development issues, a summary of received lakefront property owner comments, and suggestions to help address common issues raised.

### Project Schedule

The City has outlined a 6-month process to help address future development challenges on lakefront properties and recommend potential improvements. This process is posted on the City's website: <https://www.portagemi.gov/923/Lakefront-Property-Housing-Codes-Study> and we are currently halfway into this process. Adoption of new development codes is expected in Summer 2024.

## **II. BACKGROUND INFORMATION:**

### Development History

Properties along Long Lake, Austin Lake, West Lake, and Gourdneck Lake have been highly desirable for many decades. These lakefront properties have been used as second homes or cottages, and increasingly becoming year-round residences. During the mid-1960s when Portage became a city, many of the lakefront areas were already developed with homes. Decades later, as economic growth, employment, and education services increased in the greater Kalamazoo area, most of the lakes' shorelines became more developed. With perimeter ring roads and favorable zoning, lakefronts become fully developed with housing. Over time, older cabins and original homes were replaced with larger modern homes.

Fitting modern homes on lakefront properties was a challenge. Most properties were established prior to the mid-1960s, under different land use regulations. Many were narrow and deep, offering each owner a small slice of lakefront access and view. Many of these lots do not meet the current zoning district minimum lot size and lot width. This resulted in highly valued lakefront properties becoming a compact living environment. This lakefront housing trend can be found throughout many lakes in southern areas of the state.

### Narrow Lots, Compact Development

As development occurred on these narrow lakefront lots, owners could not meet current zoning requirements and sought flexibility through variances. Past variances were approved for reducing certain yard setbacks to accommodate a new house or an expansion and for rebuilding an old garage close to the street or property lines.

#### Past Changes to City Zoning Standards

Over time, the City adjusted various zoning standards based on received public comments and past certain approved variances. The following lists the efforts for One-family residential zoning district parcels:

- 1987: This allows existing and new accessory structures in lakeside yards up to 8' in height, no greater than 80 square feet in size, and closer to a side and rear lot line (shoreline) than that required for a house. This continues to be provided in the current zoning code.
  - 1965: (overturned in 1987-see above) Required lakeshore properties to maintain an open unobstructed yard (i.e. No allowed for new accessory structures/fences in lakeside yard (facing lake)). Existing structures allowed. "Boat wells" might be allowed with an approved variance.
- 1986: Zoning code interpretation – allowed uncovered porch or deck attached to the house may be located as close as 30' from lakeside property line (shoreline). This continues to be provided in the current zoning code.
- 2001, 2003: Prohibition of creating new water body access for non-lakefront owners or occupants (i.e. keyhole) in any zoning district. This continues to be provided in the current zoning code.

In 2019, the City provided another update to reduce restrictions in the One Family Residential zoning districts prompted in part by interest in building new housing on lakefront (these continue in current zoning code):

- Increased Lot Coverage from 25% to 30%

Lot coverage only applies to buildings; not to driveways, patios, sidewalks, etc.

- Increased maximum house height from 24' to 30'.

This is measured from grade up to mid-point of the roof.

- Increased Accessory Building height from 14'-16'.

No increase for the rear lakeside yard area.

Accessory Structures for this item include detached garages, pole barns, sheds, etc.

- Increased Accessory Structure maximum size for two-story homes to include 50% of the 2nd story home area.

Larger lot (2-acre min.) owners may seek Planning Commission approval for larger building sizes. May not exceed 20% rear yard area (not applicable to lake front yards).

### Zoning Code Flexibility

In addition to the past zoning code changes loosening development standards, the code provides other forms of additional flexibility: 1) variance process, and 2) non-conforming lot size/ width. Both are detailed below:

#### 1. Variances.

The zoning code allows a property owner to seek a variance to relieve zoning standards when the subject lot has unique factors, i.e. topographic challenges, wetlands, boulders, or other factors inheriting the property. The variance is a zoning element and does not relieve or change any building code standards. In the past, variances have been filed and approved on lakeshore lots due to smaller lot sizes/ lot width, topography, and reconstructing historic home or garage with existing footprints. The majority of past variances can be categorized into three common requests: 1) reduce the front yard setback for detached garages, 2) reduce side yard setbacks for homes, and 3) increase lot coverage for building footprints. With the development flexibility listed above under Past Changes to City Zoning Standards and Zoning Code Flexibility, fewer variances have been sought in recent years. However, many historic detached garages are located very close to front lot lines (street right-of-way) and continue to be an important component for lakefront owners.

#### 2. Nonconforming lot size/ width.

Most lakefront parcels were established and developed decades ago under different zoning codes and during the Portage Township period. As new codes were adopted, new minimum lot sizes and lot widths were established, resulting in many pre-existing lakefront parcels and non-lakefront parcels no longer meeting current size dimensions. Therefore, these parcels have become legally non-conforming in size and/ or lot width. Generally, they are still buildable but their smaller size or narrowness may not easily meet current zoning setbacks. To alleviate this challenge, the zoning code provides an automatic reduction of certain yard setbacks in the One-family Residential zoning districts. This reduction does not require seeking a variance nor additional permits. Further, it applies to all lakefront and non-lakefront parcels. These revised building standards for substandard lots continues to be provided in the current zoning code. The majority of lakefront lots are zoned R-1A One Family Residential. Of the 839 lakefront lots (Long Lake, Austin Lake, Gourneck Lake, and West Lake (Portage lots only)), 411 lots are substandard in lot width and 142 lots are substandard in lot size. Therefore, 65% of all lakefront lots have a substandard lot dimension. These lots have their following yard setback reduced:

##### I. Parcels with Substandard Lot Width:

- i. Side yard setback is reduced from 8' to 5'.

## II. Parcels with Substandard Lot Size:

- i. Front yard setback is reduced from 27' to 25', and
- ii. Rear yard setback is reduced from 40' to 30' (rear setback measured from shoreline).

Additionally, the zoning code allows common house features such as decks, covered porches, eaves, steps, sidewalks, etc. to project (without variances) into yard setbacks. Please see the attached details of R-1A regulations regarding yard setbacks and lot dimensions.

### **Data Collection**

Data collected for this re-examination of local development regulations project consisted of input received from lakefront owners and internal City data below.

#### Lakefront Community Outreach

The City contacted lakefront owners for their issues about any past or present development (or redevelopment) projects, any impacts they may have from adjacent lakefront construction, and any re-occurring or seasonal issues. The City initiated two initial public feedback methods consisting of a survey and an open house meeting. Details are listed below.

1. Survey. The City provided a survey from late December 2023 to late January 2024 asking all lakefront property owners (827 total) various questions pertaining to certain zoning regulations such as maximum height for a house or lakefront accessory building, maximum driveway dimensions, vehicular parking needs, and more. Open-ended questions were also asked, such as what the most important issue as a lakefront owner is, should there be specific lakefront code provisions, and more. Over 120 participants completed the survey. See attached survey results.
2. Open House. In addition to the survey, the City sought more open-ended dialogue with all the lakefront property owners by hosting an open house on January 16, 2024, at the Portage Lakeview Elementary School. Staff asked various questions about some main site elements, such as any concerns about construction/ rebuilding their house, garage, driveway, or any other part of their property. With over 80 people in attendance, residents were able to directly express to the City their concerns. These concerns have also been summarized in this memo.

Below is a brief summary of the results of overall community data:

- Majority of the participants are located on Austin Lake.
- The majority found existing zoning regulations to meet their needs.
- Slight majority supports additional code provisions.
- Some of the most common and important issues for lakefront owners:
  - Concerns about impacting lake views from adjacent development.
  - Desire for larger driveway
  - Wanting flexible setbacks for houses, accessory buildings.
- Most common listed concerns include:

- Preserving lake and side viewsheds due to being impacted by adjacent development,
  - Artificial raising of property for new homes,
  - Desire flexible setbacks and locations for detached garages,
  - Drainage issues, and
  - Larger driveway size.
- Many comments related to non-City of Portage regulations such as boat noise, lake weed issues, high boat speeds, etc. These topics are not the subject of this code review.
  - Some comments related to City of Portage regulations but not zoning, such as property maintenance concerns, loud lake house parties, etc. These are currently addressed outside zoning regulations.

#### City Staff Data

Staff's records indicate that as zoning standards (as noted above) have been adjusted overtime, fewer variances have been sought. And previously physically challenging vacant lots have been developed. However, some common challenges continuing to be heard from non-lakefront and lakefront owners are driveway sizing issues, and older detached garages on lakefront properties. Staff used the City's Fetch GIS mapping tool to determine the following estimates about driveways and garages on lakefront properties.

- Driveways
  - Average Maximum Width: 32'
  - Average Length: 59.8'
  - Average Size: 1,400 sq ft
  - Driveway Material: 59% Concrete; 12.3% Asphalt
  - Average front yard driveway coverage: 36.8'
- Garage
  - Average distance between Garage and Street: 43'
  - Garage Type: 52% Detached; 42% Attached.

### **III. ANALYSIS:**

#### Comparing Data, City Regulations

Staff reviewed and compared community outreach data, staff's data, existing regulations, and the built environment. As stated earlier, many past code changes have been made to facilitate development. Lakefront property owners have benefited from these changes. However, based on recent community feedback and the built environment, additional review and changes of these regulations may be needed.

### Planning Commission Consideration

Below is a synthesis of the issues for the Planning Commission to review and consider. Input from the public and the Planning Commission is being sought on these topics before staff initiates any draft code amendment drafting.

#### **1. Concerns about lake view sheds from adjacent development**

- Public Comments: New or renovated homes are often built closer to the shoreline than the previous house and are often taller than adjacent homes. This can block a portion of lakefront views and can create a boxed-in and shadow affect for existing adjacent homes. Some new homes are placed on new fill, raising the overall land and house.
- Staff Comments: Lakeviews are generally maintained through rear (lakeside) setbacks from all lakeshores, but small (nonconforming) lots may build closer to lakeshore than conforming lots (without variance). Building tall homes adjacent to shorter homes is allowed so long as height maximums are followed. Exceeding this height is rarely requested. But some new homes have artificially raised grades (using imported soil), resulting in overall taller heights being significantly taller than adjacent homes. Accessory buildings in the lakefront yard are capped at a low height and overall size.
- Related existing codes

Sec 42-350: House Height Maximum: 2 stories; 30' (measured to roof mid-point)

Sec 42-350: Rear Yard (lakefront) Setback (R-1A): 40'

Sec 42-350 Footnote (B)(2)(b): Substandard lot size reduces rear yard setback from 40' to 30.'

Sec 42-121(B)(2): Accessory building height max. in rear (lake) yard area: 8'

No prohibition of raising grades.

- Suggested Changes
  - i. Remove rear yard setback reduction for substandard lot size. Keep 40' for all lots. OR,
  - ii. Create a new averaging for rear yard setback purposes. Use an average of nearby homes along the lakeshore, AND
  - iii. Reduce the front yard setback from 25' to 20' for nonconforming lot size. AND
  - iv. Prohibit raising of grades which artificially increases overall grade and house height.

#### **2. Desire flexible setbacks and locations for detached garage**

- Public Comments: Request to rebuild existing detached garages located close to a street or side property line. There were a handful of comments requesting to have a garage across the street to provide more parking and storage.
- Staff Comments: Many detached garages exist within front (street side) yard and side yard setbacks. Rebuilding these garages is not allowed due to not meeting yard setbacks. Owners often wish to rebuild a garage on the same footprint. Some garages are partially located in City ROW. Some leave little room to safely navigate into/ out of the street. Some driveways'

entrances into the garage are located too close to the street, resulting in parked cars partially in City ROW. Garages cannot be allowed as the only building on a residentially zoned property. There needs to be sufficient setback distance for existing and future utility and infrastructure purposes. Many lakefront streets are very narrow and cannot easily accommodate utility access. The result of these conflicts is that there are many old grandfathered garages left near the street adjacent to entirely new houses.

- Related existing codes

Sec 42-350: Following setbacks apply to detached garages, sheds:

Front: 25' – 27' (depends on conforming lot size)

Sides: 5'- 8' (depends on conforming lot width)

- Suggested Changes

- i. For a substandard lot width where a garage faces the street and is located at least 20' away from the front property line (ROW): may rebuild the garage using the same footprint. AND
- ii. For standard lot width where a garage is located at least 15' away from the front property line (ROW): may rebuild the garage with the same footprint, but garage doors cannot face the street.

### 3. Desire larger driveway

- Public Comments: The Majority of survey participants indicated the need for 4+ parked vehicles. Overall, community data indicated a desire for a larger driveway.
- Staff Comments: Driveways are often the width of a 2-car garage (approx. 16') which is often too narrow to park two cars. Many choose to widen the driveway to fit more cars to the maximum allowed width (non-lakefront & lakefront). However, many lakeshore lots are narrow and substandard in lot width, which creates challenges to accommodating parking demand while meeting maximum driveway standards. Many lakeshore owners (or past owners) have expanded their driveways to accommodate their parking needs. But, in many cases, they have exceeded the maximum driveway size without permits. When these non-permitted large driveways need replacing, it is not allowed unless they are reconstructed to an allowed size. Most owners prefer not to reduce the driveway area. Further, these large driveways can create drainage issues on the adjacent street or adjacent property.
- Related existing codes:

Sec 24-111 Definition – 'Improved Driveway' consists of uniform surface (asphalt, concrete, or gravel).

Max. average width: 24' for lot width  $\leq$  100'

Max. average width: 30' for lot width  $\geq$  100'

Staff note that very few lakefront lots are 100' wide.

- Suggested Changes

- i. Allow alternative material: pervious pavers or blocks. AND

- ii. Require submittal of drainage plan; not required engineering drawing. AND
- iii. Create new 30' max average width regardless of lot width but not more than 50% coverage – for standard lot widths.

#### 4. Desire flexible yard setbacks

- Public Comments: Based on received community data, there was support for more flexible yard setbacks to provide greater development site location options. This item was also related to an accessory building (see above). Although flexible yard setbacks are supported, it may conflict with received support of preserving lake views.
- Staff Comments: Based on past code revisions and automatic reductions for substandard lot dimensions, fewer yard setbacks variances have been filed. The majority of lakefront lots are substandard in size or lot width and, thus, already have reduced yard setbacks. Two abutting properties may have different yard setbacks if one is substandard in size or lot width, which causes confusion with the other abutting property owner. In cases of unique topography or significant substandard lot dimensions, variances would likely be filed if setbacks cannot be met.
- Related existing codes:

Sec 42-350: The following setbacks apply to houses and attached garages.

Front (Street facing): 27' (or 25' for nonconforming lot size)

Front – also applies to detached garages.

Sides: 8' (or 5' for nonconforming lot widths)

Rear (lakeside): 40' (or 30' for nonconforming lot size)

- Suggested Changes
- For substandard lot size or width (either scenario):
- Reduce front yard setback from 27'/ 25' to 20'. AND
- Remove 30' rear yard reduction for nonconforming lot size; keep 40' rear yard setback.

The above code dimensions and suggested changes are also included in the attached tables showing development regulations for the Planning Commission review and discussion.

#### IV. RECOMMENDATION:

##### Attachments:

1. Lakefront Property Project Process
2. Survey Results
3. List of R-1A Standards
4. Open House Comments



## Lakefront Property Project Process

### **Goal**

Amend City codes to address unique needs of lakefront properties and reduce code non-conformities.

### **Issues**

Learn about lakefront development and reinvestment issues from lakefront owners, and evaluate problems from observed/documentated issues.

### **Address Issues**

Establish amended zoning and other code changes.

### **Overall Project Timeframe**

Approx. 6-7 months (January – July 2024)

### **STEP 1) Identify the problems/ issues – Data Collection**

#### **Timeframe: 1 month (January)**

- Survey & Open House – Collect, identify issues
  - Survey: Active for 3 weeks, expires Monday, January 22<sup>nd</sup>
  - Open House: Tuesday, January 16, 2024

### **STEP 2) Data Analysis**

#### **Timeframe: 1 month (February)**

- Data compilation, fact gathering and review

### **STEP 3) Draft Solutions/ Alternatives**

#### **Timeframe: 1-2 month (March-April)**

- Analyze and identify solutions, alternatives

### **STEP 4) Present draft solutions to Public[WE ARE HERE]**

#### **Timeframe: Meeting (estimated to be in April)**

- Planning Commission Public Workshop meeting - share results of data, draft solutions to issues, obtain commission and public feedback.
- Present zoning- and non-zoning related changes.



**STEP 5) Present Draft Ordinance to Planning Commission, Public Hearing**

**Timeframe: Meeting (estimated to be in May)**

- Planning Commission Public Hearing – Commission recommends draft ordinance that reflects April feedback to City Council.

**STEP 6) City Council Meeting – Schedules Ordinance to future Council meeting.**

**Timeframe: Meeting (estimated to be in June)**

- Present Zoning and non-zoning code changes to City Council.

**STEP 7) City Council Public Hearing, Vote on Ordinance**

**Timeframe: Meeting (estimated to be in July)**

- City Council holds public hearing and votes on zoning, non-zoning code changes.

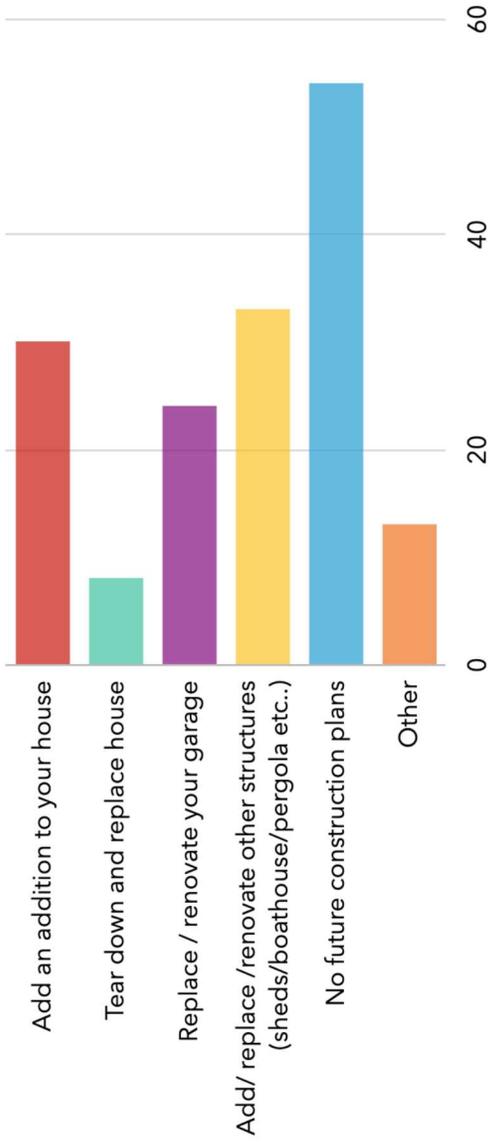


# Survey of Lakefront Housing Codes

March 1, 2024

## Future construction plans

Column Bar



[Hide table](#)

Other response  Empty categories

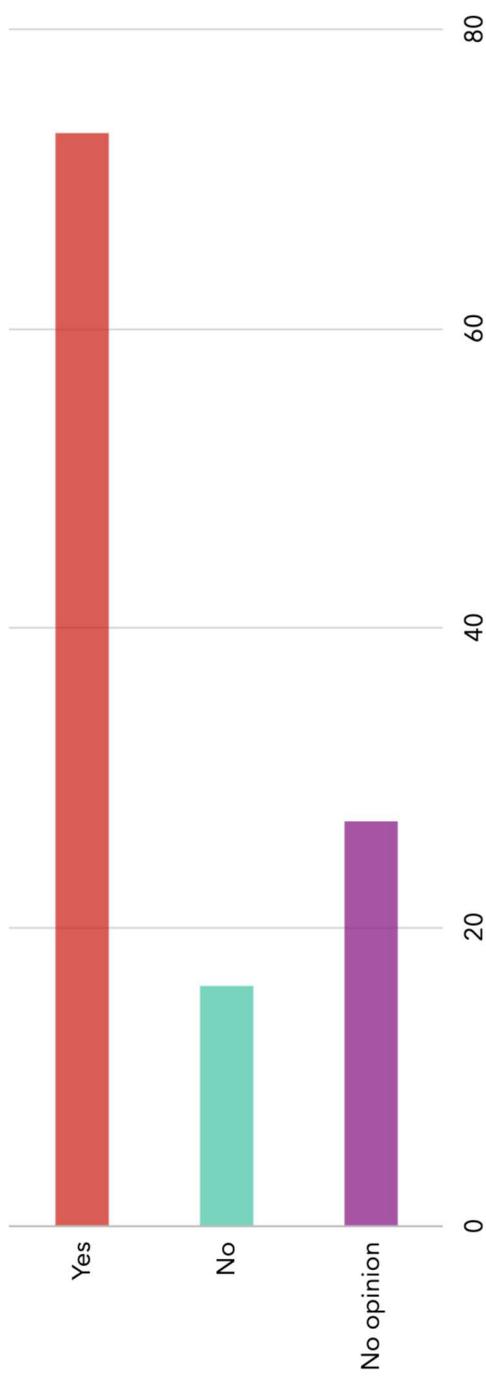
Answers	Count	Percentage
Add an addition to your house	30	24.79%
Tear down and replace house	8	6.61%
Replace / renovate your garage	24	19.83%
Add/ replace /renovate other structures (sheds/boathouse/pergola et c..)	33	27.27%
No future construction plans	54	44.63%
Other	13	10.74%

Answered: 117 Skipped: 4

## Structure height

- Does the house 30' maximum height requirement meet your needs?

Column Bar Pie Map



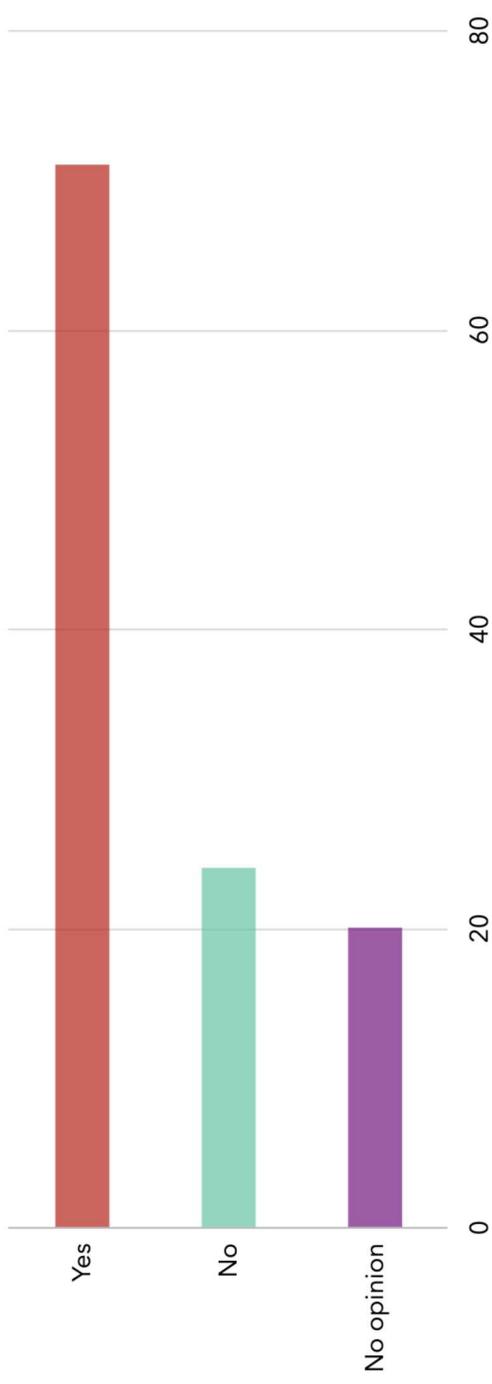
[Hide table](#)

Empty categories [Sort](#)

Answers	Count	Percentage
Yes	73	60.33%
No	16	13.22%
No opinion	27	22.31%

Answered: 116 Skipped: 5

● Does the 16' maximum detached accessory building height for front and side yards meet your needs? Bar Pie Map



[Hide table](#)

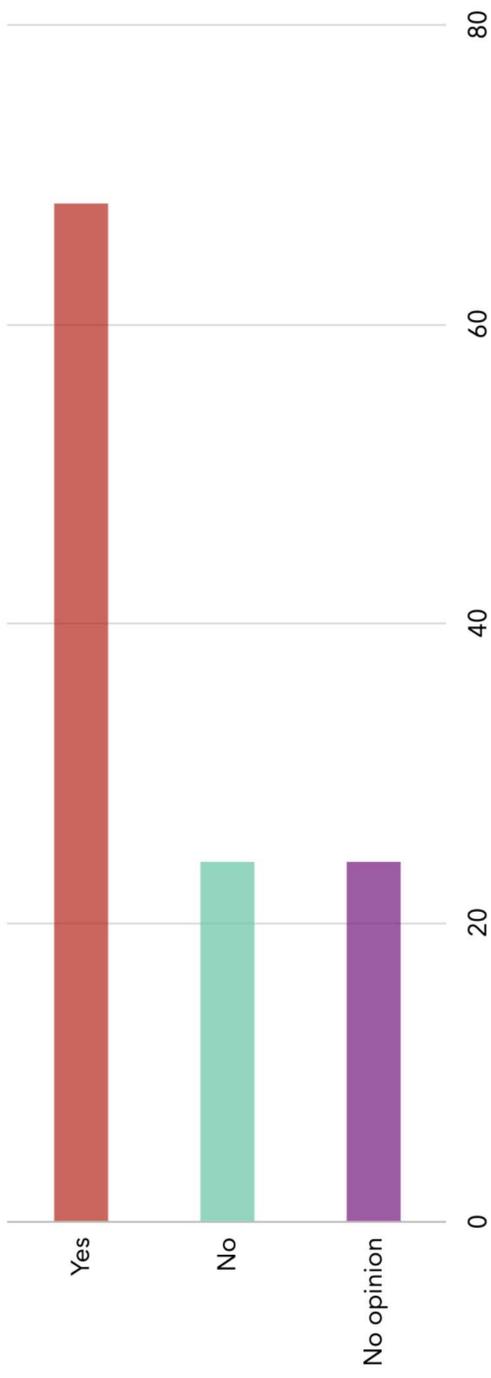
Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
Yes	71	58.68%
No	24	19.83%
No opinion	20	16.53%

Answered: 115 Skipped: 6

● Does the 8' maximum height requirement for sheds/structures in lakeside yards meet your needs?

Bar Pie Map



[Hide table](#)

Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
Yes	68	56.2%
No	24	19.83%
No opinion	24	19.83%

Answered: 116 Skipped: 5



There needs to be accessory buildings allowed. Size allowed needs to be bigger. Storage should be allowed on nearby lot if you own on the lake. 1

There is I believe a non-code/non-permitted structure on property abutting mine that blocks my view. 1

The measurement datum of "average grade" for building height is very tough on a lot of lake lots based on the dynamic elevations. No new sheds on lakeside yards! 1

The maximum height of our house is 33' 1

The 8' max height for the lake side accessory building /structure/boathouse is restrictive to my residential property. It prevents my repair/improvement of my lakeside structure. 1

Since being on a lake, more storage for ancillary stuff, kayaks tubes is needed. More storage is better. 1

Should not allow sheds on lakeside 1

Should be case by case which takes into account adjacent homes and lot size 1

Rules should meet common sense not arbitrary heights 1

Property is narrow, must be able to build higher 1

Overly tall houses disrupt the lakeside character of mostly low profile homes. ANY height structure would block our ground-level view of the Lake, and multi-story houses built up from ground level loom over the street and single-story homes. 1

Our lot isn't very large. We definitely don't want to ever impose on a neighbors view! 1

May want higher maximum for rebuild 1

It's not tall enough for walk in 1

If your on a hill the height should be flexible as long as it doesn't block the view. 1

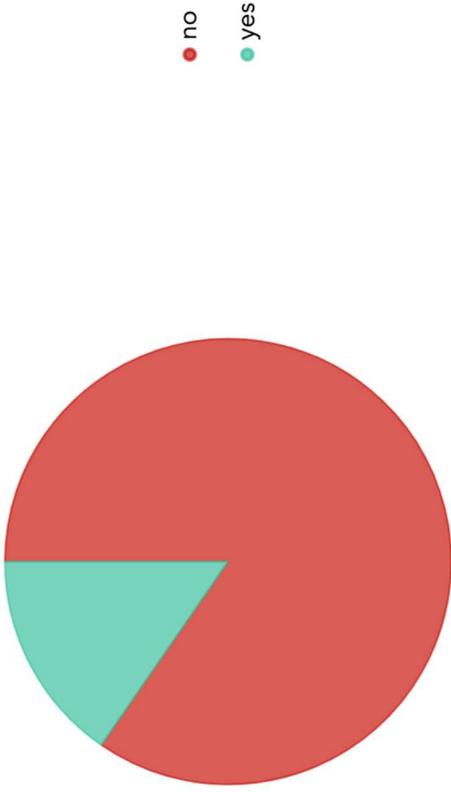
If the building is too close to the property line, variance for second story apartment should not be allowed. 1

Ideally, I would like to build a detached garage that is tall enough to store a motorhome out of sight. In order to do that, the roof needs to have a peak higher than 16 ft to still be able to drain rainwater off of it	1
I would rather see us go back to the 25' height maximum.	1
I would like to build a taller garage	1
I need a taller detached garage for boat storage	1
I may need to go higher	1
Height is too low.	1
For possible additional storage for lake toys (boat storage, kayaks, paddle boards, etc)	1
Deck is higher, but I don't think it is included in the 8' requirement.	1
Block the view of the lake	1
A garage on the street should be tall enough for two stories	1
A boat shed would need to be a little bit taller	1
8' is not tall enough for a storage shed	1
30 is TOO tall. Our next door neighbors had a complete rebuild. This new house dwarfs houses on each side and puts two houses in complete shade. One neighbor SOLD and moved - his view of this gigantic house looks like the back side of a store.	1
an addition to our home to maintain the aesthetics require more than 30 feet high.	1

Answered: 36 Skipped: 85

The placement of my neighbor's house partially blocks my lakeside view.

Column Bar Pie Map



[Hide table](#)

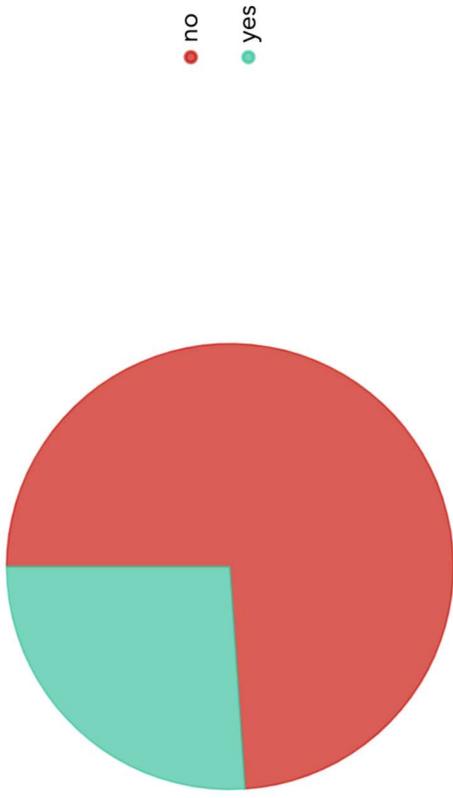
Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
no	99	81.82%
yes	18	14.88%

Answered: 117 Skipped: 4

### Have alterations done by a neighboring property negatively impacted you?

Column Bar Pie Map



[Hide table](#)

Empty categories [Sort](#)

Answers	Count	Percentage
no	85	70.25%
yes	30	24.79%

Answered: 115 Skipped: 6



Same as above.	1
Prior to our ownership, a neighbor built a deck within 1 foot of the lot line. It does not inhibit our views until there is a party, which then invades our privacy. We have things worked out well with our neighbors, but this is not always the case.	1
Previous neighbor (now moved) built house and sidewalk on the property line. We now have an unsightly concrete wall exactly on the property line.	1
Our next door neighbors had a HUGE complete rebuild. Now our lakeside yard FLOODS every time it rains. These lots are NOT big enough for huge houses - it seems as if the city is pushing for HUGE houses and not protecting space of others.	1
not at this time but what about future tear downs?	1
Neighbors lot was pie shaped with only 8.8 ft of road frontage. The house set back is behind my house and they added a paved drive that covers 80% of their front yard. The drive slopes toward my house so water now runs into my basement when it rains.	1
Neighbors built within 5' of our property and added large deck and staircase on lakeside	1
Neighboring house 30ft and land is built up. Next to a one story - the house is sure and there's no privacy even with a privacy fence due to the height.	1
Neighbor's upper deck seems to hover over my property line.	1
My neighbors got a variant to build closer and prohibit our view towards the north. Now this was done before we moved in and we do like these neighbors!	1
My neighbor built a house and as part of the construction raised the level of his yard up. This had caused water to flow from his property onto mine. By the time the final grade was complete, it was too late for him to lower the yard to the height it was.	1
Lights on patio shine in bedroom window.	1

It cost me over \$4000 to pay a city assessment for a sewer attachment to a garage lot i will never use because a neighbor wanted one. So we all had to pay for it. 1

Fence placed on property line 1

Excessively long project and now construction materials piled for years attracting rodents 1

Empty lot and then they were allowed to build way forward and up higher so even there ground plants are too high now and even block views 1

Construction materials laying around. Excessive time taken for construction leaving open places in the eaves for rodent activity and carpenter bees and wasps have become a major nuisance affecting our right to quiet enjoyment. 1

Both neighbors brought fill in to raise their yards sending much more stormwater surface flow than before into our yard which caused screening shrubbery to die. 1

Altered view 1

Allowing trees to be overgrown right by the lake that blocks views 1

Added patio and hot tub inhibit lake view and reduce privacy. 1

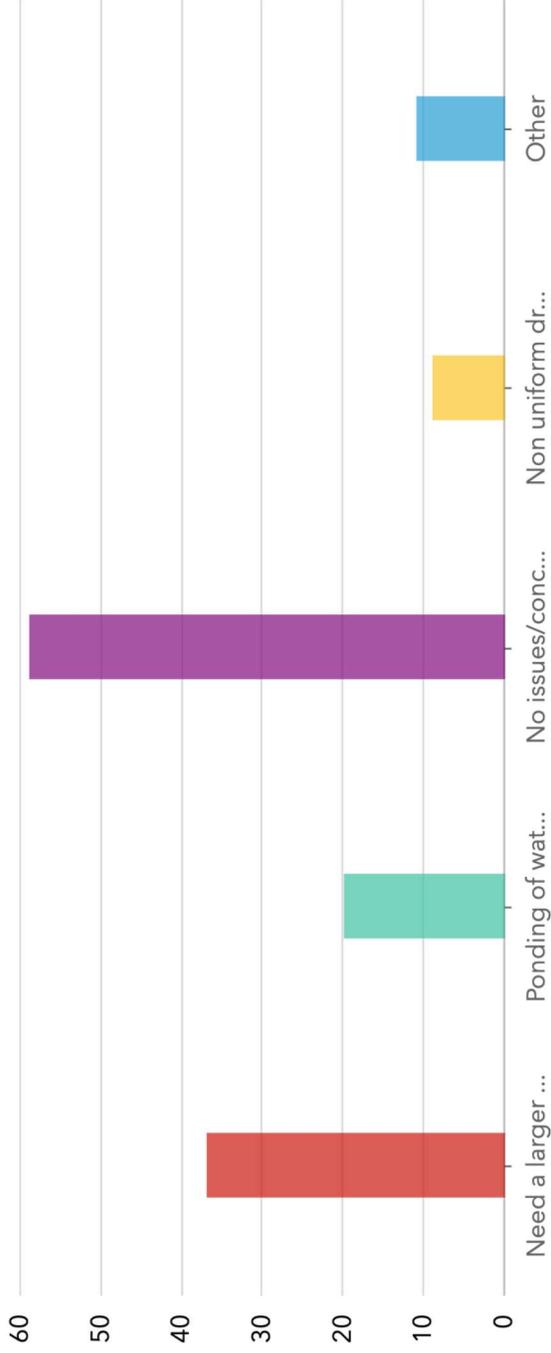
Added a shed in my view 1

A shed built on property blocks any crossing in either direction 1

Answered: 29 Skipped: 92

## Do you have any of the following driveway concerns?

Column Bar



[Hide table](#)

Other response
  Empty categories

Answers	Count	Percentage
Need a larger driveway for parking/maneuvering cars	37	30.58%
Ponding of water occurs from the driveway	20	16.53%
No issues/concerns with my driveway	59	48.76%
Non uniform driveway surfaces are unsightly (i.e. asphalt addition to concrete)	9	7.44%
Other	11	9.09%

Answered: 111 Skipped: 10

### How many vehicle parking spaces do you need

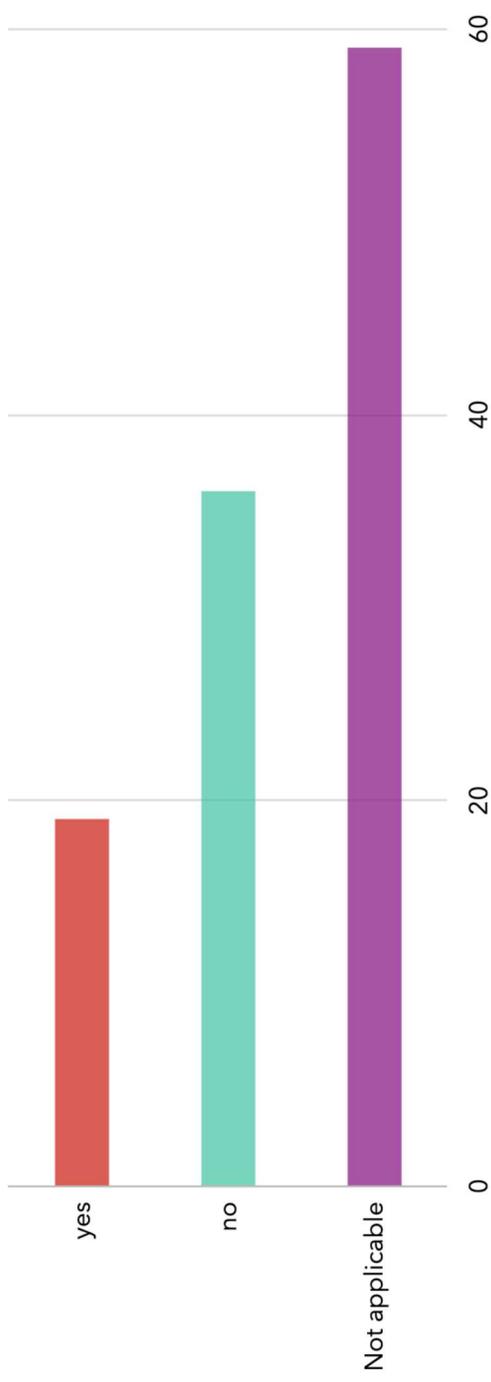
Column Bar Pie Map



Answered: 109 Skipped: 12

### Detached garage setback

Column Bar Pie Map



[Hide table](#)

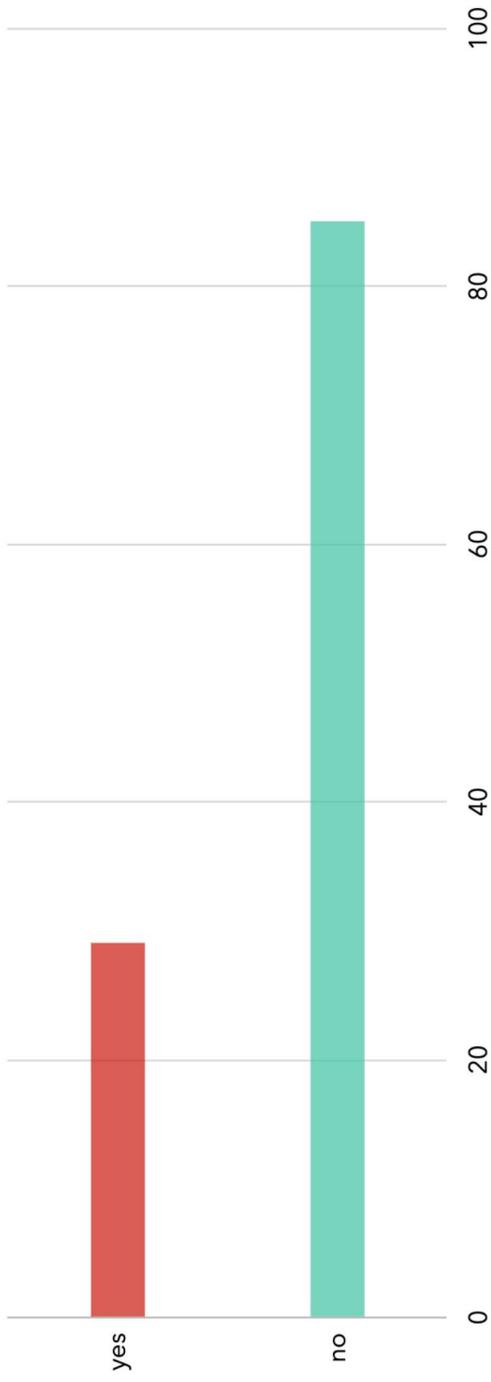
Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
yes	19	15.7%
no	36	29.75%
Not applicable	59	48.76%

Answered: 114 Skipped: 7

### Do you have re-occurring issues with rainwater collecting in your yard?

Column Bar Pie Map



[Hide table](#)

Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
yes	29	23.97%
no	85	70.25%

Answered: 114 Skipped: 7



We did not have issues until the city allowed a HUGE house to be built next door (our neighbors told us that someone at the city worked very hard with the builders to meet the 'average' 30' height.) Now every time it rains a large part of our lakeside yard floods. The great majority of lake lots are too small for huge houses. Why does this survey seem to be aimed at helping people build bigger, but not aimed at protecting others?	1
Water shed from neighbors roof and driveway tends to pool along the property lines.	1
Water collects on the street and driveway	1
There are many low spots and clay deposit areas in our yard that pool in rain	1
Tear down/rebuild next to me and ground regrading creating pooling issues between yards	1
Since the city repaved West End drive we have water pooling at our driveway and in our yard.	1
See answers above. The street and my neighbors new construction flows water onto my yard with dirt and debris.	1
Road run-off dumps rainwater into our driveway	1
Pooling about 10 feet from lake shore	1
ponding on driveway	1
No curbs to determine rain water flow	1
Neighbors elevated their yards so I get runoff. Sump pump drainage goes into my yard.	1
Neighbor's house is 2 feet from our lot line without gutters in place.	1
Lower level flooding on many occasions, but I believe we have remedied the problem.	1
I own the property behind my house across the road however I am not allowed to build a garage unless there is a "mother in law suit" built. I own the property and neighbors do not have an issue with a garage being built. I should be able to build a garage if I want.	1
I have extreme runoff from Sudan Street down my driveway into my garage and Lakeside yard creating flooding.	1

I did significant clearing and leveling to my yard which reduced water pooling however my property is low and the drainage to the storm sewer could be improved. 1

Heavy rains caused flooding in our 4 foot basement until we installed 3 sump pumps, which still run frequently in rainy weather. This was not a problem when we moved into this house in 2005. Heavy rains also create pools of water on our driveway near the street, and in low places in our back yard toward the lake. 1

Every spring & every hard rain causes another 'lake' in the low part of the front yard. 1

Due to grading of other property - we have a lot of water in our yard 1

City drains consistently clog causing flooding after heavy rain causing inches of standing water in the road that goes into our yard and the neighbors 1

Area near me can become soggy. Need sea wall. 1

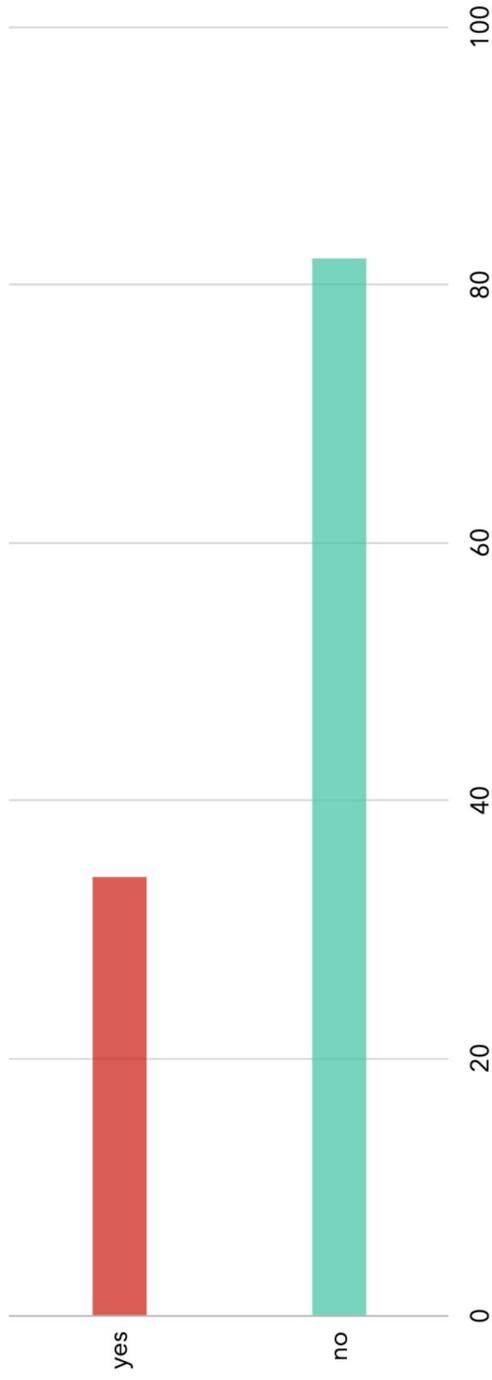
adjacent properties built seawalls that force drainage onto our property 1

As mentioned above, both neighbors have raised their yards sending more stormwater surface flow than before into our lakeside yard. We do not have a sea wall and ice from the lake pushes sod up during the winter creating a berm which prevents surface water from reaching the lake. 1

Answered: 29 Skipped: 92

### Is boat storage on your property a concern for you?

Column Bar Pie Map



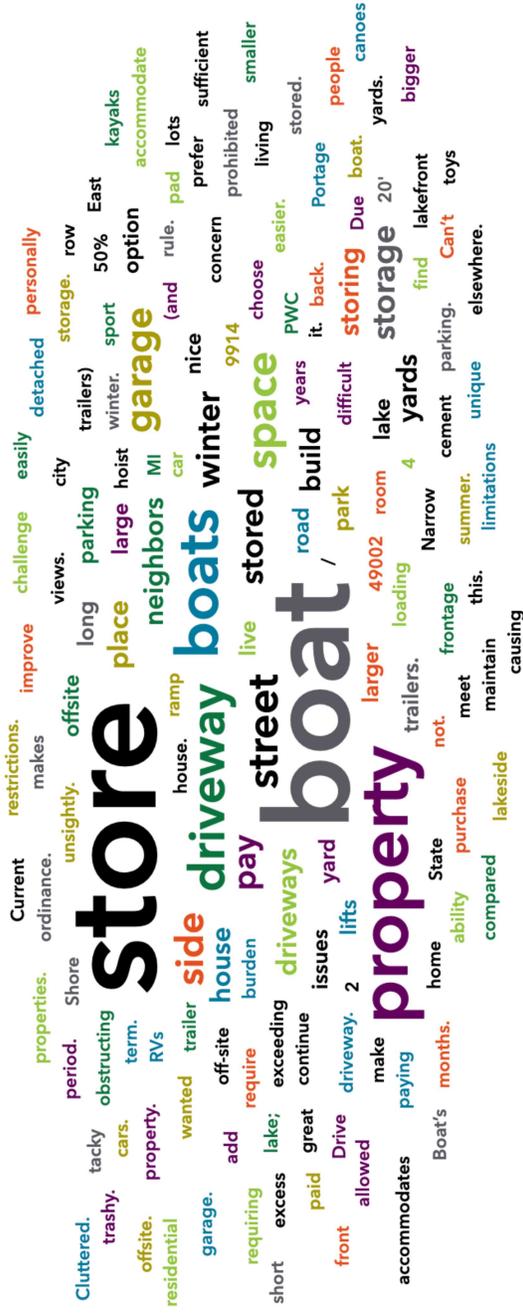
[Hide table](#)

Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
yes	34	28.1%
no	82	67.7%

Answered: 116 Skipped: 5

If answered "yes" to above, please explain



Response	Count
Would like to store it between the house and street	1
Would like to be able to continue to store boat on property	1
Would like a boat house.	1
We stored a 20' boat in the garage for several years, and it was a burden to trailer it to the State loading ramp and back. Now we keep a different boat on the lake in a hoist, and store it off-site in the winter. Many of our neighbors store their boats (and RVs and sport trailers) in their yards or driveways, which we find unsightly.	1
The only space available would require exceeding the 50% rule. That said, we still would not choose to store a boat at long term.	1
The large boat lifts that are in the yards during the winter are obstructing views.	1
Our driveway is not large enough and we only have a 2 car garage	1

Only neighbors that store so many it causes parking issues on the street	1
Not enough storage.	1
Not enough room for everything, but I can live with it.	1
No place to store we store at a boat place	1
Narrow driveway makes it difficult to park boat in driveway. Need larger frontage to street that would make it easier.	1
It would be nice to have the option if we wanted to store our boat at our house or not.	1
It would be nice to be able to store a boat on our property but currently we cannot	1
It would be great to be able to store our boat on our property	1
It is not a concern because we have enough property to store our boats now on trailers. We don't want to be prohibited from storing boats on property.	1
If you are paying to live on a lake; it seems like you should be able to store your boat on the side yard without having to have a cement pad because it is stored on the lake all summer.	1
If we were able to purchase property and build across the street from our home 9914 East Shore Drive Portage MI 49002 we would like to build a living space with a garage that could accommodate a boat.	1
I would like to improve the road side of my property so I can store my boat and meet the city ordinance.	1
I would like to add a larger detached garage for boat plus PWC storage	1
I want to maintain the ability and right to store boats on my property when necessary	1
I store some of my boats and lifts lakeside during winter months. I would prefer to keep doing this.	1
I pay to have my boat stored.	1

I own the property behind my house across the street however I am not allow to store my boat on the property but I am also not allowed to build a garage. 1

I have a driveway that easily accommodates the boat 1

I have 4 boats on trailers. It can be a challenge to store them with driveway and side yard restrictions. 1

However sometimes people store in excess of what they have space for causing issues with street parking. 1

Due to limitations on driveways and smaller lots very little place for storage requiring paid offsite storage 1

Do not appreciate my neighbors storing their boats in the front driveway all winter long 1

Currently, I have to pay for offsite boat storage 1

Current garage and parking is not sufficient to even park our boat for a short period. Our property I would say is unique compared to other lakefront properties. 1

Cannot store boat on driveway because we need space for cars. 1

Can't store on road side, even in driveway over winter and no space on side yards. Have to pay to store elsewhere e. 1

Boat's bigger than canoes, kayaks or row boats should not be stored on residential property 1

Because I do not have space I pay to have it stored offsite. I am okay with this option because personally I think storing boats / toys in yards and driveways often looks tacky and trashy. Cluttered. 1

Answered: 35 Skipped: 86



The thought from the City that lake front side is the backyard and having a 40 foot minimum to build from. This causes blocked views from existing houses and a race to the lake to prevent others from building and blocking their views. The back lot is the road side and front yard is lake side. This clarification needs to be addressed.

The only code that concerns me is one that would allow for multi family dwellings such as apartments to be built in the area.

The city needs to consider how changing the code affects neighbors. The new giant house next door ruined our drainage (our yard now floods every time it rains) and destroyed the view of the house on the other side. The house on the other side of the new huge house now seems as if it is viewing the backside of a three-story warehouse - this neighbor SOLD HIS HOUSE because of his view of the monstrosity next door. The people one more house over are now in the shadow of this huge house - completely blocking the sun that used to come in their skylights. Our neighbors (new huge house) told us that someone at the city worked with them for a long time so their house would fit the 'average' 30' height. To meet this 'code' they built up a huge side wall - so the 'ground level' was now higher - this should NOT BE allowed when working with heights of buildings. I would strongly encourage the city to disallow this practice.

The ability to add an accessory building on a lot that is near your house. It should be allowed in the back Non lake side or Lakeside if there is enough room.

Small lots, 30% is low 1

Size, height and distance from road for a detached garage. 1

Side lot setback 1

Short term rentals. 1

Section 24 and the limited amount of driveway 1

Protect from over development of back lots. 1

Property setback from road. 1

Property across street and accessory buildings lake side and garage and quality with lake 1

Possible flooding if the number of new structures, close in proximity to existing homes, removes trees and shrubs that currently absorb ground water. 1

Please enforce the existing roadside parking requirements. ie parking on improved surfaces and being a licensed vehicle or trailer. 1

Permission for a boat house that can span 4 seasons 1

Percent of lot coverage is too restrictive and driveway width restrictions can be a problem, especially on smaller lot sizes. 1

Owners do not have the ability to make changes to their homes because of over arching regulations. 1

Our drive way, as a home with a more narrow lot, we would like to expand our drive way and thus improve the overall property in visual appeal and practicality. But would technically be unable to do so because of the width of our lot at the front of our property. If there were an amendment for properties in our situation it would allow us to make these improvements. 1

Obstruction of lake views by remodeling or new builds. 1

Not positive of the codes however street side code should be a little more opened. For example. Not that I am going to but should I want to add a 3rd bay to my garage on the street side.... There shouldn't be an issue when calculating the area of the home to yard ratio. It doesn't block any views for example. 1

No opinion 1

No issue 1

New construction restricting existing residences view. 1

New construction blocking the lake view for neighbors. 1

Neighbors' un-attached garage with newly constructed apartment living in upper level has a window that looks straight through my upper level. His cutting down of privacy arborvitae provides him with full access to the viewing of my porch, dining room, living room, and family room. This violation of my privacy, while numerous construction workers build, plus the future of his family members viewing my home is a disappointment for the City of Portage gave him the variance to complete this construction. One further note, he told the city that the upper level would be for storage, but he told neighbors that it is actually living space.

Neighbors who build closer to the lake than other houses around them, thus blocking lake view from others houses.

Need uniform setbacks from lake so new construction doesn't block lake view of older homes.

Need greater than 50% front of front yard for driveway.

N/A

Many owners are having large parties with live music that is very loud. There should be limits on the volume because sound travels very far across lakes. Also, there should be a noise limit on boats. We have a couple on west lake that sound like the cigarette boats on Lake Michigan.

Managing the water in Woodhams road during excessive

Making sure the plans of neighbors do not impact my property negatively. If they need a variance, it should be approved by the neighbors. This includes changes that can impact my view such as deck placement in concern to the lake. If they are going to raise the level of the property they need to get approval of the neighbors unless the overall height of the house and the increase in property height do not exceed the current height limitations.

Lot line issues that are hard to determine because the views / placement of neighbor's home is straight instead of at an angle to follow the lot line.

Land usage around lakes. There should be more open space around the lakes to ensure the health and quality of the lakes in Portage.

Lakefront owners maintain the easement between the houses on Austin lake and are the rightful owner of said land. City needs to release the 5 foot easements originally intended for fire access to the lake

Lake facing shed or storage restrictions too strict	1
Lake access by non-lake side individuals. Egress access only.	1
Lack of code enforcement. A neighboring derelict property reduces my property value by up to 15 percent.	1
Keep properties clean and free from debris	1
keep good records of variances	1
It would be helpful for city staff to outline potential options instead of constantly saying "No" to ideas. Wastes a lot of time going back to the city again and again with potential solutions when it would be helpful if they would provide some direction as to what is allowed.	1
I'm completely against anything but single unit homes on the lake. Goguaac in BC has this problem. If you look at the 42 or so boat spaces in the DNR access and all of the riparian lots Austin Lake is already overcrowded and getting dangerous. Also most folks don't follow simple in-land boating laws, which makes it worse. I read some place that a lake our size should have no more than 100 boats at anytime. Gosh the DNR lot takes almost half of that already.	1
I would like to build a standalone garage and city code is preventing me from building a structure to meet my needs with the setbacks requirements that are now in place	1
I am hoping to add a garage soon and the side and road setbacks will be important to me.	1
I am hoping to add a garage soon and the side and road setbacks will be important to me.	1
Huge houses dominating the street and the waterfront seem to be oversized compared to mid-century constructions	1
Houses located in front of their neighbors houses.	1
Following the policy of no vacation rentals allowed.	1
Encroaching on setbacks causes crowding	1
Driveways. Lake lots can be very small, so covering 50% of front yard or less may be difficult to adhere to.	1

Driveway width maximum needs to be enlarged to accommodate more garage doors. With the shallow lots on the lake from water to road, even if they are wide lots, the current ordinance forces a nonfunctional parking scenario. Too many 3-4 car garages with gravel beside the concrete or asphalt because of current ordinance. The gravel that home owners are using opposed to a consistent surfaces is unsightly and usually full of grass or weeds.

Driveway space for parking. 1

Driveway coverage since many lots are small and need the extra space to park. 1

Detached building on lake side for a pergola or sun room since we sit so far back also would like to have land across the street to build garage 1

Designating what is the front and back yard, the front yard should be the lake side. 1

Current setbacks for a sufficient size garage. 1

By far, the setback from the lake is the biggest issue (followed by building height). Many existing houses are located quite distant from the lake. I once read an old, unenforceable rule written when this neighborhood was established that a home could not be built closer to the lake than a string pulled from the fronts of the two neighboring homes. The intent is obviously to prevent blocking neighbors' views of the lake. If this were to happen to us, we'd seriously consider leaving Portage.

Building a garage on property behind my across across the road. Determine ruling for easements 1

Boat storage 1

Being able to build closer to property lines. 1

Allowing larger driveways would reduce number of vehicles that have to park in the street. 1

Add a storage shed on small lots 1

Access to lakeside yard by neighbors (especially renters) when side yards don't allow access for any type of equipment without going on neighbor property. It's fine as long as neighbors are friendly but if they're not and unreasonable what is homeowner to do. Same with directing roof water through gutters that direct water toward neighbor house instead of toward lake. Water drain off from rain should be directed to lake side yards rather than toward side yards. 1

Able to expand my driveway and be able to have more tasteful storage built other than plastic sheds. Be able to have a garage. 1

Ability to adjust building in relation to what adjacent homeowners have already done 1

Ability to add a roof structure over patio for sun protection. We genetically have high risk for skin cancer and with family visiting umbrellas are not sufficient. Patio is attached to back of house, not lakeside. 1

Answered: 72 Skipped: 49

Overall, do you think there should be additional code provisions specific for lakefront property?

Column Bar Pie Map



[Hide table](#)

Empty categories [↑↓ Sort](#)

Answers	Count	Percentage
yes	55	45.45%
no	48	39.67%

Answered: 103 Skipped: 18



There should be provisions that limit ownership to single family on the lake. If rentals they should be long term. A  
Iso codes and regulations for development of property surrounding lake property but not actually on the lake. 1

There should be additional storage buildings allowed near the lakefront house. 1

The smaller lot sizes typically associated with lakefront properties require more lenient (less restrictive) zone codi  
ng than standard neighborhood housing. 1

The narrow lots make difficult to meet side setbacks. 1

The 'ground' should not be allowed to be falsely elevated from the current ground level so houses can be built ta  
ller (by building a huge wall and backfilling it with dirt.) A person at the city spent a lot of time consulting with our  
next-door neighbors do this (falsely heighten the ground) so they could build higher! This seems to skirt the cod  
e - why have a code if the city is going to help people dodge it? This made one side of this new house so tall, flat,  
and unappealing that the neighbor who had to look at it sold his house. Please remember that most lake lots are  
very small and proportions are important. 1

That you cannot build any closer to the lake than the neighbor on either side of you so as to not block the lake vi  
ew of your neighbors. The current 40' foot rule needs to change. 1

Sometimes there may need to be a variation to the 5 foot set back for certain projects. If the neighbors don't min  
d then approve it. 1

Same as above. 1

Protect access to non-lake side individuals. 1

Perhaps not codes, but it would be nice if there were some kinds of benefits to long-term owner/occupants to be  
able to remain in their lakefront homes without impositions from new neighbors. 1

Nothing specific at this time. 1

Not more restrictions. More informational meetings to discuss lake front property to find common ground. 1

Not allowing houses to be located in front of the neighbors houses. (Lake side) 1

No structures should block other peoples view and if you make modifications that cause flooding these owners should be oenskitt	1
No other than making sure new homes aren't built too close to the lake and obstructing the views of homes already there. I thought those rules were already in place but obviously some new homes block older homes views.	1
Need more flexibility and care for lake and consistency with existing structures	1
Need code to prevent new construction from blocking the lake view for neighbors.	1
My parents had a neighbor on West Lake that used one of those rental apps. It was horrible for them. Constant parking, noise, trash, and lewd behavior. Lake properties are so close to each other making it even worse. I'm against those short term rentals.	1
Most of the current code is set up for suburban neighborhood lots, or subdivisions. They do not take into account realities of lake lots -- narrow lots, requirements for moving boats and other marine craft in and out of driveway	1
Maybe a little larger square feet for lakeside storage building.	1
Loosen restrictions for detached garages.	1
Less restrictive not more	1
Lakefront properties have been around for decades and the footprint of current houses and detached garages make it difficult at times to adhere to current setback/code requirements	1
Lakefront properties are different, but can also impact property owners in the other side of the street that may lose a view or other attributes if the Lakefront owners get a variance. However, they also need to be able to have a house big enough to live in.	1
Lake facing shed or storage restrictions too strict	1
Just keeping track of what people put up they can negatively impact other properties	1
I think the city should make more of an effort to protect property owners from renters and rental properties.	1

I think all codes should be reviewed So the homeowner can build a structure tastefully done to fill the needs, since we do pay a higher tax rate, I believe that should be allowed. 1

I own property on Austin Lake, 9236 East Shore Dr. I suggest changing the zoning to allow for a building or parking across the street from our home. This would increase my property value. My name, Norman Schuen, 9236 East Shore Dr, phone # 3272863. My wife Elvira Schuen is of same mind. Thank you, Norman Schuen 1

I do not believe that lake properties should have ADU's. People may abuse the use of ADU's and use them for weekly rentals for personal gain. 1

Height restrictions should be monitored at lakefront property, also storage facilities affecting the neighbors View of the lake should be considered. 1

Ex: my yard is 30ft wide I would like to have a bit more cement for two cars and build a tasteful house matching shed storage. 1

Continued awareness of placement of additions, remodeling, and new building, as well as sheds and other structures that may block lake view. 1

Codes are challenging for narrow lake lots, need more flexibility. 1

Buildings too close to the property line should not be given variance for upper levels. 1

Boat lifts are very unsightly in the Winter and should be subject to stiff setbacks from the lake's edge. 1

bigger driveways to facilitate modern houses with 3+ car garages. the average width requirement makes it very hard to design a driveway for a 3+ car garage that isn't extremely tough to maneuver. 1

Because most of the lots are non-conforming it makes remodeling or building difficult without applying for a variance.... giving more flexibility to lakefront owners would lessen the amount of variances sought Again, section 24 needs to be changed 1

Be flexible and use the properties on both sides of the house to judge what the property owner should or shouldn't do. Every lot is unique and has to be treated that way when improving it. 1

Allowing variances that do not impact quality of life for adjacent properties. 1

Allow boats to be parked in side yards without requiring a prepared surface. There is nothing wrong with parking boats/trailers on grass or small pavers without requiring full gravel or asphalt parking pads. 1

Allow boat storage more easily 1

A formula for calculating setback from waters edge/property line that limits impact on existing residences by new construction. 1

Answered: 49 Skipped: 72

## Additional concerns for lakefront property owners.

## Word cloud

we've trees depositing acquired street? tracking? busy available? barrels basis average shore. board space. improvements large  
 down. Drive. houses. construction neighborhood projects  
 removing big residential Drive? skis lights difficult making houses green neighborhood  
 noise Additional I'm front storm limit Put weed concerned weeds neighbors Development Problems streets &  
 care blocking drains lakes park 25 driveway Austin force lake jet traffic increased wake major tear  
 racing decrease late build Riparians add ti access public 10 boat lake. property lanes boats taking yard lots shining tax  
 wheel issue build Riparians add ti access public 10 boat lake. property lanes boats taking yard lots shining tax  
 Panda set Riparians add ti access public 10 boat lake. property lanes boats taking yard lots shining tax  
 natural walkers don't boat lake. Portage pick follow up? paying residents oil continue housing side time. huge 2 presence close. lot  
 Leaf shrubs west East control left make Regulate things properties damage yard. wave lakes. place on. Fix (underwater  
 scheduled good control left make Regulate things properties damage yard. wave lakes. place on. Fix (underwater  
 possibility extremely living make Regulate things properties damage yard. wave lakes. place on. Fix (underwater  
 pretty high poor sewer Prevailing concerns includes waves erosion week. cul pedestrian  
 1/3 action room Prevailing concerns includes waves erosion week. cul pedestrian

## Response

## Count

- What is the intent of use for the property recently acquired along East Shore Drive? Will there be public parking available? Will there be an increased police presence to deal with problems from more pedestrian tracking? Will the residents across the street continue to be able to use it for visitor parking? Will we be able to continue removing lake weeds and depositing across the street? If not, will the city be doing a scheduled pick up? We currently average between 2 & 10 wheel barrels per week. Prevailing winds force all the weeds of the lake to East Shore Drive.
- Weeds in the lake. Leaf pick up has been poor as of late
- We are still concerned of the possibility of a high density housing development near the lake where it does not fit. We are concerned that West Lake will be too busy if a marina or kayak rental is put on the lake. It's just too small to have more boats on it.

We already had to follow setbacks in other projects and had to follow setbacks - we could've used some flexibility while we were making improvements and before having to tear other structures down. There should still be distance between properties and the road. I don't want to see houses right up to the street and no room for parking and no green space. Houses are already pretty close. 1

Wake board boats create large waves which cause a lot of erosion damage to lots with and without sea walls. If sea walls are in place, the wave action causes scouring (underwater erosion) which removes aquatic plants (and habitat) along the shore. 1

Unregistered boat in driveway for years, never used 1

The purpose and intent of this survey is unclear. 1

The company we use for weed control in the lake hasn't proven successful rather the opposite. I feel like the weeds are worse since we've started paying to treat it. 1

That the city is trying to increase vehicle traffic on neighborhood streets by "shrinking major roads. Portage and Sprinkle Rds are major through streets. We don't need to make it difficult for drivers to go down these roads and DEFINITELY don't need to add ways for cars to cut through the 25 mph neighborhood streets. 1

That all this data is being collected and the city will ignore it and do what they want like they have been recently and lying to the public about projects and what is really going on. Too many lawsuits against the city lately for not planning correctly, like the entrance to Culvers, Chik-fa-la, Panda forest, and letting the person who put those bike lanes in downtown Kalamazoo talk portage into those things that don't work. 1

thank you for having a meeting on it and taking it into consideration when making housing codes 1

Speed limit is not enforced. 1

some riparians may be in violation of 42-130 E&G near Lake View Park (letting neighbors who don't have lake property share a dock). 1

Sizable property taxes on many older fixed income people that may drive them out of their home based on cost. 1

Should regulate the storage of dock and lift on the property line 1

Seawalls need to be regulated and submitted for permits 1

Put storm drains and curbs on Lakeside Drive! Regulate tree cutting. Fix Road. 1

public boat launches that bring in inconsiderate weekend boaters who have no respect for the lake that they trash and misuse. 1

Problems arising from right-of-ways. 1

Prairie View Park has boat ramp which does not have boat wash to decrease invasive species being brought into lake. This has led to contamination and invasive weeds. We pay taxes for weed control when source contamination is uncontrolled from public sites. 1

Portage lakes have a long history of human use and have followed a typical trajectory of lake properties. Summer homes have been transformed into year-round residences. Riparians have been largely middle class residents for a long time. As the property values increase, these owners are being rapidly displaced by very wealthy buyers who demolish and build new. This wealth comes with influence, and the ultimate cost has come to bear on many multigenerational Portage families who are ultimately forced out of their beloved homes. The increased tax revenues may be handy, but it also comes at a cost: community. 1

Please do not expand tunnel access between Austin and West lake. West lake is small and cannot handle overflow traffic from Austin if it gets crowded over there. We do NOT want their boats or boats from the public access. 1

People walking along your side yards to get to the lake rather than use the Public Access or the City Park. We have had things stolen from our backyard/lakeside yard. 1

Over use of public access for fishing tournaments 1

Our biggest concern is the huge overuse of pesticides, herbicides, and fertilizers used around the lakes and along the drainage areas. Many people use lawn care companies that use these products on a monthly basis or more often. This has resulted in huge increases in weeds in the lakes and a filling in of the lakes. Very soon we will all be living on swamps instead of lakes. West Lake has 1/3 less depth than 30 years ago. 1

Off-lakers from the launch totally disrespect lakefront property owners. This has caused extensive damage to our boat, jet ski and they have left lures where our family has stepped on them. Also the traffic and speeding around the lake make it extremely unsafe for walkers and bikers. 1

New construction blocking neighbors' view or causing flooding on neighboring lots 1

Municipal storm drains dump directly into lake. This means oil, fertilizer, pet feces, and everything else washes into lake. These should be rerouted. Lack of weed treatment is a chronic issue and should be more aggressively addressed. 1

Modern lighting standards that maintain dark skies and keep neighbor's lights from disrupting wildlife and shining in our windows. Keep setbacks. Improve flood protections. 1

Lake side pathways for public access between properties 1

I think the neighbors in the Sudan Dale neighborhood should be held accountable for maintaining an outside appearance i.e. no trash in the front yard. Additional parking on the lawn should be enforced to maintain lake living. 1

I think the city does a good job for the most part. I recognize that it is extremely difficult to write a set of rules that is fair in every situation. Further I recognize that half the residents want no changes and the other half want extreme changes. For that I thank you all for your hard work and good luck on this journey to please all homeowners! 1

I don't agree with the city owning property on lake fronts that are not kept up to basic standards. The Green Space on portage road doesn't have any green space and the headlights shining across the lake are a nuisance and light pollution. 1

I am concerned with the city's plans for the land surrounding Austin Lake. The lake already has had issues with bacteria in the lake and it has been documented that more development around lakes decreases the quality of water which includes more bacteria's. This is harmful both to people and wild life. 1

How can they build way up then add retaining wall so there blocking view with big house and existing shrubs and plants and small trees added after that further block view because so forward 1

Houses being built in front of their neighbors houses. 1

His driveway is too close to the property line also. 1

Having front driveway restriction to 50% of front yard often forces owners to park cars on front yard grass, this is visually unappealing. 1

Encroachments such as over growing landscaping. Natural landscape overgrowth. 1

Development of surrounding lands 1

Deck construction setbacks from utilities. Cable, sewer, electric are all underground and lake owners do their best to avoid, but some of the requirements - like 10-ft setbacks for sewer seem over zealous. 1

Concerns about city plans to divert traffic into neighborhoods due to the reduction of lanes on portage rd. I'm concerned about the interest of the city to connect the cul de sacs around Austin lake. I'm worried more traffic will start diverting off Portage rd and onto neighborhood streets, putting walkers and bikers at increased risk of getting hit by cars. Cars already go too fast around the lake. 1

Boat lifts block view in all seasons. Noise of jet skis Loud sound systems on wake boats 1

Better enforcement 1

Better cable access 1

As to where I live my neighbors are very nice and we all are respectful. 1

As stated above, changes made to properties should not affect neighbors or the look of the lakeshore. For example houses should not be closer to the lake than their neighbors if they are granted a variance from the current of fsets. Finally, the new park on west lake has people past the time parks are closed. They shine head lights into the lake the reflect into our houses. This use to not be an issue when it was private property. 1

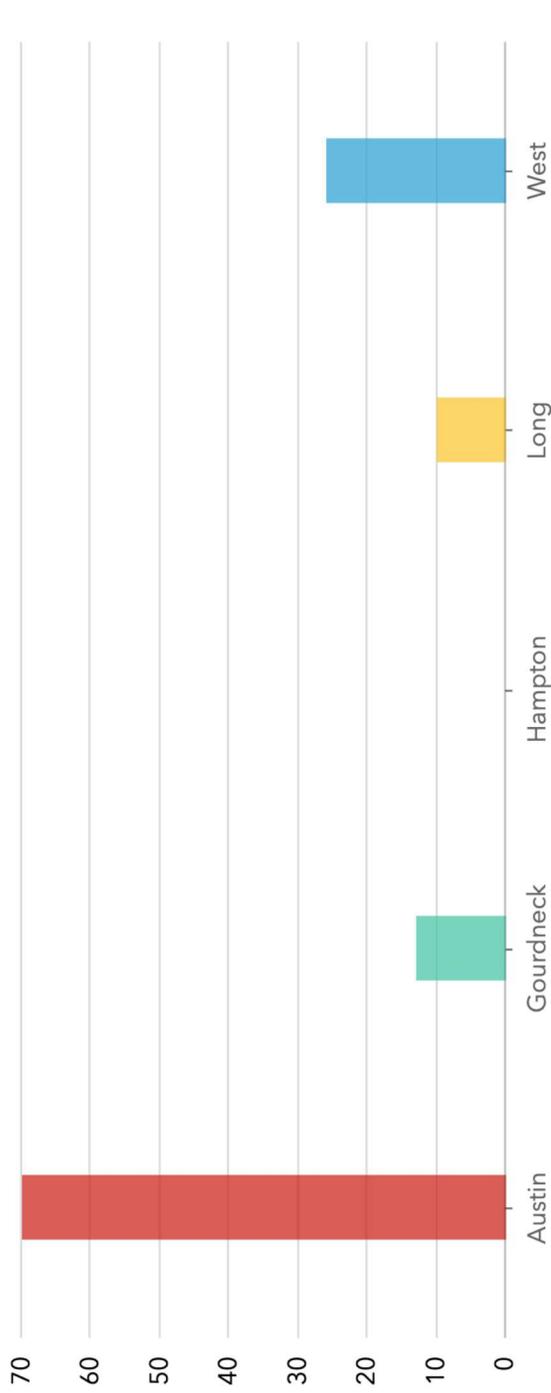
Allowing racing jet skis, which are significantly louder than any normal watercraft (pontoon boat, waverunner, ski boat, etc) onto a residential lake like Austin Lake. There are plenty of other places to do this activity that would not disrupt the quiet enjoyment of lake living for the residents and natural habitat. I believe this specific type of jet ski violates the City of Portage noise ordinance and therefore they should not be allowed on a residential lake. They set up camp at the boat launch and spend hours racing in front of it. 1

Additional concerns, our property owners taking advantage of road ends and public right ways.... many property owners have taken over these right away that includes planting shrubs to discourage them from being used by others.... that's very unfortunate, because not only are they taking away a right of other people they are not paying any taxes on the usage of this property 1

Ability to add a fence to keep the neighbor's yard waste, fallen leaves and trash from blowing into our lawn. They do not maintain property well. 1

### Which lake do you reside on?

Column Bar Pie Map



[Hide table](#)

Empty categories [Sort](#)

Answers	Count	Percentage
Austin	70	57.85%
Gourdnck	13	10.74%
Hampton	0	0%
Long	10	8.26%
West	26	21.49%

Answered: 119 Skipped: 2



Question	Response
<p><b>1. Do you think it is important to maintain a certain setback distance between the shoreline and future house, house addition? Why?</b></p>	<p>When rebuilding a new house there should be a firm rule that the new house can not go closer tot the lake than the two houses on each side. Use a string to show how far to move toward the lake. We dont need a race to the lake.</p> <p>Yes</p> <p>Yes, so as not to restrict lake view of your neighbors</p> <p>Keep the current setbacks to maintain the look of our lake</p> <p>Important to maintain certain setback from lake. Yes, but each situation should be house by house, depending on neighborhood, lots, neighbors grandfathered situations etc.</p> <p>There should be a minimum setback distance from the shoreline. Houses closer to the lake can shadow neighbors yards. It can also cause flooding.</p> <p>Yes, Always people respect neighbors and dont buld closer to the lake that doesnt seem to happen anymore.</p> <p>Yes, boat lift and dock storage in winter should also be set back.</p> <p>Absolutely yes. Common courtesy is not so common these days. Too many long term residents lost their views.</p> <p>Yes, To avoid issues with lake vision and neighbor problems. Otherwise building wil eventually be on the shore line.</p> <p>Yes, potential to ruin views of existing homeowners</p> <p>Concerend about new construction blocking the views of adjacent neighbors. Given the 100 years that houses have been on the lake, there should be consideration of the existing residents.</p> <p>New construction/new neighbors should bee good neighbors to the adjacent neighbors.</p>
<p><b>2. In 2019, the City increased maximum house height from 25' to 30' and increased lot coverage from 25% to 30%. Was this sufficient to meet your needs?</b></p>	<p>No opinion</p> <p>Coverage is defined as finished space it looks like. As we look to imporve outside of our house we'd like to build a additional garage to hold "toys" aka boats, snowmobiles ectct. However, the setbacks to main road could be an issue with closeness to road. I think keeping with setbacks prevent us from improving residnece on the roadside only. Setbacks on lake and side yards seem to be good.</p> <p>Do not increase house height maximum it will block views and reduce property values for existing homes</p> <p>Not necessarily there are driveway/road setbacks that cause trouble even inf your not at 25-30% of lot coverage. Percentage does'nt equal where the need is.</p> <p>N/A</p> <p>yes</p> <p>Admittedly it was a bit of a challenge when we remodeled 20 years ago but we made 25' work. We could have used 30% then.</p> <p>No, We dont need to increase height of buildings we have some pretty big houses on lake 2 and 3 stories already. Is the reason to increase height to make four level homes?</p> <p>Lot coverage is a joke. The way the city calculates the house area is inconsisntnet and rediculous home area should be only liveable indoor area.</p> <p>Yes</p>
<p><b>3. Are there issues of your property making it difficult to rebuild your house or house addition?</b></p>	<p>N/A</p> <p>We have no issues. Please do not allow homeowners to ruin our beautiful lake property.</p> <p>Inconsistent votes on zoning/city council and road setbacks for garages</p> <p>No</p> <p>yes at max</p> <p>Weve been done rebuilding for a long time. Some things were difficult then, but again we made it work.</p> <p>Lot area/coverage.</p> <p>Biggest issue is many homes (older) have code violations. The city did not keep good records when variances were applied. Those older violations are then grandfathered in. Lot lines are not necessarily where existing homes look like they are - new buyers are suprised.</p> <p>No, the permit process was great that isnt our concern.</p>
<p><b>Solar and Charging Stations</b></p>	<p>Are there codes in place for roof solar panels?</p> <p>Are there code restrictions on charging stations in garages?</p>

**Question**

**Community Input**

**Is your garage in need of replacement, expansion? Why?**

Our garage is in desperate need of replacement because it is original to the property and very old. It is currently unsafe, foundation is falling apart and concrete is as well. Want to upgrade and improve my garage at 3629 E Shore Dr. I think there may be a limit on height. Is this the case?  
Yes, the original structure is too close to the house to back out.  
We want to make sure our detached garage; 2 story with apt which is now grandfathered; will not be effected from any changes  
2 garages need to be replaced. Both currently too close to the street. Those should be able to be rebuilt in the current location.  
No

**Should the location, height and size of detached garages be restricted? Why?**

I believe it is dependant on the property  
Height should be restricted to one story. Garages with 2nd floors impact the feel of the neighborhood streets. Rental Apts above garages should not be allowed.  
Restricted - Only to the extent of the size of the lot  
Restrictions - No, they should be manageable and in line with the neighbors & neighborhood - & serve the needs of the homeowner.  
> setback ; side, lakeside, distance from lake  
We have concerns about the size limitations and setbacks for tearing down old detached garage and building new.

The City of Portage granted the Hybels a variance to add a second story to their garage for storage. The city granted the variance because the arborvitae would protect our view. We would not see the 2nd story. That's fair, right? Well, Matt Hybel cut the arborvitae and place a window into the 2nd story of this garage. This window has a view through my entire upper level. I have lost privacy because Matt Hybel Chose to cut down the arborvitae. This garage is 1' from the easement. The garage should have been moved over. Therefore, no outbuildings should be allowed to be altered unless they follow code in regards to property line. PLEASE COME OUT AND TAKE A LOOK. Note, the chopped up arborvitae, the window and its intrusiveness into my home. Note how close it is. (2) He has not had a silt fence up for at least 1.5 years into this 3 year (so far) construction project. Plus, I am really tired of the outhouse. Expand sizes of garages because these are small non conforming lots. Garage is limited because lake lake is limited because lot is small  
Yes, Junk accumulation a problem

**What are your needs for a Garage?**

Lake garages need more storage or you get boats stored in driveways and yards. Very Unsightly  
Parking  
Our current garage is not useable and we need a garage for parking cars/trucks inside with small room for additional storage of tools, etc. NOTE: Our situation seems to be unique since our house/garage is located (West Lake on Paul Ct.) We Welcome the opportunity to further discuss options.  
Needs: Cars, a boat, workshop and storage  
Security, Parking and space for snowplows, mowers etc...  
2 car detach  
Want to convert 1 car garage to mother in law suite.  
Lake property should be allowed more storage than size of house if lot will hold

**Other responses**

9212 Portage Rd. Is City buying/demoing house?  
Constituent wants a hardcopy of survey. Place on counter  
Allow pergulas on lakeside over existing patio?  
Rebuild garage? Its close to Property Line.  
Garage is located too close to property line. If burned down, can I rebuild?  
We've made the rules work for us for 35 years of remodeling & Upgrades by working with officials.  
Drainage Issues A. If a neighbor digs out, we don't want run off onto our property  
Drainage Issues B. If a neighbor is running off, we need to re-course to get them to correct it  
I have a boat house (built before 1965) at the water line. It is on my riparian land between the water line and my surveyed property line. When I wished to repair and improve it (new improved roof), I was told I was limited in height and cost. All of the immediate neighbors view are not compromised as I and they are 35' above the grade at the waterline/shore. Anything I would do would be an improvement in structure and appearance. I have not seem enforcement of code on accessory buildings establish since accessory building code was established. Thank you.

**Question**

**Response**

**What are Overall Needs for your driveway?**

Need to replace old concrete/ peagravel with new concrete  
Widen to be as wide as garage  
We are restricted on parking on roads, so parking is the biggest concern  
Need bigger & re-align lateral to the street, ne to raise the grade because all the runoff goes to us, even with the 2 new catch basins  
Asphalt falling away from Woodlawn at driveway leaving sizable bumps when entering  
Repair and replace  
Replace existing  
Vehicle parking  
Re-construct - cracks over 40 years old, same size  
Straighten the driveway, currently curves

**What is the reason(s) for needing to reconstruct your driveway?**

Worn-Out  
Add driveway & remove stone  
Broken cement, tree root damage, cracks in the asphalt  
Stormwater run-off, we must raise the grade, get curb cuts. Rearrange drive so it does not collect run-off which goes right into our garage currently. It is undermining the structural integrity of the garage.  
Worn-Out, chips & cracks  
Just repaving  
Commercial vehicles at Bigby Coffee have used my driveway as a turnaround and destroyed it. (Also, water line) I'll need to use thicker concrete and rebar.  
Worn-Out, someone put a dry-well underneath  
Just replace due to trees having it buckled up

**Should there be any location, width, or size restrictions on driveways?**

Should be no restrictions  
We should be able to have our driveways as wide as garages, The gravel a lot of us have is unsightly.  
No  
There needs to be some yard but we need wider driveway allowances, Maybe 2-3 feet of grass on each side  
Fewer restrictions, I want driveway width to be entire lot frontage  
Have a sliding scale for driveway width for homes under 100 foot of frontage  
Should be able to replace existing size and location, like for like.  
Heck NO!  
Driveways need to be part of appeal process and not part of the city code.

## Question

## Response

## Miscellaneous

Portage records need to be fixed, to look up history system locked up Mid 90's. That's what city staff said

Fences - who is responsible for taking care of cleaning a white fence between properties? What is the plan for right of ways, who owns roads between properties and who can use them for parking? Why are some people given the right to store seasonally used vehicles at side of road (long-term), cars, trailer boats etc.? This space is more for short term, but should it become a storage site. Would like to add a patio-removing grass and adding to what we have with a small lot, hopefully this is less restrictive.  
Encroaching on neighboring home owner views by building 40 feet, way too close from the lake lot line, pull the string.  
We are interested in curb & gutters along the east side of Woodlawn (lake side). That would help with all of the run-off we have in heavy rains

What is considered the front lot & back lot needs to be changed. If someone wants to build a new house they should only be allowed to build close to the lake, no closer than the house to either side of it. The 40 foot code for back yard is what needs to change. Please be the strong message to all home owners on the lake. When a new homeowner buys a small house then tears it down and moves closer to the lake blocking the view. The String rule - Where you go to both houses next door and go to the furthest to lake to the house next door. That needs to be the line to build to. (Drawing included)

A general concern I have is the large tax bill. Any chance we can lower or keep the taxes the same?

Concern regarding O'Brien property across street on E. Shore Dr. Non residential parking? Lake weed disposal? Security enhancements if made a public trail?

Over development of back lot property. Yes, set backs for storage of boat lifts.

Fisherpeople fishing at our dock. Some Sunday mornings I can walk out my dock & onto their fishing boat with my cup of coffee.

Totally against fences on lake side of house. Would like separate building for lawn equipment but only on street side of house. Why do we have to have an entry door on the side of the house? We have sliders on the lake side and an exit on the street side. If there's a fire I'm heading out to the large body of water no the narrow path between houses. Also, paid for the door and never used it.

Property Maintenance Concerns: Farm animals kept on a lake lot. Construction materials stacked for years. Wood Wasps, carpenter ants coming from next door. Rodent issues now coming from next door.

Not impacting neighbors w/ larger structures that go beyond their current footprint. Yes, height and grading of land to not flow or affect neighbors.

Yes, Standards which require people to respect neighbors not build in front of the front of their home.

Other Concerns - Single thing that requires a city code? Nothing - it should all be individually considered. Additional Standards - No, there are standards that have worked for years - and many new large homes have been built - consideration should be individual via what the owners want & neighbors can accept.

ADU's on lake - not close to lake. What is back yard, how far back from lake?

Can you limit where you put boat lifts (not in yard) Limits on size of storage, ok on lake side.

Concern re: O'Brien property along E. Shore Dr. - oppose changing and zoning (ie. remain natural w/out structures)

"Right of Ways"

Adding fences?

Adding separate buildings for lawn equipment?

My name is Robert Vella and I am a Portage property owner that has property located on Austin Lake on East Shore Drive.

I am writing to express my support for changing the zoning to allow for accessory buildings, storage, and parking on property that is adjacent to or across the street from and near to lakefront properties in the City.

In addition, I would be very interested in the ability to combine my property with the property across the street with a lot line adjustment.

I feel the new zoning changes would positively impact my property values and lake life enjoyment. If you have any questions or would like further input, please feel free to call me at the number below.

To the Department of Community Development, I plan to briefly attend the Open House scheduled for 6:30 on 16 January, but I wanted to make sure that my comments were heard, so I am emailing the to Peter Dame, as well as printing them out to submit at the Open House. Not that I am also copying Patrick O'Brien who appears to have an interest in my opinions. Here are my thoughts on the land use challenges that I see around Austin Lake: 1) I am in favor of developing a walkway/bikeway following the railroad on the east side of Austin Lake to meet with the Vicksburg trail. It would give my neighborhood a better avenue for accessing and enjoying the natural spaces. I would also be interested in buying the land across from my house if it were to go up for sale, so that could be in control of it, but I would NOT wish to use it for storage, parking, garage or dwelling units, as I treasure the unimpeded shoulder/walkway and would not want anyone else on East Shore Drive developing the far shoulder either. Traffic and sight-line issues are bad enough as it is, and more congestion on East Shore would be a serious loss to our neighborhood. I would be happy to have the trail use the portion of the parcel across the street even if I did buy it, because foot and bike traffic do no represent a problem to me (although steps must be taken to disallow motorized vehicles from using the trail). I would hope that the City and Patrick O'Brien could either come to an agreement on a fair price for the land if the City wants it all, or allow Mr. O'Brien to sell sections of the strip between East Shore and railroad to residents BUT NOT ALLOW IT TO BE USED FOR STORAGE, PARKING, GARAGE OR DWELLING UNITS. 2) I moved to this area of Portage because the open spaces and undeveloped land parcels make it feel like country living and a more "natural place to move". The constant construction around Austin Lake has been eroding the wilderness character of the area since we arrived in 2005. Equipment trailers, construction equipment and building materials have been stored on the shoulders of the roads around Austin Lake for mor htat a year in some cases, disrupting bikers, walkers and drivers as well as damaging roads and clogging drains. It seems to me that expanding the space of single family residences has been rewarding for the individuals, bt a loss for the character of the neighborhood. Large footprint houses of ever-increasing height are blocking the view of the lake from the streets, while extensions, patios and sheds are interfering with the lakefront experience. I would prefer to see MORE restrictions on expandint existing houses around the lake, not less. If the City goal is to increase housing availability, then subdividing lots and smaller houses should be encourage, not combining lots and expanding single houses. 3) I believe that buildings are not restricted from interfering with the neighbors lake vista - a notable new construction down the road from us was substantially closer to the shore than the existing homes next to it. The challenge is that East Shore Drive becomes very close to the shore in some places, so location options are limited (along the driveway and garage options). The obvious move to address this would be to require smaller homes on the smaller plots, but that is not what home-buyers want to see. So it seems that to enable larger homes, wither small lots need to be combined, or codes need to be changed. I would NOT wish to see the codes changed to allow bigger houses when the amount of land around the lake is limited. 4) We used to see the Milky Way at night from the lake. Since the number of lakefront houses have installed exterior lights, the schoolbus depot was built, and Pfizer expanded their operations, we hardly see the stars anymore. There is research that says night-sky darkness is good for health and well-being, but more importantly all of this light pollution is taking away the country character of the lakes area, and I believe it is also impacting migrating and hibernating wildlife. Code changes can reduce harmful light pollution without reducing the utility of the lighting. A number of progressive cities have instituted dark-sky policies. 5) A walk around Austin Lake is frequently met with a whiff of sewage smell. There have been some steps taken to address sanitary sewer problems, but they persists. I don't know if any of the buildings near the lake still have septic systems, but I do know that there is sewage in the area somewhere. I would call this a land use challenge because there are two large houses under construction in an area known for the smell, but I don't know what can be done about it. Certainly a revisions to code should be clear about avoiding this kind of issue. 6) My final land use issue is that any increase in population density is going to increase traffic UNLESS ALTERNATIVES ARE PROVIDED. That means increasing density only in areas that have good public transportation (of which Portage has few), or providing more goods and services in the areas that are increasing in density (like Crossroads or City Center), or renovating street configurations so that more people can travel without increasing traffic congestion. I have been disappointed in recent findings by the Traffic people that seem to indicate higher congestion is not a problem in areas where it clearly will be, so I think the first two options are best, but they require a gradual, planned transition from current state to desired state, and I am hoping that is what the Master Plan is going to enable. I really do not think that changes to codes are going to make a positive impact on housing availability around the lakes without having a negative impact on the enjoyment that current homeowners derive from living on the lake. At best, provisions that encourage maintenance and modernization will keep lake living from deteriorating over time, which is the greatest threat from homeowners not having the priority or funding to keep up their lake properties. Richard Nellums

# CITY OF PORTAGE PLANNING COMMISSION

**Thursday, April 18, 2024**  
**7:00 PM Portage**  
**Portage City Hall Council Chamber**

The City of Portage Planning Commission meeting of April 18, 2024, was called to order by Chair Corradini at 7:00 p.m.

## **IN ATTENDANCE**

- Catherine Kaufman, City Attorney
- Biqi Zhao, Deputy Director of Planning and Zoning
- Eric Feldt, Senior City Planner
- Alex Johnson, City Planner/Project Manager
- Peter Dame, City Chief Development Officer

## **ROLL CALL**

Mr. Feldt called the roll: Chairman Corradini (yes); Vice Chair Baldwin (yes); Secretary Freiman (yes), Adams (yes); Pezzoli (yes); Joshi (yes); Fries (yes); Youngs (yes); and Longjohn (yes).

9-Present; 0-Absent.

## **APPROVAL OF MINUTES**

1. Minutes dated April 4, 2024.

Motion by Vice Chair Baldwin, seconded by Commissioner Pezzoli to approve the April 4, 2024, amended meeting minutes, as submitted. Motion carried 9-0.

## **PUBLIC HEARINGS**

A Preliminary Plat approval for Oakland Farms No. 4 residential subdivision at 9840 Oakland Drive.

Mr. Feldt provided a PowerPoint presentation addressing the project by showing various maps of the overall area and subdivision phase. He described the overall subdivision platting process and the subject permitting step for the project. He stated that the project meets the applicable planning procedures and submittals, consistent with the City's improvement design standards, and is consistent with local zoning district standards. He concluded his presentation by recommending that Planning Commission recommends to the City Council approval of the Preliminary Plat for Oakland Farms No. 4 (Phase 5) residential subdivision at 9840 Oakland Drive.

Commissioner Longjohn asked about the City water main easement location.

Mr. Feldt confirmed this by showing the City's Fetch GIS map.

Commissioner Frieman asked about nearby residents' concerns about tree protection during construction. Mr. Feldt stated that neighbors can contact the City to determine any tree-related regulations and can contact the developer. Mr. Feldt also stated that the City does not have a tree preservation ordinance.

Chair Corradini expressed concerns about the City's 16" water main and easement in future residential back yards, looping the future water main, and utility layer for the subject phase.

Mr. Feldt clarified that the water main will stay in place and that its related easement is likely 10' on each side - total 20'; looping the water main is preferred; and that the utility layer is required only at final approval. Further, he stated he will pass the concerns about the water main location, size, and depth to the City Development Review Team and the Director of City Transportation & Utilities Department. (122)

Chair Corradini asked for the applicant to present:

Dan Martz, representative with Hyland Associates (7545 S 10<sup>th</sup> St Kalamazoo 49009), stated that staff's presentation satisfactorily summarized the project; indicated no intention to clear cut the site except for street and utility installation but try to preserve as many trees as they can.

Chair Corradini opened up the public hearing:

1. Martha Dahlinger, 2612 Chopin Ave.

Indicated her support of the developer keeping the trees and communicating with concerned neighbors.

Motion by Commissioner Pezzoli, seconded by Vice Chair Baldwin to close public hearing. Chair Corradini asked for voice vote; motion carried 9-0.

Motion by Commissioner Fries, seconded by Commissioner Pezzoli to recommend to the City Council approval of the Preliminary Plat for Oakland Farms No. 4 residential subdivision at 9840 Oakland Drive. Motion carried 9-0.

### **SITE/FINAL PLANS**

None.

### **OLD BUSINESS**

None.

### **NEW BUSINESS**

#### **Lakefront Property Housing Codes**

Mr. Feldt provided a detailed presentation about the Lakefront Property Housing Codes project by explaining the purpose, history of development around the lakes, public and staff data, analysis, and suggested code changes. He summarized the results of a lakeshore property survey and received input during an open house. He described past zoning code changes and current land use flexibility. Mr. Feldt summarized areas of code changes into several categories: driveways, lakeviews, yard setback, and detached garages. He then suggested code changes related to those categories. Mr. Feldt concluded his presentation by showing the project's next steps, asking the Commission to accept public comment on those suggested code changes, and asking Planning Commission direction on those changes.

Commissioner Youngs asked if this would reduce the number of variances.

Mr. Feldt stated that he is hopeful.

### **STATEMENT OF CITIZENS**

1. Lester Minor, 2514 E Shore Dr.

He stated various concerns about existing public lake access points. He noted drainage problems along E. Shore Drive. And he stated concerns about through traffic and any future street connections. He also stated concerns of a traffic light at Forest Drive and Portage Road and the adjacent property sold to the City.

2. John Fisher, 3506 E Shore Dr.

He stated various concerns about existing public lake access points. He also stated concerns about artificial grade raising of property causing drainage impacts to neighboring property, and general

concerns about street drainage. He also stated some historic elements of development on Austin Lake and past cultural divides. Further, he spoke about the history of property north of Austin Lake where Upjohn/Pfizer planted trees to offset their pollution.

3. Mary Wieloposki, 8836 E Shore Dr.  
She thanked staff for the lakeshore housing project by listening to the public and stated overall support for staff's recommendations.
4. Steve Pieczko, 3420 E Shore Dr.  
He spoke about the 'speakupportage.com' website which supports transparency and democracy. He also spoke about a new petition gaining signatures about the keeping single family residential in the master plan and to consider the existing conditions of a neighborhood; the petition will be distributed at the next meeting.
5. Mary Zoeller, 2024 Ames Dr.  
She asked staff why her neighbor could not replace the driveway to the original size (losing about 30% of parking).  
Mr. Feldt stated certain driveway size restrictions as the reason for the loss of driveway size, and offered to address specific circumstances outside of the meeting.
6. Sue Garvey, 2514 E Shore Dr.  
She stated that she owns a business and had to build a runoff pond for stormwater to reduce the water pollution, and asked why lakefront owners don't have to do the same for lake environmental reasons. Mr. Feldt stated that the same storm water regulations generally do not apply to single family residential property. However, when residences are part of a subdivision, they are part of a larger storm water network meeting storm water regulations. The large size of commercial buildings and associated parking lots and driveways generally have a large impervious surface area and, therefore, is more critical for meeting storm water regulations.
7. Scott Price, 10627 Sudan St.  
Asked if the land between Gourneck Lake and Portage Road is considered a public access point, as he has seen building activity and doesn't know whose property it is. He also indicated that the 24' ROW labeled as street, and the 5' ROWs aren't labeled at all.

#### **STATEMENT OF COMMISSIONERS/ STAFF**

1. Commissioner Longjohn stated concerns about drainage and pollution runoff draining into the lakes, and asked the difference between an engineer plan and drainage plan.  
Mr. Feldt stated that the goal of the drainage plan is to direct drainage to appropriate places and away from flooding. It does not have to address a severe flooding event, such as a 100-year flood, but to address more common rainfall events. Most lakeside streets don't have the infrastructure (curbs, gutters) that the rest of the city has for addressing drainage. There is no water treatment required for the drainage plan. Requiring an engineer plan could be excessive and costly from homeowner, but if the Planning Commission wants to pursue it they can.
2. Commissioner Fries asked about the percentage of lakefront driveways that exceed 50% of the front yard, and that it's likely more of a problem in lakefront homes. He mentioned that pervious asphalt, pavestones, and different pervious surfaces should be considered, even just for the part of the driveway that exceeds 50%.

Mr. Feldt stated that he does not have the information. But, having a large driveway can cause drainage and aesthetic concerns. He stated that he does not know the long-term efficacy of pervious material for driveways, but would not be prohibited if meeting the improved surface material standards.

3. Chair Corradini stated the occurrence of long-term drainage issues around the lakes; the lower ground contributing to groundwater problems; and that large driveways cause more water runoff. He asked staff if driveways can be built or rebuilt over existing utilities.

Mr. Feldt stated he would ask the City's Transportation & Utilities Department about that item.

4. Commissioner Joshi stated concerns about the public access paths.

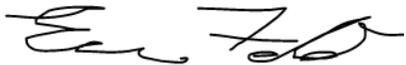
5. Attorney Kaufman stated that the City of Portage has hired an outside riparian attorney to analyze and gather data about public accesses around the lakes.

Commissioner Longjohn will be absent from the next meeting (5/2/24).

### **ADJOURNMENT**

There being no further business to come before the Commission, Chair Corradini adjourned the meeting at 8:42 p.m.

Respectfully submitted,



Eric Feldt  
Senior City Planner

## Lakefront Property Project Process

### **Goal**

Amend City codes to address unique needs of lakefront properties and reduce code non-conformities.

### **Issues**

Learn about lakefront development and reinvestment issues from lakefront owners, and evaluate problems from observed/documentated issues.

### **Address Issues**

Establish amended zoning and other code changes.

### **Overall Project Timeframe**

Approx. 6-7 months (January – July 2024)

### **STEP 1) Identify the problems/ issues – Data Collection**

#### **Timeframe: 1 month (January)**

- Survey & Open House – Collect, identify issues
  - Survey: Active for 3 weeks, expires Monday, January 22<sup>nd</sup>
  - Open House: Tuesday, January 16, 2024

### **STEP 2) Data Analysis**

#### **Timeframe: 1 month (February)**

- Data compilation, fact gathering and review

### **STEP 3) Draft Solutions/ Alternatives**

#### **Timeframe: 1-2 month (March-April)**

- Analyze and identify solutions, alternatives

### **STEP 4) Present draft solutions to Public[WE ARE HERE]**

#### **Timeframe: Meeting (estimated to be in April)**

- Planning Commission Public Workshop meeting - share results of data, draft solutions to issues, obtain commission and public feedback.
- Present zoning- and non-zoning related changes.



**STEP 5) Present Draft Ordinance to Planning Commission, Public Hearing**

**Timeframe: Meeting (estimated to be in May)**

- Planning Commission Public Hearing – Commission recommends draft ordinance that reflects April feedback to City Council.

**STEP 6) City Council Meeting – Schedules Ordinance to future Council meeting.**

**Timeframe: Meeting (estimated to be in June)**

- Present Zoning and non-zoning code changes to City Council.

**STEP 7) City Council Public Hearing, Vote on Ordinance**

**Timeframe: Meeting (estimated to be in July)**

- City Council holds public hearing and votes on zoning, non-zoning code changes.

**TO:** Planning Commission

**DATE:** May 16, 2024

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** A Final PD Site Plan for ‘Oakland Commons’ a proposed 58-units residential development at 9581 Oakland Drive.

## **I. INTRODUCTION:**

The applicant (Mike West, representative of Green Development Ventures, LLC) is seeking Final ‘Planned Development’ Site Plan approval for a residential development at 9581 Oakland Drive located in the southwest area of Portage approximately 1/3 mile north of Oakland Drive & Shaver Road intersection.

The proposed development called ‘Oakland Commons’ consists of 58 new homes on the 12.57-acre site, with a density of 4.6 units/ acre. Homes consist of a mix of detached and attached buildings (see home types further below) with individual private driveways and garages. All homes will be designed as 2-stories, sized 1,440 - 1,640 square feet, consisting of 3-4 bedrooms, 2-3 bathrooms, and 1-2 car garages. Concept images of homes are attached. All homes will be occupied as rentals, as the underlying property owner (same as developer) will oversee all site and general building maintenance. The project does not involve any land divisions or subdivisions. Therefore, all homes will be on an existing, single overall property.

List of proposed home types:

- Detached Single Family homes: 26 homes
- Duplex homes: 6 homes
- 3-plexes: 18 townhomes
- 4-plexes: 8 townhomes
- Total: 58 Units
- Proposed Density: 4.6 Units/ Acre

The overall site will be served by a new private driveway off of Oakland Drive leading to individual driveways to each home. New utilities, general community open space, landscaping, 4’ wide sidewalks and internal 6’ wide asphalt pedestrian connections, and guest parking will also be provided.

A new left-turn lane on Oakland Drive will be provided to accommodate the project’s expected traffic volume, turning movement, and ensure overall efficient traffic flow along Oakland Drive. This new turn lane is shown in the attached plans and was reviewed and approved by the City’s Transportation & Utilities Department.

New City water and sewer will be provided to each home, as well as private utilities (i.e. electricity, gas, cable, internet). A new private stormwater basin will be constructed and located in the northwest corner.

An emergency-only access driveway will be constructed and connected to an existing private driveway on the property to the east (Weathervane Self Storage-owned by JC Customs, LLC), per City Fire Department review. A cross-access agreement will be established to allow the use of the proposed emergency-only access drive.

The applicant may seek development incentives for the creation of approximately 20% ‘workforce housing’ income level (80-120% AMI ‘Area Median Income’) rental units.

If the Final PD Site Plan is approved, the applicant is expected to purchase the property. The current owner has agreed to the project.

The development will avoid the existing wetlands located in the northern section of the property.

## **II. BACKGROUND INFORMATION:**

The Tentative Plan and Rezoning were approved by the Planning Commission on August 17, 2023, and by the City Council on October 3, 2023. The property is currently zoned for Planned Development.

## **III. ANALYSIS:**

### **Applicable Zoning Regulations**

The following sections of Chapter 42. Division 4 & 5 Zoning Districts and District Regulations apply and are addressed further in this report:

- [Subdivision 2](#) – Site Plan Review
- [Subdivision 11](#) – PD, Planned Development District

### **Proposed Planned Development Final (Site) Plan**

The applicant has submitted a Final PD Site Plan application seeking final site plan approval for the project per *Sec 42-375 Planned Development and Sec 42-482 Site Plan Review*. The attached plans and narrative address these code sections.

#### *Sec 42-482 Site Plan Review*

The applicant has submitted a detailed site plan, showing parking areas, access points, building footprint, utility locations, and other site improvements. These are summarized at the beginning of this report.

#### *Sec 42-375 Planned Development*

The Planned Development review process involves the following two steps per Sec. 42-375:

Step 1) Tentative Plan review (Items A-G, Sec 42-375)

Identifies part(s) or whole project features; analyzes the impact on the neighborhood, streets, utilities,

and the natural environment; and determines consistency with adopted local plans.  
*This step was recommended for approval by the Planning Commission on August 17, 2023, and gained approval by the Council on October 3, 2023.*

Step 2) Final Plan review (Items H-M, Sec 42-375)  
 Identifies and analyzes detailed drawings of project improvements (streets, utilities, lot dimensions, etc.).  
*The Final PD Site Plan application is at this step.*

Building Setbacks

As indicated in the approved Tentative Plan, there are no defined building yard setbacks under the Planned Development district. Per Sec 42-374, the Planning Commission has accepted the following approved building locations as being appropriate for the project and neighborhood.

**Proposed Yard Setbacks**

<b>Unit Type</b>	<b>Front (to private drive)</b>	<b>Side (between buildings)</b>	<b>Outer perimeter</b>	<b>From Oakland Drive</b>
Single Family detached	20'	12'	30'	30'
Duplex	25'	18'	30'	30'
3-plex	25'	30'	30'	30'
4-plex	25'	30'	30'	30'

Planned Development - Sec 42-374 Site Development Requirements and Sec 42-375 (Items H-M) Plan review and approval (PD, planned development); Final Plan

Staff analysis has been prepared based on consistency with the previously approved Tentative Plan and Sec 42-375 Items H-M Final (Site) Plan. Overall, staff find the proposed plans to be consistent with the previously-approved Tentative Plan.

Environmental Conditions

As indicated in the approved Tentative Plan, there are mapped wetlands in the northern portion of the property. The applicant’s submitted site plan indicates the limits of the wetlands were field inspected in April 2023 by Environmental Science and Planning LLC. The applicant is not proposing any development or site improvements within the existing wetlands.

Evaluations from the Development Review Team (DRT)

This Final PD Site Plan was reviewed by the Development Review Team (DRT) who ensures that the proposed development is consistent with the City’s infrastructure, engineering, and other requirements including streets, sewers, water mains, storm water, parking, landscaping, and others. The DRT is

composed of representatives from the Department of Transportation and Utilities, Fire Department, Department of Parks and Recreation, Office of the City Assessor, and Department of Community Development. At this time, DRT has the following pending item:

1. A Revised note at the bottom of the profile to indicate that the developer will be responsible for all water main installation.

This item will be addressed in the subsequently required Public Utility Plan (PUP) – next step. Staff recommends this item be the condition of approval of the subject Final PD Site Plan.

### **Public Noticing Requirement**

Public meeting notice requirements apply to the project.

## **IV. RECOMMENDATION:**

Staff recommends the Planning Commission recommend to City Council the approval of the Final PD Site Plan for 'Oakland Commons', a proposed 58-unit residential development at 9581 Oakland Drive with the following condition:

1. A revised note at the bottom of the profile to indicate that the developer will be responsible for all water main installation in the Public Utility Plan submission.

### **Attachments:**

1. Location Map
2. Development Review Application
3. 2024-04-25 Oakland Commons Draft Storm Water Agreement
4. Previously Approved Tentative PD Plan, Rezoning Documents
5. 2024-05-07 Oakland Commons Final PD Plan
6. 2024-04-10 Oakland Commons Frontage Landscape Plan & Renderings
7. 2023-12-08 Oakland Commons Final PD Landscape Plan Renderings
8. Staff's 8.9.23 Site Pictures

# Vicinity Map



Green Development Ventures, LLC  
2186 East Centre Avenue  
Portage, MI 49002

---

Eric Feldt, AICP, CFM  
Senior Planner  
Department of Community Development  
City of Portage  
7900 South Westnedge Avenue  
Portage, Michigan 49024

December 5, 2023

Re: Oakland Commons Final PD Plan and Public Utility Plan, 9581 Oakland Drive (12.57 acres, Parcel #00029-425-G)

Dear Eric,

Attached please find the Final PD Plan and Public Utility Plan application package for the above captioned project. Attached are the following documents:

- Signed Development Application – Final PD Plan
- Letter of Authorization from Mike Fisher, J.C. Customs LLC dated March 10, 2023
- Oakland Commons Final PD Plan set
- Stormwater Calculations
- Review Fee Checks for Final PD Plan (\$1,136) and Public Utility Plan (\$396)

A more detailed Landscape Plan and renderings for the Oakland Drive frontage and entrance statement will be forwarded to your attention during the week of December 11<sup>th</sup>.

The Final PD Plan has been designed consistent with the PD Rezoning/Tentative Plan which received City Council approval on October 3<sup>rd</sup>. We look forward to continued discussions of this exciting residential project with the City of Portage and request formal Planning Commission consideration as soon as practical.

If you have any questions or require additional information, please contact me at your earliest convenience.

Sincerely,



John Lovely  
Engineering Project Manager  
Green Development Ventures, LLC  
2186 East Centre Avenue  
Portage, Michigan 49002  
M: (269) 391-0542  
[jlovely@allenedwin.com](mailto:jlovely@allenedwin.com)



# DEVELOPMENT REVIEW APPLICATION

Department of Community Development  
7900 South Westnedge Avenue – Portage, Michigan 49002 – (269) 329-4477

Applicant must complete all items in sections 1-9 (if applicable).

Please note: Separate applications must be submitted for Plumbing, Mechanical and Electrical Work Permits

**No Work is to Start Prior to the Issuance of Building Permit**

## 1) PROJECT INFORMATION

Project Description	Oakland Commons PD	Address	9581 Oakland Drive
---------------------	--------------------	---------	--------------------

## 2) IDENTIFICATION (OWNER)

Name	JC Customs, LLC (Mike Fisher)	Address	10032 West Gull Lake Road	
City	Richland	State	Michigan	Zip 49083 Phone

## 3) ARCHITECT OR ENGINEER

Name	MEGA (Pat Flanagan)	Address	1209 East Milham #D	City	Portage
State	Michigan	Zip	49002	Phone	(269) 344-6165
License Number		Expiration Date		Email	

## 4) BUILDING CONTRACTOR

Company name	Address		City
State	Zip	Phone	
Email	Federal Employer ID		
Builder License Number	Expiration Date		
Workers Comp Insurance Carrier	MESC Employer Number		

## 5) TYPE OF DEVELOPMENT PROJECT

- Site Plan                       Building Plan                       Final Plan in Planned Development  
 Subdivision                       Condominium                       Land Division Requiring Public Improvements  
 Landscape Plans                       Public Water Main     Public Sanitary Sewer Main  
 Other: \_\_\_\_\_

*(Note: Please provide a Portable Document Format (PDF) of the plans for the project at time of document submittal. The submitted format shall be CD/DVD or USB. If a PDF is not submitted, an additional \$25 fee plus \$1 for each plan sheet after 20 sheets will apply.)*

## 6) TYPE OF IMPROVEMENT

- New Building     Alteration     Addition     Repair     Demolition     Foundation Only  
 Relocation     Special Inspection     Solar     Other: \_\_\_\_\_

## 7) DESCRIPTION OF WORK

- A) Oakland Commons PD - mixed single family detached and attached residential community (58 total units)
- B) Valuation of Project: \$ \_\_\_\_\_
- C) Any known soil and/or groundwater contamination?     Yes     No

## 8) CHARACTERISTICS OF THE BUILDING

### A) Water Meter Size

- 5/8"       3/4"       1"       1 1/2"
- Other \_\_\_\_\_       Irrigation Size \_\_\_\_\_       Fire Sprinkler Size \_\_\_\_\_

### B) Electric Service Size

- 200 AMP     400 AMP     600 AMP     800 AMP     1000 AMP     Other \_\_\_\_\_

### C) Type of Mechanical

- Air Conditioning?  Yes     No    Fire Suppression?  Yes     No    Hood System?  Yes     No

## 9) CONSTRUCTION PLANS SUBMITTED

### Required submittals for plan review:

- Plans shall be signed and sealed when required in accordance with State of Michigan Act No.299 of Public Acts of 1980.
- All plans shall be drawn on uniform sheets no greater than 30"X42".
- All plans shall be drawn to an architectural scale.
- All plans shall be clear, legible and accurate.
- Plans shall be stapled along the left margin.

### Type of Plan Submitted:

- Building       Electrical       Plumbing       Mechanical       Energy

**Building Code:** Site plan, foundation plan, soil bearing capacity, floor plans, building elevations, building sections, framing plan, details, roof plan, roof finish schedule, roof live and dead loads.

**Electric Code:** Lighting layout, circuiting, switching, conductor and raceway sizes, wattage schedule, service location and riser diagram, load calculations, and appropriate plans showing standard symbols of all electrical equipment.

**Plumbing Code:** Site plan, floor plans, DWV riser diagrams and water distribution system and roof plan, Show direction of flow, pipe sizes, grade of horizontal piping, elevations, drainage fixture unit loading of both stacks and drains in the DWV system, supply fixture unit load for the water system, branch supplies serving more than one plumbing fixture, appliance or hose outlet, meter locations.

**Mechanical Code:** Plans indicating heating equipment, air conditioning equipment, ductwork material and layout, fire dampers, ventilation of rooms and areas, location of chimneys and vents, piping layouts, kitchen equipment layout, and combustion air. (Plans for fire suppression systems may be submitted after permit issuance, but are required prior to installation.)

**Energy Code:** Floor plans, building sections, details, average annual degree days, exterior envelope components materials, "U" values of elements, "R" values of insulating materials, size and type of apparatus and equipment, energy calculations.

**Note: Additional plan review fee(s) required of all non-concurrent plan submittals**

## APPLICANT INFORMATION

Applicant is responsible for the payment of all fees and charges applicable to this application and must provide the following information:

Name Green Development Ventures, LLC	Phone (269) 391-0542 John Lovely
Address 2186 East Centre Avenue	City Portage
State Michigan	Zip 49002

I hereby certify that the proposed work is authorized by the owner of record and that I have been authorized by the owner to make this application as his/her authorized agent, and I agree to conform to all applicable laws of the State of Michigan and ordinances of the City of Portage. All information submitted on the application is accurate to the best of my knowledge.

Signature of Applicant: \_\_\_\_\_

*John Lovely*

Date: 12/5/23

**STORMWATER BEST MANAGEMENT PRACTICES  
OPERATIONS & MAINTENANCE AGREEMENT**

THIS AGREEMENT, effective \_\_\_\_\_, 20<sup>24</sup> between the City of Portage, a Michigan municipal corporation, whose address is 7719 South Westnedge Avenue, Portage, Michigan 49002 (City) and GREEN DEVELOPMENT VENTURES, LLC, [status of landowner; i.e. individual(s) or companies] whose address is 2186 E. CENTRE AVE., PORTAGE, MI 49002 (Landowner).

*Recitals:*

- A. The City is regulated under the U.S. Environmental Protection Agency's (EPA) Phase II Stormwater Program since it has a municipal separate storm sewer system (identified in the Performance Standards as MS4). Therefore, the City is required to have a National Pollutant Discharge Elimination System (NPDES) Permit for its discharge of stormwater. The Michigan Department of Environmental Quality (MDEQ) administers the NPDES permit program for the State of Michigan (33 U.S.C. 1251 et seq., P.L. 92-500, 95-217) under Part 31, Water Resources Protection, of Michigan's "Natural Resources and Environmental Protection Act", 1994 PA 451 (NREPA).
- B. Landowner owns real estate in the City at 9581 OAKLAND DRIVE, Portage, MI 49024 - Parcel No(s) 00079-~~2~~-425-6 - and which is more specifically described in Exhibit A (Property).
- C. Landowner uses the Property for multi-family residential, commercial, industrial purposes, or a combination of those uses. Landowner is making improvements to the Property that requires approval under the City's Site Plan Review process, or is modifying the existing stormwater discharge system on the Property that either impacts the City's system or the retention of stormwater on the Property. As a result of those uses, improvements or modifications, Landowner agrees: (i) to install and maintain stormwater best management practices (BMPs) on the Property in accordance with approved plans and conditions; and (ii) to ensure that the BMPs continue serving the intended function in perpetuity.
- D. Before signing this Agreement the Landowner, including its representatives, contractors or agents, has reviewed or had the opportunity to review the Performance Standards, work sheets or other documents maintained by the City relating to the City's regulation of its Stormwater Program and this Agreement.

THEREFORE, in consideration of the above recitals and the covenants, conditions, and restrictions stated below, the parties agree as follows:

1. Recitals. The above recitals are acknowledged as true and correct, and are incorporated by reference into this paragraph.
2. Installation and Maintenance. Landowner is solely responsible for the installation, maintenance and repair of the stormwater BMPs.
3. Inspections and Repairs. Landowner shall regularly inspect, maintain, repair or replace the private stormwater BMPs consistent with the Manufactured Treatment Device (identified in the Performance Standards as MTD) as recommended by the manufacturer, and those recommendations provided in the "Low Impact Development Manual for Michigan – A Design Guide for Implementers and Reviewers" (Southeast Michigan Council of Governments and MDEQ, 2008), and "Michigan Nonpoint Source Best Management Practices Manual" (MDEQ, 2014).
4. Submittal of Reports. Landowner shall annually submit a report to the City regarding stormwater BMPs Operation & Maintenance for each of the MTDs and other BMPs. Landowner shall deliver the report to the Director of Transportation and Utilities either by mail to 7719 South Westnedge Avenue, Portage, Michigan 49002, via fax at 269-324-9240, or via e-mail at PortageT&U@portagemi.gov, within 30 calendar days of the inspection date.
5. Modifications to the Stormwater System. Landowner shall contact the City for approval prior to any design modifications to the stormwater treatment and/or conveyance system on the Property.
6. City's Access to the Property. Landowner, its successors and assigns, hereby grants the City, its authorized agents and employees, the right to enter upon the Property to inspect the stormwater BMPs whenever the City reasonably considers an inspection necessary in carrying out the intent and purpose of this Agreement. For example, an inspection may occur: (i) to follow-up on reported deficiencies in Landowner's exercise of stormwater BMPs; or (ii) to address lack of submitted documentation Landlord is required to submit to the City; or (iii) to respond to citizen complaints. The City shall provide Landowner with copies of the inspection findings, including any directive to perform maintenance, repairs or replacements, if necessary, to the stormwater conveyance system on the Property.
7. Default by Landowner/Remedies. If Landowner fails to maintain the stormwater BMPs and associated stormwater conveyance system in good working condition acceptable to the city, the City may enter upon the Property and take whatever steps necessary to correct deficiencies, including those identified in the inspection report. Landowner is responsible to pay the costs the City incurred for those repairs. The City will provide an itemized list of the repairs in an invoice to Landowner, which is due within 30 days of the date on the invoice. To secure any amount owed by Landowner to the City under this Paragraph, the City has the right to place a lien against the Property in the same manner as delinquent taxes,

including accruing interest, penalties and administrative expenses until the lien is fully satisfied.

It is expressly understood and agreed that the City is under no obligation to inspect, maintain or repair the stormwater BMPs or stormwater conveyance system; and in no event shall this Agreement be construed to impose those obligations on the City.

8. No Liability of the City. This Agreement imposes no liability of any kind whatsoever on the City and the Landowner agrees to hold the City harmless from any liability if the stormwater BMPs and/or stormwater conveyance system failure to operate properly.
9. Compliance with other Laws. This Agreement does not replace or change the requirements of the Landowner to comply with all other applicable federal, state and local laws, rules and regulations; specifically including, without limitation, Chapter 75 of the Code of Ordinances (Stormwater Management).
10. Binding Effect/Third Parties. This Agreement is binding on and shall inure to the benefit of the parties to this Agreement and their respective successors. In this regard, this Agreement shall constitute a covenant running with the Property, and shall be binding on Landowner and any grantees or successors that acquire title to all or any part of the Property. The parties do not intend to confer any benefits on any person, firm, corporation, or other entity which is not party to this Agreement.
11. Governing Law. This Agreement is governed under applicable Michigan law. Both parties had the assistance of or the opportunity to seek legal counsel regarding the signing of this Agreement. Therefore, no construction or ambiguity of this Agreement is resolved against either party.
12. Waiver. A party does not waive any of its rights under this Agreement if that party fails to complain about an act or omission by the other party, no matter the duration of that act or omission. And a waiver by either party, whether expressed or implied, of any breach of a provision in this Agreement is not considered a waiver or consent to any subsequent breach of this same or other provision.
13. Exhibits. This Agreement includes the following exhibits Landowner agrees to provide:
  - Exhibit A:** Legal description of the real estate for which this Agreement applies ("Property").
  - Exhibit B:** Location map(s) showing a location of the Property and an accurate location of each stormwater BMP affected by this Agreement.
  - Exhibit C:** A List of all stormwater BMPs, including Manufacturer, Model, and locational reference to Exhibit B.

- 14. Headings. Headings in this Agreement are for convenience only and are not intended to interpret or construe its provisions.
- 15. Entire Agreement/Counterparts. This Agreement supersedes all agreements previously made between the parties relating to the subject matter. There are no other understandings or agreements between them. The parties may sign this Agreement in counterparts, which together shall comprise a single agreement, and the effective date for which is the date it is signed by both parties.
- 16. Authorization. Each of the parties represents and warrants to the other that this Agreement and its execution by the individual(s) on its behalf are authorized by the city commission, the board of directors or other governing body or organizational agreement of that party.
- 17. Definitions. The terms set forth in this Agreement shall have the same meaning as commonly used, except any term that is defined under statutes, ordinances or laws identified above, or any other applicable state statute shall have the meaning set forth under that ordinance, statute or law, including any subsequent amendments.
- 18. Recording. This agreement shall be recorded with the Kalamazoo County Register of Deeds.

Dated: \_\_\_\_\_

LANDOWNER

\_\_\_\_\_  
By: \_\_\_\_\_

Its: \_\_\_\_\_

**[This notary section for use by Corporations]**

STATE OF MICHIGAN        )  
  )SS.  
COUNTY OF KALAMAZOO )

On this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, before me personally came the above named \_\_\_\_\_, to me personally known, who being duly sworn did and each for himself say that he/she is the \_\_\_\_\_ for said corporation named \_\_\_\_\_ in and who and that said instrument was signed on behalf of said corporation by authority of its \_\_\_\_\_; and said \_\_\_\_\_ acknowledges said instrument to be the free act and deed of said corporation.

\_\_\_\_\_, Notary Public

\_\_\_\_\_ County, Michigan

My commission expires: \_\_\_\_\_



When Recorded Return To:  
City of Portage Department of Transportation & Utilities  
7900 S. Westnedge Avenue  
Portage, MI 49002

THIS INSTRUMENT PREPARED BY:

[Insert name and address of person]

PATRICK D. FLANAGAN

1209 E. MILHAM ROAD, SUITE B

PORTAGE, MI 49002

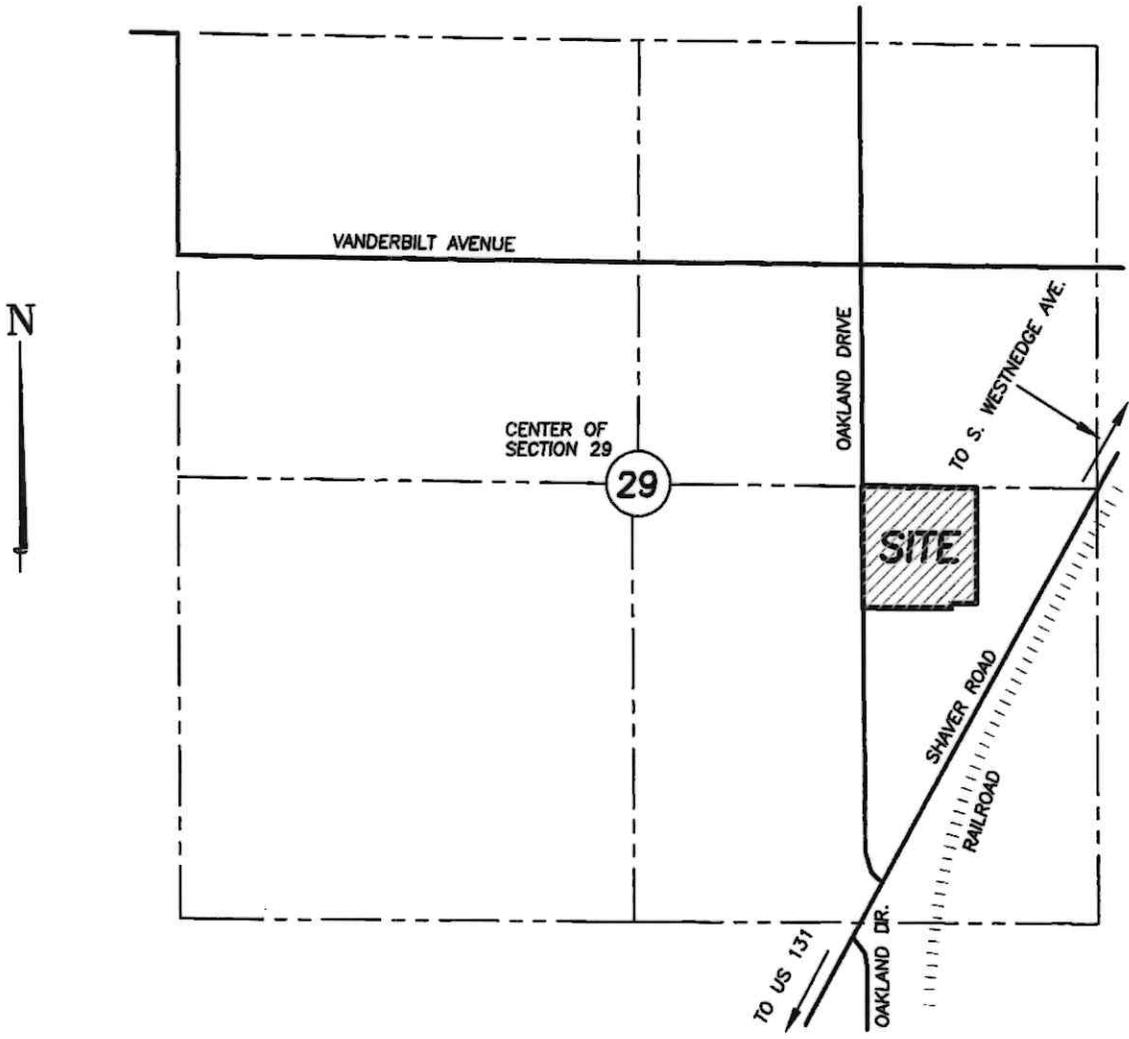
\_\_\_\_\_  
\_\_\_\_\_

# EXHIBIT "A"

## PARCEL DESCRIPTION PER CITY WEBSITE:

A PARCEL OF LAND SITUATED IN THE SE ¼ OF SEC 29, 3S, 11W: COMMENCING AT THE EAST ¼ COR OF SEC 29, 3S, 11 W; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS SOUTH 89 DEG 55' 07" WEST) 331.47 FT ALG THE NORTH LI OF THE SE ¼ OF SD SEC TO REFERENCE POINT "A"; TH SOUTH 90 DEG 50' 19" EAST (PREVIOUSLY RECORDED AS NORTH 90 DEG 55' 07" EAST) 331.47 FT ALG SD NORTH LI TO SD EAST ¼ COR; TH NORTH 90 DEG 59' 23" EAST (PREVIOUSLY RECORDED AS EAST) 116.95 FT ALG THE SOUTH LI OF THE NW ¼ OF SEC 28 3S, 11W TO THE NORTHWESTERLY ROW LI OF SHAVER RD; TH SOUTH 29 DEG 38' 07" WEST (PREVIOUSLY RECORDED AS SOUTH 29 DEG 39' WEST) 152.31 FT ALG SD ROW LI; TH NORTH 60 DEG 21' 53" WEST 145.41 FT; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS NORTH 89 DEG 55' 07" EAST) 198.90 FT PARALLEL W SD SOUTH LI; TH NORTH 44 DEG 50' 19" WEST 67.78 FT TO A LI EXTENDING SOUTH 00 DEG 05' 30" EAST FROM SD REFERENCE POINT "A"; TH NORTH 00 DEG 05' 30" WEST 12.78 FT TO SD REFERENCE POINT "A"; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS SOUTH 89 DEG 55' 07" WEST) 278.54 FT ALG SD NORTH LI TO THE POB; THN SOUTH 00 DEG 09' 41" WEST 753.56 FT; TH NORTH 89 DEG 50' 19" WEST 163.82 FT (PREVIOUSLY RECORDED AS NORTH 89 DEG 55' 07" EAST) PARALLEL W SD NORTH LI; TH SOUTH 00 DEG 09' 41" WEST 26 FT; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS SOUTH 89 DEG 55' 07" WEST) 543.56 FT PARALLEL W SD NORTH LI TO THE WEST LI OF THE EAST ½ OF SD SE ¼ AS ESTABLISHED BY "OAKLAND FARMS NORTH" SUBDIVISION; TH NORTH 00 DEG 04' 14" EAST (PREVIOUSLY RECORDED AS NORTH) 779.56 FT ALG SD WEST LI OF THE EAST ½ TO SD NORTH LI OF THE SE ¼ ; TH SOUTH 89 DEG 50' 19" EAST (PREVIOUSLY RECORDED AS NORTH 89 DEG 55' 07" EAST) 708.61 FT ALG SD NORTH LI OF THE SE ¼ TO THE POB, CONTAINING 12.57 ACRES OF LAND. THE WESTERLY PORTION BEING SUBJECT TO EASEMENT FOR OAKLAND DR. SPLIT/COMBINED ON 08/20/2018 FROM 00029-425-E, 00029-425-C;

# EXHIBIT "B" (PAGE 1)



VICINITY MAP  
NOT TO SCALE



# EXHIBIT "C"

## Storm Sewer Facilities:

- All storm sewer pipe is ADS "N-12" HDPE pipe (dual wall, smooth inside, corrugated outside) or Reinforced Concrete Pipe.
  - All storm sewer castings are made by East Jordan Iron Works of East Jordan, Michigan
  - All storm sewer structures are 24", 48", or larger pre cast structures (made by Bush Concrete of Muskegon, Michigan).
  - Includes 12" culvert and two 24" diameter storm sewer structures near entryway (within Oakland Drive right-of-way) which shall be maintained by the property owner.
  - Includes KSI storm water treatment device which shall be maintained by the property owner. (KSI 3000 Series) See attached pages & see project Final Plan.
- 

## Storm Sewer Inspection & Maintenance Requirements:

- Storm sewer structures shall be inspected at least once per year. Sediment and debris shall be removed as needed, and when sump is half full of sediment (or fuller).
- All other storm sewer pipe and facilities shall be inspected at least once per year. Maintenance shall occur and facilities shall be cleaned as needed.

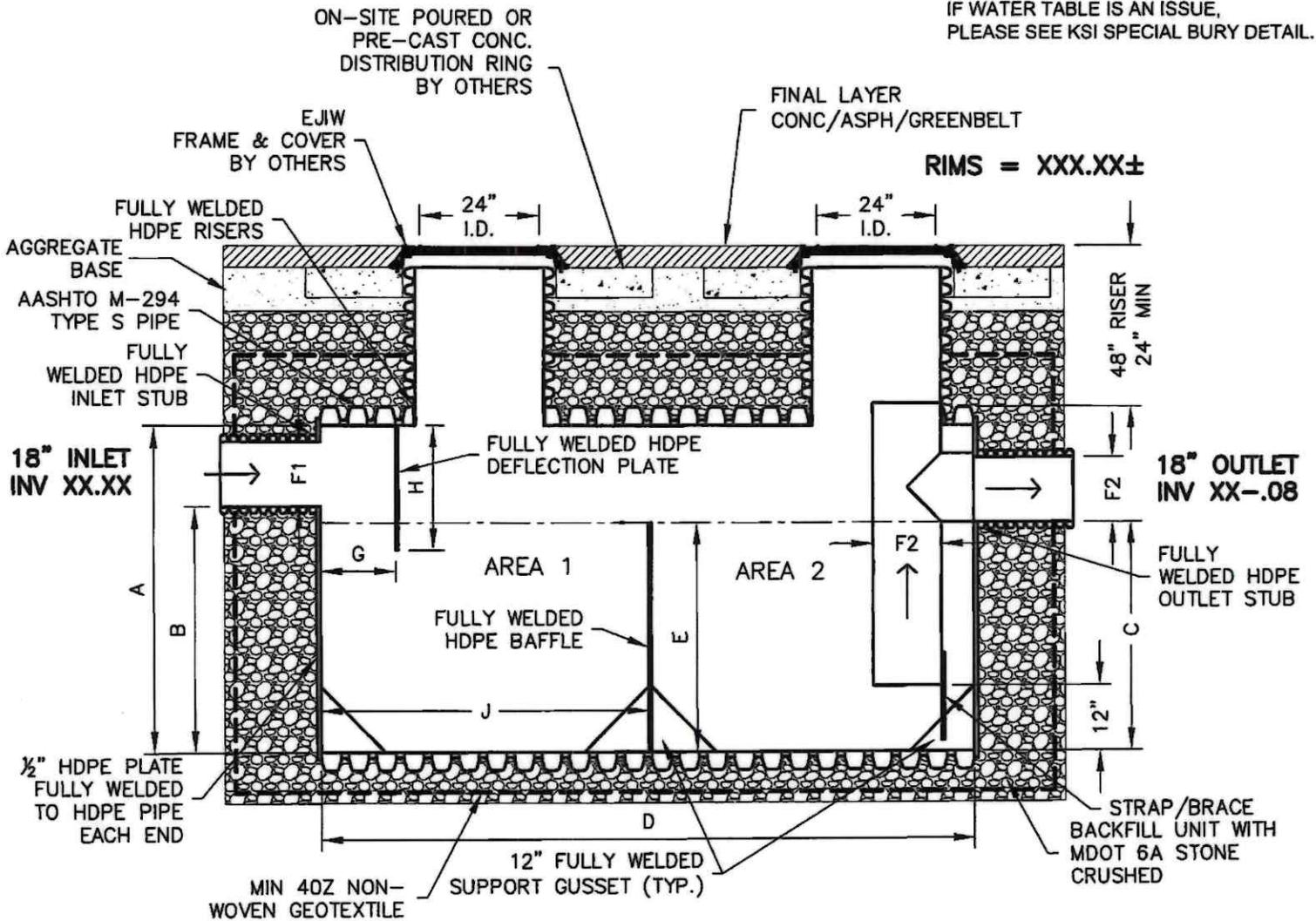
# STORM WATER TREATMENT CHAMBER

## KSI SERIES 3000 HDPE CHAMBER

**NOTE:**  
INSTALL UNIT PER ASTM D2321

4 OZ NON-WOVEN GEOTEXTILE  
FABRIC WRAP REQUIRED  
SEE TRENCH DETAIL

IF WATER TABLE IS AN ISSUE,  
PLEASE SEE KSI SPECIAL BURY DETAIL.



**CROSS SECTION**

### STORM WATER TREATMENT CHAMBER DIMENSIONS AND CAPACITIES

MODEL NO.	A	B	C	D	E	F1	F2	G	H	J	AREA 1	AREA 2
3000-60-18	60"	39"	38"	298"	38"	18"	18"	18"	24"	149"	162 CF	274 CF

DESIGN BY: <b>AG / RK</b>	REV: 10-21-22 DATE: 10-10-08	<b>2 OF 2</b> SHEET NO.
MANUF. APPROVAL BY: .	SCALE: <b>NTS</b>	

**KSI** KENNEDY SOLUTIONS, INC.  
10051 Bergin Road  
Howell, MI 48843  
Ph: 800-896-046

# STORM WATER TREATMENT CHAMBER TRENCH DETAIL

**NOTES:**

1. FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH A FOUNDATION OF MDOT 6A CRUSHED STONE AS DEFINED IN ASTM D2321, LATEST EDITION; AS AN ALTERNATIVE TRENCH BOTTOM MAY BE STABILIZED USING A WOVEN GEOTEXTILE FABRIC AND OR A GEOGRID.
2. BEDDING: SUITABLE MATERIAL SHALL BE MDOT 6A CRUSHED STONE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION. UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, MINIMUM BEDDING THICKNESS SHALL BE 12" FOR 36"-120"Ø SWTC.
3. HAUNCHING AND INITIAL BACKFILL: SUITABLE MATERIAL SHALL BE MDOT 6A CRUSHED STONE AND INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION, MAXIMUM TEN INCH (10") LIFTS.
4. UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, MIN. TRENCH WIDTHS ARE AS FOLLOWS.

<u>NOMINAL I.D.</u>	<u>NOMINAL O.D.</u>	<u>MINIMUM RECOMM. TRENCH WIDTH</u>
36"	42"	90"
48"	54"	102"
60"	66"	114"
72"	80.5"	129"
96"	105.9"	154"
120"	131.3"	190"

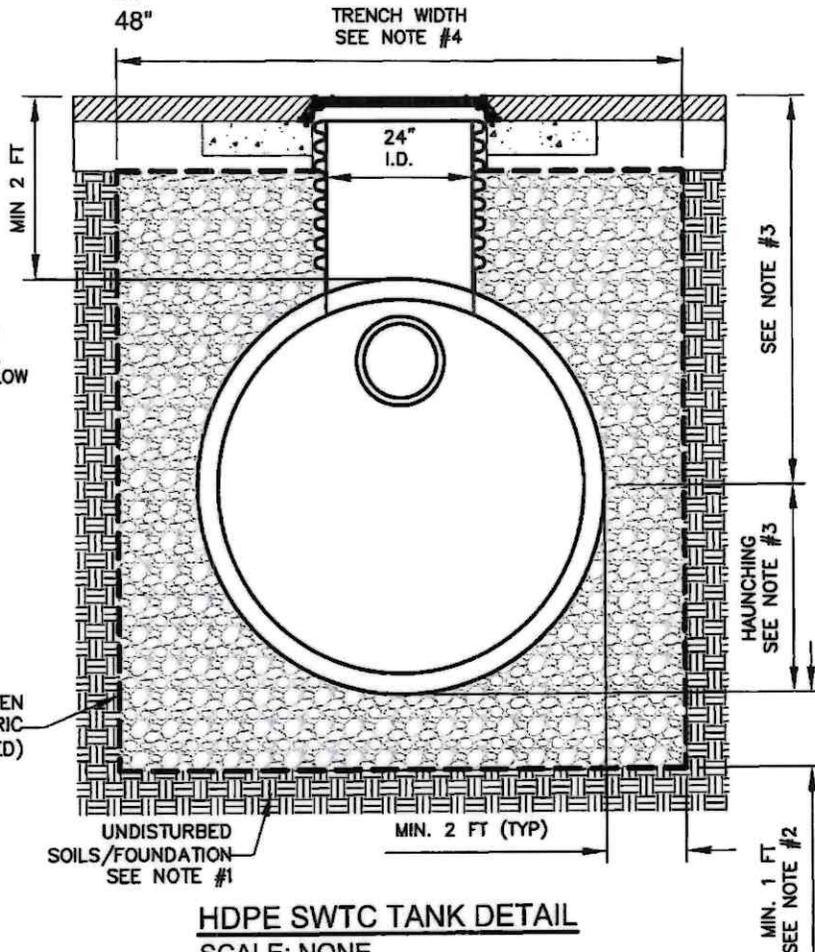
5. MINIMUM COVER: MINIMUM RECOMMENDED COVER FOR VARIOUS LIVE LOADING CONDITIONS ARE AS SUMMARIZED IN THE FOLLOWING TABLE. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TAKEN FROM THE TOP OF THE SWTC TANK TO THE GROUND SURFACE.

<u>SURFACE LIVE LOAD</u>	<u>MINIMUM REQUIRED COVER FOR WAYNE COUNTY (ALL SIZES)</u>
HS-25/H25 (FLEXIBLE PAVEMENT)	MIN. 24" AS MEASURED FROM THE BOTTOM OF THE FLEXIBLE PAVEMENT
HS-25/H25 (RIGID PAVEMENT)	MIN. 24" AS MEASURED FROM THE TOP OF THE RIGID PAVEMENT
E80 RAILWAY	24"
HEAVY CONSTRUCTION	48"

**NOTE:**  
ALSO SEE RISER  
INSTALLATION  
OPTIONS PAGE

**NOTE:**  
FOR INSTALLATIONS WITH  
LESS THAN 2 FT COVER,  
CONTACT KSI FOR SHALLOW  
BURY ASSISTANCE. SEE  
SHALLOW BURY DETAIL

4 OZ NON-WOVEN  
GEOTEXTILE FABRIC  
(REQUIRED)



**HDPE SWTC TANK DETAIL**  
SCALE: NONE

**NOTE:**  
INSTALL UNIT PER ASTM D2321

4 OZ NON-WOVEN GEOTEXTILE  
FABRIC WRAP REQUIRED

IF WATER TABLE IS AN ISSUE,  
PLEASE SEE KSI SPECIAL BURY DETAIL.

DESIGN BY: <b>AG / RK</b>	REV: 11-17-23 DATE: 10-10-08	1 OF 1
MANUF. APPROVAL BY: .	SCALE: <b>NTS</b>	SHEET NO.



**KENNEDY SOLUTIONS, INC.**  
10051 Bergin Road  
Howell, MI 48843  
Ph: 800-698-7046

# STORM WATER TREATMENT CHAMBER INSTALLATION AND MAINTENANCE GUIDELINES

## INSTALLATION GUIDELINES

1. EXCAVATE AREA FOR KSI SWTC AND PREPARE TRENCH BOTTOM PER ASTM D2321, SECTIONS 6 & 7.
2. THE KSI SWTC SHALL BE INSTALLED ON A BED OF NO LESS THAN 12" MDOT 6A CRUSHED STONE MATERIALS COMPACTED TO 95% PROCTOR DENSITY. COMPACTED TO 95% (90% MIN. FOR MDOT 6A CRUSHED STONE MATERIAL) OF THE BACKFILL MATERIAL'S MAXIMUM WEIGHT AT A MOISTURE CONTENT NOT GREATER THAN THE OPTIMUM.
3. INSTALL KSI SWTC UNIT, HIGH FLOW BYPASS LINE (IF APPLICABLE), DIVERSION STRUCTURE AND EXITING STRUCTURE AT ELEVATIONS INDICATED ON SITE PLAN. COUPLE INLET AND OUTLET HDPE SPLIT COUPLERS TO CONVEYANCE PIPE.
4. BACKFILL UNIT WITH MDOT 6A CRUSHED STONE MATERIALS PER ASTM D2321. BACKFILL SHALL BE COMPACTED TO 95% PROCTOR DENSITY.
5. THE HDPE ACCESS RISERS SHALL BE FIELD CUT TO FINISH GRADE BY THE CONTRACTOR. SEE RISER INSTALLATION OPTIONS PAGE.
6. KSI RECOMMENDS FILLING THE UNIT WITH WATER UPON COMPLETION OF INSTALLATION UP TO THE BAFFLE HEIGHT.
7. ONCE THE UNIT IS INSTALLED, PLACE A ORANGE SAFETY FENCE 4-5 FT HIGH WITH TEE POST, AROUND THE SYSTEM. PLACE FENCE 5 FT BEYOND GRID FOOTPRINT. ANY DAMAGE TO THE SYSTEM AS A RESULT OF NOT FOLLOWING THESE INSTRUCTIONS AND THE BLUEPRINT DETAILS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR THE SYSTEM TO KSI'S SATISFACTION. IF GIVEN IN WRITING, A 3-4 DAY LEAD TIME, KSI WILL HAVE A REPRESENTATIVE AVAILABLE ON SITE DURING THE INSTALLATION.

## MAINTENANCE GUIDELINES

1. ALL STORM WATER TREATMENT CHAMBERS WILL REQUIRE PERIODIC MAINTENANCE DEPENDING ON SPECIFIC SITE CONDITIONS.
2. KSI RECOMMENDS CLEANING THE SWTC QUARTERLY AND AFTER HEAVY RAIN STORMS. SEDIMENT IS EASIER TO REMOVE WHEN IT IS REMOVED ON A REGULAR BASIS.
3. DISPOSAL OF MATERIAL FROM THE KSI SWTC ARE SIMILAR TO THAT OF ANY OTHER BEST MANAGEMENT PRACTICES (BMP). LOCAL GUIDELINES SHOULD BE CONSULTED PRIOR TO DISPOSAL OF THE SWTC CONTENTS. PETROLEUM WASTE PRODUCTS SHOULD BE REMOVED BY A LICENSED WASTE MANAGEMENT COMPANY.
4. IF A HYDROCARBON REMOVAL SYSTEM WAS INSTALLED - REPLACE IT WHEN IT TURNS BLACK. UNIT CAN BE DISPOSED OF VIA NORMAL REFUSE REMOVAL. SPENT UNIT DOES NOT LEACH CAPTURED CONTAMINATES.

WHITE = NEW

GRAY = WORKING

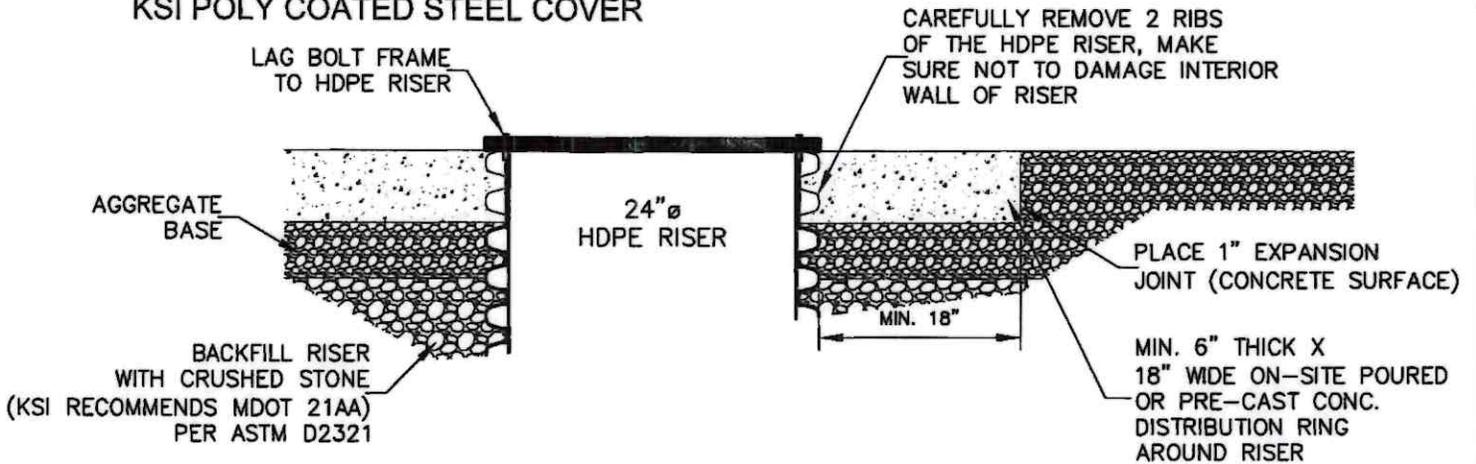
BLACK = SPENT - NEEDS REPLACEMENT - CONTACT KSI FOR REPLACEMENT PARTS

5. AFTER CLEANING THE UNIT - KSI RECOMMENDS REFILLING THE UNIT WITH WATER.

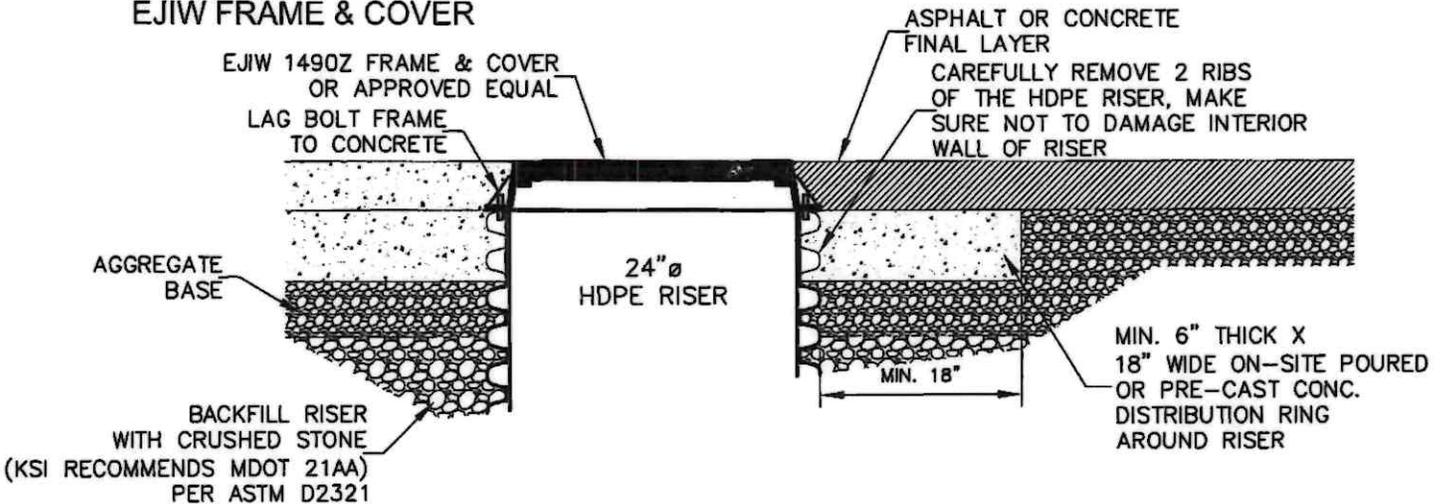
DESIGN BY: <b>AG / RK</b>	REV: 4-1-22 DATE: 10-10-08	<b>1 OF 1</b>		<b>KENNEDY SOLUTIONS, INC.</b> 10051 Bergin Road Howell, MI 48843 Ph: 800-699-4046 (148)
MANUF. APPROVAL BY: .	SCALE: <b>NTS</b>	SHEET NO.		

# KSI RISER INSTALLATION OPTIONS FOR PAVED SURFACE APPLICATIONS

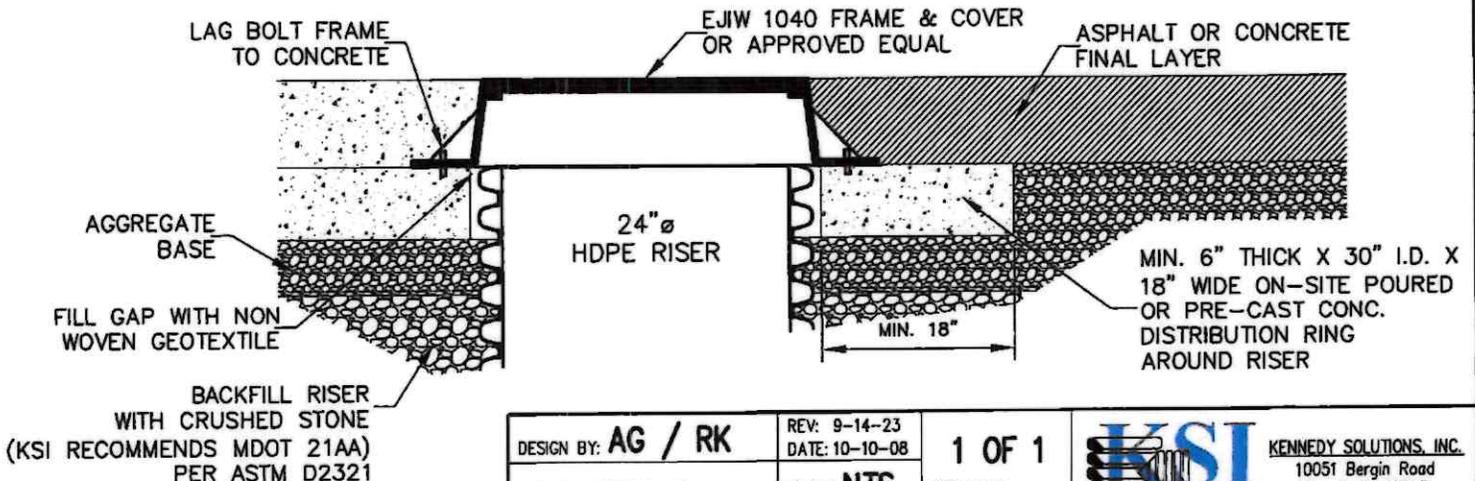
## OPTION 1: KSI POLY COATED STEEL COVER



## OPTION 2: EJIW FRAME & COVER



## OPTION 3: FROST SITUATION EJIW FRAME & COVER



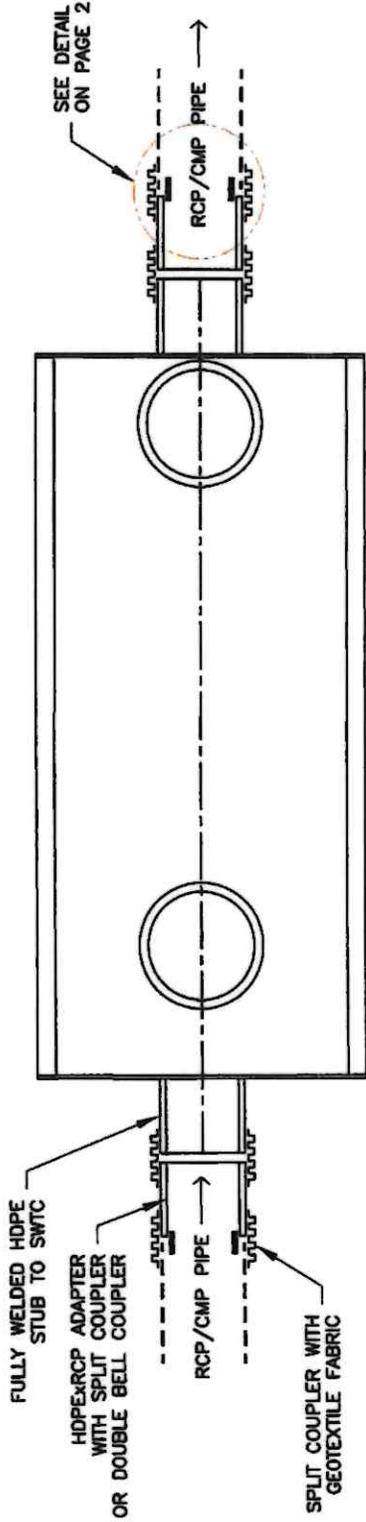
DESIGN BY: AG / RK	REV: 9-14-23 DATE: 10-10-08	1 OF 1
MANUF. APPROVAL BY: .	SCALE: NTS	SHEET NO.
DRAWING NO. RISER INSTALLATION		



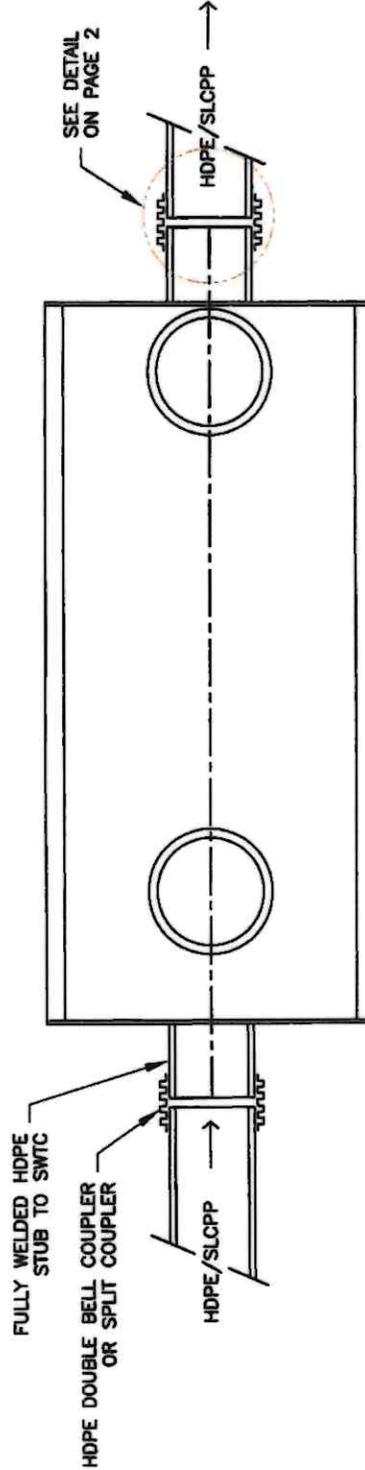
**KENNEDY SOLUTIONS, INC.**  
10051 Bergin Road  
Howell, MI 48843  
Ph: 800-699-4046

(149)

# STORM WATER TREATMENT CHAMBER CONNECTION OPTIONS



## CONNECTION TO RCP/CMP PIPE



## CONNECTION TO HDPE (SLCPP) PIPE

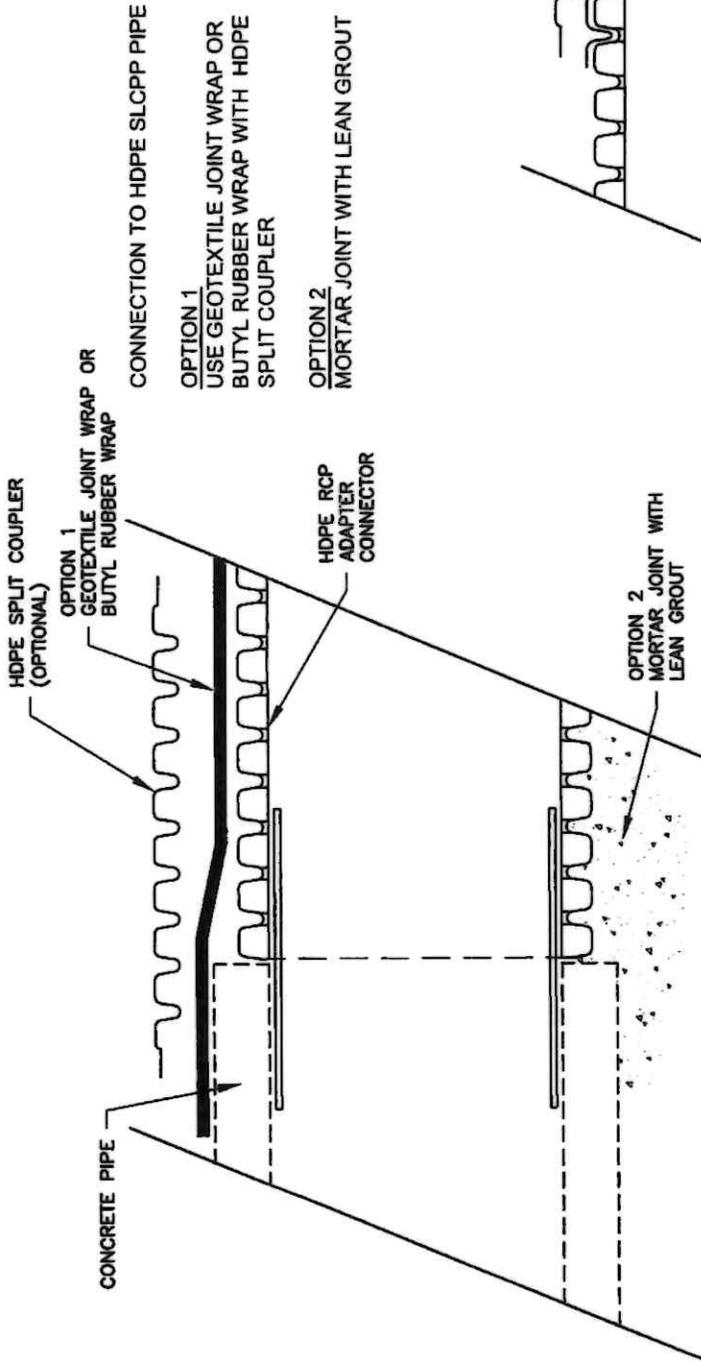
IN WAYNE COUNTY  
1. NEW CONSTRUCTION USE - DOUBLE BELL COUPLERS WITH GASKETS.  
2. RETRO-FIT CONSTRUCTION USE - DOUBLE BELL COUPLER UPSTREAM AND SPLIT COUPLER DOWNSTREAM.

DESIGN BY: <b>AG / RK</b>	REV. DATE: 5-31-16	1 OF 2 SHEET NO.
MANUF. APPROVAL BY: .	SCALE: <b>NTS</b>	
DRAWING NO. <b>SWTC CONNECTION OPTIONS</b>		



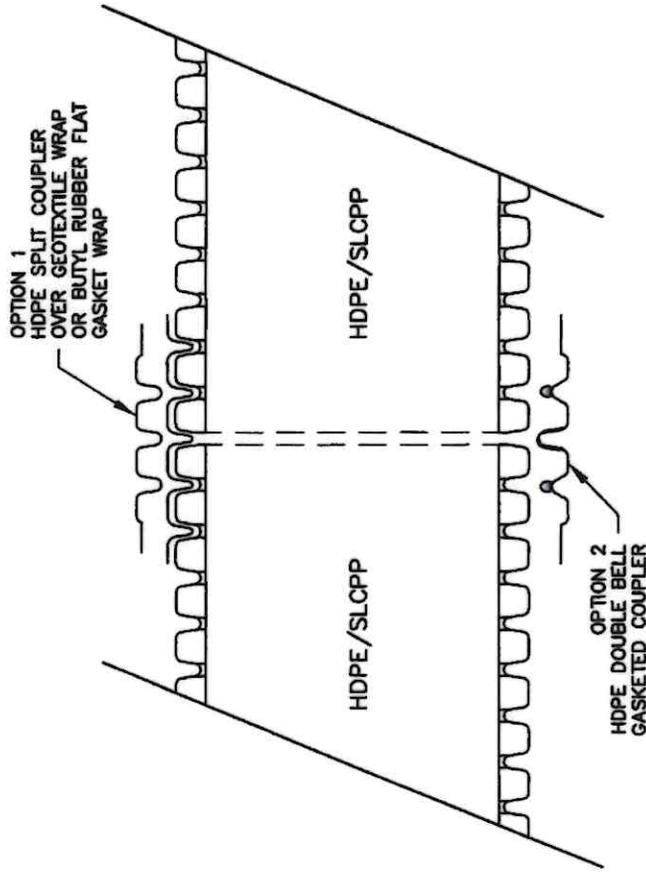
KENNEDY SOLUTIONS, INC.  
10051 Bergin Road  
Howell, MI 48843  
Ph: 800-699-4046

# STORM WATER TREATMENT CHAMBER CONNECTION OPTIONS



## CONNECTION TO RCP/CMP PIPE

- CONNECTION TO HDPE SLCPP PIPE
- OPTION 1  
USE GEOTEXTILE JOINT WRAP OR BUTYL RUBBER WRAP WITH HDPE SPLIT COUPLER
- OPTION 2  
HDPE DOUBLE BELL COUPLER WITH GASKETS BOTH SIDES



## CONNECTION TO HDPE (SLCPP) PIPE

DESIGN BY: <b>AG / RK</b>	REV. DATE: 5-31-16	2 OF 2 SHEET NO.
MANUF. APPROVAL BY: .	SCALE: <b>NTS</b>	
DRAWING NO. <b>SWTC CONNECTION OPTIONS</b>		



KENNEDY SOLUTIONS, INC.  
10051 Bergin Road  
Howell, MI 48843  
Ph: 800-699-4046

**TO:** Planning Commission

**DATE:** August 17, 2023

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** A Rezoning and Tentative Plan Application (#23/24-1), from I-1 Light Industrial District to Planned Development for a proposed residential development at 9581 Oakland Drive.

## **I. INTRODUCTION:**

The applicant (Mike West, representative of Green Development Ventures, LLC) proposes to establish a residential development at 9581 Oakland Drive by filing a Rezoning and Tentative Plan Application to rezone the property from I-1 Light Industrial district to PD Planned Development district.

The proposed development is called ‘Oakland Commons’ and consists of a total of 58 new homes with a mix of fifty-eight detached and attached units, on a 12.57-acre site, new private drive, new utilities, open space, and pedestrian connections. All homes will be available to rent. All common areas including the private drive, sidewalks/ trail, storm water, general lawn areas, etc. will be maintained by the owner-developer (associated with the applicant). All homes will be designed with 2-stories, sized 1,440 square feet - 1,640 square feet, consisting of 3-4 bedrooms, 2-3 bathrooms, and 1-2 car garages. A new left-turn lane on Oakland Drive will be provided to facilitate the project’s traffic into a proposed private drive. A common guest parking area will also be provided. A sidewalk will be provided and connected to all homes, and link to a new trail leading to the common open space of the development. A new stormwater basin will be constructed in the northwest corner. An emergency-only access driveway will be constructed and connected to the existing private driveway on the property to the east (Weathervane Self Storage-owned by JC Customs, LLC). These proposed development items are described and shown in the applicant’s attached narrative, site plan drawing, and concept images.

The applicant may seek development incentives for the creation of approximately 20% ‘workforce housing’ income level rental units. This has not been submitted for City’s consideration.

Below is a list of proposed home types:

- Detached Single Family homes: 26 homes
- Duplex homes: 6 homes
- 3-plexes: 18 townhomes
- 4-plexes: 8 townhomes
- Total: 58 Units
- Proposed Density: 4.6 Units/ Acre

If the proposal is approved, the applicant would purchase the property from the current landowner- JC Customs, LLC, who is the same landowner of the Weathervane Self Storage to the east. The current landowner has agreed to the proposal. A cross-access agreement with JC Customs, LLC will be established to allow the use of the proposed emergency-only access drive.

The development will avoid the existing wetlands located in the northern section of the property.

**II. BACKGROUND INFORMATION:**

In 2018, the subject property was split off from the site to the east (9600 Shaver Road; Weathervane Self Storage). This results in the subject site having no access to Shaver Road, and the only access is along Oakland Drive.

The following table summarizes **Existing Conditions** in this general area:

Existing Land Use/ Zoning	<ul style="list-style-type: none"> <li>• The subject site is undeveloped and zoned I-1 Light Industrial.</li> <li>• <u>North (across wetlands)</u>: Single Family Homes (9247 Oakland Dr., 2021 Vanderbilt Ave., and 2011 Vanderbilt Ave., ‘High Performance Metal Finishing’ business; Zoned: R-1C and I-1 Light Industrial.</li> <li>• <u>South</u>: Romence Gardens (plant nursery), 9660 Shaver Road; Zoned I-1 Light Industrial.</li> <li>• <u>East</u>: Weathervane Self Storage, 9600 Shaver Road; Zoned I-1 Light Industrial</li> <li>• <u>West (across Oakland Dr)</u>: Single-family Homes (9520 Oakland Dr., 9526 Oakland Dr., 9590 Oakland Dr., and 9608 Oakland Dr.); Zoned R-1C Single Family Residential.</li> </ul>
Public Streets	The surrounding roadway network: Oakland Drive – 2022 Kalamazoo Area Transportation Study (KATS) recorded 4,552 total AADT (Annual Average Daily Trips) trip counts along Oakland Drive north of Shaver Road intersection.
Public Utilities	City sanitary sewer and water are available along Oakland Drive.
Environment	The subject site is undeveloped. It has some perimeter natural vegetation along Oakland Drive. There is a wetland on the northern portion of the site but most of it is relatively level topography.
Unique Conditions	There are mapped wetlands in the northern portion of the site. There are no mapped historic buildings/ structures, or flood hazards on the site.

**Applicable Zoning Regulations**

The following sections of Chapter 42. Division 4. Zoning Districts and District Regulations apply and are addressed further in this report:

- [Subdivision 11](#) – PD, Planned Development District, and
- [Division 8](#) – Administration and Enforcement (for Rezoning Application)

**Proposed Planned Development Tentative Plan**

The applicant has submitted a Rezoning application and a Tentative Plan application seeking approval for a residential development consisting of 58 dwelling units per Sec 42-375. The Planned Development review process involves the following two steps per [Sec. 42-375](#):

Step 1) Tentative Plan review (Items A-G, Sec 42-375) identifies part(s) or whole project features; analyzes the impact on neighborhoods, streets, utilities, and the natural environment; and determines consistency with adopted local plans. The filed Tentative Plan is at this step.

Step 2) Final Plan review (Items H-M, Sec 42-375), identifies and analyzes detailed drawings of project improvements (streets, utilities, lot dimensions, etc.). A Final Plan application has not been submitted at this time, awaiting approval of the Tentative Plan.

The Planning Commission shall review the project for consistency with Sec 42-375 Tentative Plan items A-E during a public hearing and provide a recommendation to City Council. The applicant has submitted a project narrative addressing PD criteria under Sec 42-375 and generally describing the development; site drawings showing general building footprints, setback distances, parking and landscaping, and common areas; and concept renderings of the site and building elevations.

Per Sec 42-373 and -374, PD allows the developer to incorporate a variety of housing styles. The following project features are being sought under the PD district:

- The development consists of four housing types: traditional single-family detached homes, duplexes, and 3-plex and 4-plex townhomes. The entire development will be within the existing single property boundary; no land divisions or subdivisions are proposed. The applicant is proposing unique building setbacks for the specific project, as provided in the table below and explained further in the applicant’s narrative.

**Proposed Yard Setbacks**

<b>Unit Type</b>	<b>Front (to private drive)</b>	<b>Side (between buildings)</b>	<b>Outer perimeter</b>	<b>From Oakland Drive</b>
Single Family detached	20'	12'	30'	30'
Duplex	25'	18'	30'	30'
3-plex	25'	30'	30'	30'
4-plex	25'	30'	30'	30'

Per Sec 42-374 of PD, there are no defined building yard setbacks under PD district. However, the Planning Commission shall review the proposed building locations to generally determine neighborhood

appropriateness. Therefore, if the project is approved, the proposed setbacks as provided above would become the established yard setbacks for the subject PD project. In the typical R-1A thru R-1T (single family detached and townhouses) districts, front yard setbacks are 27'- 30' to the right-of-way, side yard setbacks are 8' - 12' to side property lines, and rear yard setback is 40' to the rear property line.

Utilities

The development will connect to City water and sewer utilities within Oakland Drive. A new private stormwater basin will be constructed in the northwest corner of the site. The basin and all other common open spaces will be maintained by the owner-developer.

Transportation

Vehicular access to the property will be provide by a new private drive connected to Oakland Drive, to be constructed and maintained by the property owner. The applicant will install a new left-turn lane into the project on Oakland Drive. This private drive will provide direct access to each dwelling unit. As required by the City Fire Department, an emergency access driveway is designed in the southeast corner connecting to an existing private parking lot to the east at the Weathervane Self Storage (9600 Shaver Road). A cross-access easement and agreement will ensure this cross-access is formalized. The agreement will be required to be recorded to ensure both existing and future property owners continue to allow for this cross-access.

Pedestrian/ bicyclist access will be accommodated throughout the project with internal and exterior pathways. A 4' wide sidewalk will be provided along one side of the internal private drive and lead directly to each dwelling unit. A 6' wide walking trail (asphalt surface) will be provided within the central common space and connect to the front of most of the single-family detached homes. This trail will also connect to a proposed 6' sidewalk on the entire frontage adjacent to Oakland Drive, consistent with the City's Complete Streets Policy. Further, the trail will extend along the wetlands to a proposed viewing platform area with benches. Sidewalks were required to be installed with the nearby expansion of the Prairie Edge Church (9316 Oakland Drive) in 2005.

**III. ANALYSIS:**

Staff analysis has been prepared based on items A-G, Sec 42-375 PD Tentative Plan requirements, Comprehensive Plan, surrounding development patterns, and general land use considerations. Issues to be analyzed include consistency with the Future Land Use Map, preliminary evaluations from Development Review Team (DRT), suitability of the existing zoning classification, and the impacts of the proposed zoning classification.

Comprehensive Plan

Rezoning applications are reviewed for consistency with the City of Portage Comprehensive Plan which establishes long term land use goals, and the Future Land Use Map. The following goal is relevant to the proposal:

- Goal LU1: Provide a range of housing opportunities to meet the needs for income levels for current and future residents.
- 

The Future Land Use map designates the subject property as ‘Shaver Road Business Corridor’, which is generally defined as an area along Shaver Road designated for general industrial use. Properties to the north and west (across Oakland Drive) are classified as ‘Low Density Residential’, defined as residential single family detached housing development at densities between 1 and 4 units/ acre; corresponding zoning districts include R-1A thru R-1D and Planned Development. The subject site used to be combined with the site to the east (Weathervane Self Storage; 9600 Shaver Road). However, as a result of the land division, the subject site no longer has access to Shaver Road. Its only public access is from Oakland Drive. The site to the south is developed with a plant nursery (Romence Gardens). The sites to the north are developed with single family homes but are separated from the subject site by wetlands.

As developable land becomes scarce and housing demand continues to increase within the Portage area, the need to facilitate higher density housing also increases. While housing demand remains high, industrial land continues to be desirable in the Portage and greater Kalamazoo region. Most industrial zoned land is located within industrial parks or fronts along industrial corridors. The subject site used to front the Shaver Road industrial corridor and continues to be classified as the Shaver Road Industrial Corridor. The subject lot, which only fronts Oakland Drive, is smaller in overall size after being split, and is partially encumbered by wetlands, might be less desirable for an industrial user. Oakland Drive is primarily developed with low- to medium-density housing and, therefore, carries mostly residential traffic. As a result of those changes and neighborhood conditions, the Future Land Use classification of industrial for the site may no longer be suitable for determining best development on the subject site.

### City Council’s Goals

The proposal of 58 detached and attached dwelling units will help meet the Council’s FY 23-24 Housing Strategies goal and the adopted Portage Attainable Housing Plan in December 2022.

### Environmental Conditions

There are mapped wetlands in the northern portion of the property. The applicant’s submitted site plan indicates the limits of the wetlands were field inspected in April 2023 by Environmental Science and Planning LLC. The applicant is not proposing any development or site improvements within the existing wetlands.

### Zoning Map

The subject site is zoned I-1 Light Industrial. Properties to the south and east are of the same zoning district. Properties to the north and west (across Oakland Drive) are zoned R-1C Single Family

Residential. The applicant seeks a rezoning of the entire property to Planned Development for the following reasons:

- Allow for residential development and incorporate a variety of housing styles at a density of 4.6 units per acre.
- Incorporate a mix of detached and attached housing.
- Provide a variety of yard setbacks which do not specifically exist within any existing residential districts.

#### Neighborhood Impacts

Staff find the applicant's proposal has incorporated an overall project design and layout that will not increase the impact on the neighborhood, for the following reasons:

- The proposed use of a mix of single family detached and townhome dwellings will not result in negative impacts on the adjacent residences or commercial/ industrial users because of the perimeter landscaping, single access point, and general low residential density.
- The proposed 2-story residential building heights are allowed in the current I-1 Light Industrial district and the adjacent R-1C One-Family Residential district.
- The preserved vegetative buffer along Oakland Drive will soften the visibility of the back of the proposed single family dwelling units and keep a more natural appearance along the road.
- The general noise impact from the proposal would be lower than a typical industrial user and, therefore, less impact on adjacent land use.

#### Preliminary Evaluations from the Development Review Team (DRT)

This project was reviewed by the Development Review Team (DRT) who ensures that the proposed development is consistent with the City's infrastructure, engineering, and other requirements including streets, sewers, water mains, storm water, parking, landscaping, and others. The DRT is comprised of representatives from the Department of Transportation and Utilities, Fire Department, Department of Parks and Recreation, Office of the City Assessor, and Department of Community Development. DRT's preliminary review does not recommend any additional improvements to the Tentative Plan. The following are DRT discussion items which have been addressed in the submitted plans:

- The applicant was encouraged to create a natural buffer along Oakland Drive. The applicant is providing this through preserving existing vegetation as much as possible.
- The applicant was also encouraged to buffer the proposed homes from industrial uses to the east (Weathervane Self Storage) and south (Romence Gardens). The applicant is providing this buffer through new landscaping.
- The applicant was required to provide a left-turn lane on Oaklan Drive for south-bound traffic into the new private drive to mitigate traffic congestion along Oakland Drive. This has been incorporated into the applicant's Tentative Plan.

- A traffic impact study was not required for this proposal because Oakland Drive can accommodate additional vehicular trips with the proposed left-turn lane by the applicant.

The DRT also provided additional feedback to the applicant in preparation for the subsequent Final PD Site Plan (Permitting Step 2), which would be addressed prior to the Planning Commission’s review of the Final PD Site Plan.

Planned Development - Sec 42-374 Site Development Requirements and Sec 42-375 Plan review and approval (PD, planned development); Tentative Plan

Staff find these sections are met with the submitted application and discussed earlier in the report. Sec 42-375 (D - G) indicate required public hearings by the Planning Commission and City Council.

**Public Noticing Requirement**

Required public notice was sent to all property owners/occupants within 300 feet of the subject property on August 1, 2022. As well as a public notice published in the Kalamazoo Gazette on August 1, 2022. As of the date of this report (August 12, 2022) staff have not received any public comment.

**IV. RECOMMENDATION:**

If no additional public comment is requested by and the Commission is supportive of the proposed Rezoning and Tentative Plan request, staff recommend the Planning Commission to recommend to the City Council the approval of the Rezoning and Tentative Plan Application #23/24-1, from I-1 Light Industrial to PD Planned Development for Oakland Commons residential development at 9581 Oakland Drive.

**Attachments:**

1. Applicant's Rezone Application
2. Applicant's Tentative Plan Application
3. Applicant's Project Narrative
4. Applicant's Tentative Site Plan
5. Applicant's Development Rendering
6. Rezoning Notification Map
7. City's Future Land Use Map
8. Existing Zoning Map
9. Location Map
10. Staff's 8.9.23 Site Pictures



**APPLICATION FOR ZONING AMENDMENT**

Application number \_\_\_\_\_

Date 5-18-23

APPLICATION INFORMATION:

Meetings of the Portage Planning Commission are held on the first and third Thursday of each month at 7:00 p.m. in the Council Chambers of Portage City Hall, 7900 South Westnedge Avenue, Portage, Michigan. All zoning amendment applications must be properly filled out and submitted to the Department of Community Development and the zoning amendment fee paid at least 15 working days prior to the meeting at which the public hearing is held. The applicant will be notified in writing of all such public hearing/meetings. *Please note: City Staff and/or Planning Commissioners may visit the subject property while the application is under review.*

For more detailed information about the zoning amendment process, please refer to Portage Land Development Regulations, Article 4, Division 8, Subdivision 2.

TO THE PLANNING COMMISSION:

I (WE), the undersigned, do hereby respectfully make application and petition the Portage Planning Commission to amend the Zoning Ordinance and/or change the Zoning Map as hereinafter requested. In support of this application, the following is submitted:

**ZONING MAP AMENDMENT**

1. a. Platted Land:

The property is part of the recorded plat: The property sought to be rezoned is located at \_\_\_\_\_ between \_\_\_\_\_ Street and \_\_\_\_\_ Street on the \_\_\_\_\_ side of the street, and is known as Lot Number(s) \_\_\_\_\_ of \_\_\_\_\_ Plat (Subdivision). It has a frontage of \_\_\_\_\_ feet and a depth of \_\_\_\_\_ feet.

b. Unplatted Land:

The property is in acreage, and is not therefore a part of a recorded plat. The property sought to be rezoned is located and described as follows: (Indicate total acreage and parcel number).  
9581 Oakland Drive (Parcel #00029-425-G); 12.57 acres

2. a. Do you own the property to be rezoned? Yes  No

b. Name of the owner of the property to be rezoned: JC Customs, LLC (Mike Fisher)

Address 10032 West Gull Lake Drive, Richland, MI 49083

3. My (our) interest in the property and purpose for submitting the proposed Zoning Amendment: Contingent purchaser - Letter of Authorization from Owner (attached)  
Proposed "Oakland Commons Residential PD" (detached/attached units)

4. CURRENT ZONING: I-1 PROPOSED ZONING: PD

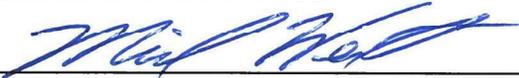
**ZONING TEXT AMENDMENT**

1. The proposed language to be considered is (attach additional sheets as necessary):  
\_\_\_\_\_  
\_\_\_\_\_

2. The Zoning Code Chapter and Section wherein the proposed text would be modified/inserted.  
\_\_\_\_\_

3. My (our) interest in and purpose for submitting the proposed Zoning Ordinance Amendment.  
\_\_\_\_\_  
\_\_\_\_\_

We attach a statement hereto indicating why, in our opinion, the change requested is necessary for the preservation and enjoyment of substantial property rights, and why such amendment will advance the public health, safety and welfare. An assessment of the impact of the proposal on the community and property of other persons in the vicinity of the amendment or affected by the amendment is also attached.

	
(Signature of Applicant)	(Signature of Applicant)
Green Development Ventures, LLC	
2186 East Centre Avenue, Portage, MI 49002	
(Address)	(Address)
(269) 365-8548 (Mike West)	
mwest@allenedwin.com	
(Phone)	(Phone)

A copy of all actions taken regarding this application shall be attached and shall be considered a part of this application.

T:\COMMDEV\2021 2022 Department Files\Forms\Zoning Forms\2022 Rezoning Application.docx



# DEVELOPMENT REVIEW APPLICATION

Department of Community Development  
7900 South Westnedge Avenue – Portage, Michigan 49002 – (269) 329-4477

Applicant must complete all items in sections 1-9 (if applicable).

Please note: Separate applications must be submitted for Plumbing, Mechanical and Electrical Work Permits

**No Work is to Start Prior to the Issuance of Building Permit**

## 1) PROJECT INFORMATION

Project Description <b>Oakland Commons PD Tentative Plan</b>	Address <b>9581 Oakland Drive (Parcel #00029-425-G); 12.57 acres</b>
--	--

## 2) IDENTIFICATION (OWNER)

Name <b>JC Customs, LLC (Mike Fisher)</b>	Address <b>10032 West Gull Lake Drive</b>
City <b>Richland</b>	State <b>Michigan</b>
Zip <b>49083</b>	Phone <b>(269) 280-2998</b>

## 3) ARCHITECT OR ENGINEER

Name <b>Monument Engineering</b>	Address <b>1209 E. Milham Ave</b>	City <b>Portage</b>
State <b>Michigan</b>	Zip <b>49002</b>	Phone <b>(269) 344-6165</b>
License Number	Expiration Date	Email

## 4) BUILDING CONTRACTOR

Company name <b>Allen Edwin Homes</b>	Address <b>2186 East Centre Avenue</b>	City <b>Portage</b>
State <b>Michigan</b>	Zip <b>49002</b>	Phone <b>(269) 365-8548</b>
Email <b>mwest@allenedwin.com</b>	Federal Employer ID	
Builder License Number	Expiration Date	
Workers Comp Insurance Carrier	MESC Employer Number	

## 5) TYPE OF DEVELOPMENT PROJECT

- Site Plan                       Building Plan                       Final Plan in Planned Development  
 Subdivision                       Condominium                       Land Division Requiring Public Improvements  
 Landscape Plans                       Public Water Main     Public Sanitary Sewer Main  
 Other: **PD Tentative Plan** *(Note: Please provide a Portable Document Format (PDF) of the plans for the project at time of document submittal. The submitted format shall be CD/DVD or USB. If a PDF is not submitted, an additional \$25 fee plus \$1 for each plan sheet after 20 sheets will apply.)*

## 6) TYPE OF IMPROVEMENT

- New Building     Alteration     Addition     Repair     Demolition     Foundation Only  
 Relocation     Special Inspection     Solar     Other: \_\_\_\_\_

## 7) DESCRIPTION OF WORK

A) **Mixed Detached/Attached Residential Community**  
**(see attached Project Summary Narrative)**

B) Valuation of Project: \$ \_\_\_\_\_

C) Any known soil and/or groundwater contamination?     Yes     No

## 8) CHARACTERISTICS OF THE BUILDING

### A) Water Meter Size

- 5/8"       3/4"       1"       1 1/2"  
 Other \_\_\_\_\_       Irrigation Size \_\_\_\_\_       Fire Sprinkler Size \_\_\_\_\_

### B) Electric Service Size

- 200 AMP     400 AMP     600 AMP     800 AMP     1000 AMP     Other \_\_\_\_\_

### C) Type of Mechanical

- Air Conditioning?  Yes     No    Fire Suppression?  Yes     No    Hood System?  Yes     No

## 9) CONSTRUCTION PLANS SUBMITTED

### Required submittals for plan review:

- Plans shall be signed and sealed when required in accordance with State of Michigan Act No.299 of Public Acts of 1980.
- All plans shall be drawn on uniform sheets no greater than 30"X42".
- All plans shall be drawn to an architectural scale.
- All plans shall be clear, legible and accurate.
- Plans shall be stapled along the left margin.

### Type of Plan Submitted:

- Building       Electrical       Plumbing       Mechanical       Energy

**Building Code:** Site plan, foundation plan, soil bearing capacity, floor plans, building elevations, building sections, framing plan, details, roof plan, roof finish schedule, roof live and dead loads.

**Electric Code:** Lighting layout, circuiting, switching, conductor and raceway sizes, wattage schedule, service location and riser diagram, load calculations, and appropriate plans showing standard symbols of all electrical equipment.

**Plumbing Code:** Site plan, floor plans, DWV riser diagrams and water distribution system and roof plan, Show direction of flow, pipe sizes, grade of horizontal piping, elevations, drainage fixture unit loading of both stacks and drains in the DWV system, supply fixture unit load for the water system, branch supplies serving more than one plumbing fixture, appliance or hose outlet, meter locations.

**Mechanical Code:** Plans indicating heating equipment, air conditioning equipment, ductwork material and layout, fire dampers, ventilation of rooms and areas, location of chimneys and vents, piping layouts, kitchen equipment layout, and combustion air. (Plans for fire suppression systems may be submitted after permit issuance, but are required prior to installation.)

**Energy Code:** Floor plans, building sections, details, average annual degree days, exterior envelope components materials, "U" values of elements, "R" values of insulating materials, size and type of apparatus and equipment, energy calculations.

**Note: Additional plan review fee(s) required of all non-concurrent plan submittals**

## APPLICANT INFORMATION

Applicant is responsible for the payment of all fees and charges applicable to this application and must provide the following information:

Name <b>Green Development Ventures, LLC</b>	Phone <b>(269) 365-8548</b> <b>Mike West</b>
Address <b>2186 East Centre Avenue</b>	City <b>Portage</b>
State <b>Michigan</b>	Zip <b>49002</b>

I hereby certify that the proposed work is authorized by the owner of record and that I have been authorized by the owner to make this application as his/her authorized agent, and I agree to conform to all applicable laws of the State of Michigan and ordinances of the City of Portage. All information submitted on the application is accurate to the best of my knowledge.

Signature of Applicant: \_\_\_\_\_

Date: 8-8-23

# Oakland Commons Planned Development

## 9581 Oakland Drive

### Project Narrative

July 21, 2023 (revised)

#### Project Objective

The subject property, addressed as 9581 Oakland Drive (Parcel #00029-425-G), is 12.57 acres and currently zoned I-1, Light Industry. A rezoning to PD, Planned Development is requested to accommodate a unique residential housing development concept consisting of a mixture of detached and attached residential dwellings. The objective of this project is to provide much needed, quality housing options for residents in the City of Portage in a new and creative development design. A further description of the project is provided below.

#### General Plan of Development

As depicted on the Tentative Plan, the Oakland Commons PD proposes a total of 58 residential dwelling units on the 12.57 acre property with a resulting development density of 4.6 units/acre. More specifically, the project includes 26 single family detached homes, three duplex dwellings (6 units), six 3-unit townhome buildings (18 units) and two 4-unit townhome buildings (8 units) accessed through a looped private roadway network from Oakland Drive, and developed and managed as an exclusive rental community. Individual homes and exterior grounds including the interior private roads will be professionally managed and maintained by the developer.

Homes within the Oakland Commons PD will be 2-story dwellings ranging between 1,440-1,640 square feet with 3-4 bedrooms, 2-3 bathrooms and 1-2 car attached and detached garages. A sample rendering package of these homes is attached with this application. Each home will be served by a private driveway with additional off-street parking (18 spaces) provided along the eastern portion of the site. Minimum building setbacks and separations are proposed as follows:

#### Single-Family Detached Homes

Front: 30-feet (from Oakland Drive right-of-way); 20-feet (from edge of private roads)

Rear: 30-feet (from outer perimeter of property)

Side: 12-feet (separation between homes)

#### Two-Family Dwellings (Duplexes)

Front: 30-feet (from Oakland Drive right-of-way); 25-feet (from edge of private roads)

Rear: 30-feet (from outer perimeter of property)

Side: 18-feet (separation between duplexes)

#### Townhomes (3-Unit and 4-Unit Dwellings)

Front: 30-feet (from Oakland Drive right-of-way); 25-feet (from edge of private roads)

Rear: 30-feet (from outer perimeter of property)

Side: 30-feet (separation between townhomes)

Individual homes will be leased at market rate with rents ranging between \$2,100-\$2,400/month. Contingent upon the city adopting a Payment In-Lieu of Taxes (PILOT) ordinance for a portion of this project, the developer would commit to designating a minimum of 20% of these homes (12 homes) as attainable “workforce housing” units with reduced rents based on income levels between 80-120% of the Area Median Income (AMI).

The Oakland Commons PD is proposed to be constructed in one development phase with construction activities planned to commence in the Spring of 2024.

### **Open Space/Natural Feature Preservation/Sidewalks & Walking Paths**

As shown on the Tentative Plan, a regulated wetland/pond is located along the northern portion of the property. The boundary of this wetland was professionally evaluated and delineated by Environmental Science & Planning, LLC in April 2023 and is depicted on the Tentative Plan. Careful consideration of this valuable natural feature was taken in the design/layout of the Oakland Commons PD and no impact to this wetland area is proposed with the project. Additionally, the natural wooded area surrounding the wetland/pond, as well as the mature tree line located along the Oakland Drive frontage, will be preserved to the greatest extent possible.

In lieu of sidewalks along both sides of the internal private street network, an alternative pedestrian circulation system consisting of a network of interconnected concrete sidewalks and asphalt walking trails is proposed and has been incorporated into the Oakland Commons PD design. Four-foot wide concrete sidewalks are proposed around the outer perimeter of the looped interior private roadway (north side of Road “A”, east side of Road “D” and south side of Road “B”). Additionally, a six-foot wide asphalt walking trail is proposed within the interior courtyard area of the site (running east-west from Road “D” to Oakland Drive), along the Oakland Drive frontage and around the south side of the natural wetland area located in the northern portion of the site. Collectively, this system of interconnected sidewalks and trails will be directly accessible from the front door of each home and will create a unique walkable pedestrian circulation network within the Oakland Commons PD. A concrete patio area with benches overlooking the natural wetland, pond and wooded area is also planned within the northeast portion of the site.

### **Screening/Buffering & Landscaping**

Since the property abuts the Weathervane Self Storage Facility to the east and Romence Gardens site to the south, screening will be installed along the eastern and southern property line to buffer homes from these adjacent land uses. Screening/buffering will consist of a mixture of deciduous and evergreen trees planted in an alternating double row along the eastern and southern property lines.

Additional street trees and supplemental landscaping will also be planted along the interior private roadway network, around the storm water management system and along the asphalt trail system within the middle courtyard area and northern portions of the site.

### **Access/Private Roads/Utilities**

Access to the Oakland Commons PD is proposed through a single private street access point from Oakland Drive, along the southwest portion of the parcel. This access will include an ingress lane and two egress lanes (right-out/left-out). A left-turn lane into the development for southbound Oakland Drive traffic will also be constructed. Individual homes within the residential community will be served by a looped interior private roadway network: No individual home will be directly accessed to/from Oakland Drive. The project will be served by municipal water, sanitary sewer and underground franchise utilities. Storm water from the project will be collected and conveyed to a storm water treatment cell and infiltration basin located in the northwest corner of the property.

### **Consistency with Comprehensive Plan & Appropriateness of Proposed PD Project**

The city is currently in the process of updating the 2014 Comprehensive Plan with completion of this Plan Update (Forward Together 2045 Master Plan) anticipated in the Spring/Summer of 2024. The Future Land Use Map contained in the 2014 Comprehensive Plan designates the subject parcel, along with adjacent properties to the east and north/south along Shaver Road, for "Shaver Road Business Corridor" land use. Adjacent properties to the west and north/south along Oakland Drive are designated for "Low Density Residential" land use. The existing zoning pattern closely mimics these Future Land Use Map designations. The "Shaver Road Business Corridor" classification envisioned a mixture of commercial and industrial land use along the Shaver Road frontage, while the "Low Density Residential" classification envisioned single family residential land use along the Oakland Drive frontage with a development density of up to 4 units/per acre.

When the 2014 Comprehensive Plan was adopted almost 10 years old, the subject parcel (9581 Oakland Drive) and the adjacent Weathervane Self Storage parcel to the east (9600 Shaver Road) were one large vacant parcel (approximately 20 acres) with frontage on both Shaver Road and Oakland Drive. In 2019, the Weathervane Self Storage facility was constructed on the eastern portion of this previously vacant parcel and a land division was executed splitting this larger parcel into two new parcels: 9600 Shaver Road (Weathervane Self Storage parcel with frontage/access from Shaver Road) and 9581 Oakland Drive (vacant parcel with frontage/access from Oakland Drive). Based on these development activities over the past few years, it is believed the current I-1, light industry zoning designation of 9581 Oakland Drive and the current "Shaver Road Business Corridor" Future Land Use Map classification is no longer appropriate for the subject property and should be re-evaluated with the Forward Together 2045 Master Plan update.

Encouraging industrial land use on the 9581 Oakland Drive parcel with access from Oakland Drive would not be incompatible with the established residential land use pattern along Oakland Drive. Additionally, Oakland Drive is a two-lane roadway that is classified as a "Residential Conservation Corridor" and not intended for more intensive industrial traffic. The proposed Oakland Commons PD project with a mixture of detached and attached residential dwellings is a more appropriate land use for this Oakland Drive parcel that will help address a growing need and demand for housing in the City of Portage. The proposed Oakland Commons PD is also consistent with Preliminary Land

Use Framework of “Mixed Residential” which is being considered with the Forward Together 2045 Master Plan. Finally, The Oakland Commons PD with a commitment to provide a minimum of 20% of these homes as attainable “workforce housing” units is consistent with Portage Attainable Housing Plan adopted by City Council on December 20, 2022 and will help address the need for 7,750 new residential dwelling units across Kalamazoo County by 2030, as detailed in the W.E. Upjohn Institute Kalamazoo County Housing Plan published in July 2022.

### **Conclusion**

The Oakland Commons PD has been designed with careful consideration of the natural features present on the property and proposes a unique and creative residential housing concept consisting of a mixture of detached single family homes, two family dwellings (duplexes) and townhomes (3-4 units buildings) in a professionally managed rental community. With an overall development density of 4.6 units/per acre and a mixture of housing types in a pedestrian friendly, walkable design, the Oakland Commons project is in conformance with the PD ordinance. The project is also consistent and compatible with the surrounding zoning and land use pattern and will bring much needed, quality and more attainable housing options for existing and future residents in the City of Portage.

We look forward to the opportunity to work with the City of Portage on this exciting project.

Sincerely,



Michael West, AICP  
Land Planning Manager  
Green Development Ventures, LLC  
2186 East Centre Avenue  
Portage, Michigan 49002

# TENTATIVE PLAN for Proposed "OAKLAND COMMONS"

N



NOTE: EXISTING MATURE TREES ALONG EAST SIDE OF OAKLAND DRIVE SHALL BE PRESERVED AS MUCH AS POSSIBLE

NOTE: DESIGN OF STORM SEWER, SYSTEM AND STORM WATER DISCHARGE SYSTEM IS PENDING. SCENARIOS, SIZE AND LOCATION OF STORM WATER TREATMENT CELL AND INFILTRATION BASIN IS SUBJECT TO FINAL DESIGN. DESIGN SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF PORTAGE'S STORM WATER DESIGN CRITERIA MANUAL. DESIGN WILL BE SHOWN ON PENDING FINAL PLAN.

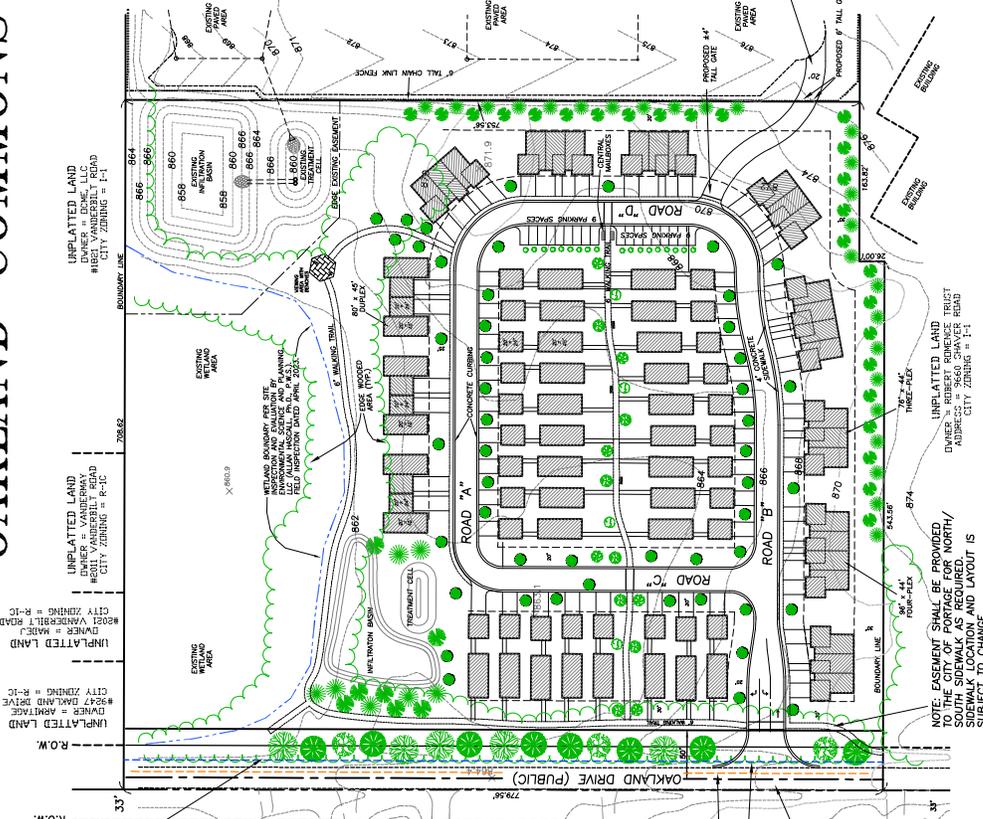
ADDRESS = 9581 OAKLAND DRIVE  
 PARCEL ID = 00029-425-G  
 CURRENT ZONING = I-1  
 PROPOSED ZONING = PD  
 SUBJECT PROPERTY = 12.57 ACRES  
 (AREA INCLUDES OAKLAND DRIVE RIGHT-OF-WAY AREA)

SITE DATA:  
 -PARCEL AREA = 12.57 ACRES  
 -# DWELLINGS = 58  
 -DENSITY = 4.6 DWELLINGS / ACRE

**BUILDING SEPARATION & SETBACKS**  
 SINGLE-FAMILY HOMES:  
 -30' SETBACK FROM BACK OF CURB  
 -MINIMUM 12' BETWEEN BUILDINGS  
 -MINIMUM 30' OFF OAKLAND DRIVE R.O.W.  
 DUPLEXES:  
 -25' SETBACK OFF BACK OF CURB  
 -MINIMUM 18' BETWEEN BUILDINGS  
 THREE-FLEXES AND FOUR-FLEXES:  
 -25' SETBACK OFF BACK OF CURB  
 -MINIMUM 30' REAR SETBACK  
 -MINIMUM 30' OFF OAKLAND R.O.W.

**FIRE DEPT. NOTES**  
 COMMERCIAL BUILDINGS SHALL BE CONSTRUCTED WITH FIRE RESISTANT AND OPERABLE AND  
 THE FIRE FLOW FOR EACH BUILDING IS 1000 GPM FROM  
 THE MAIN FIRE FLOW FROM THE MAIN FIRE FLOW FROM THE  
 4-4" FLOORING SHALL BE PROVIDED IN ALL BUILDINGS AND  
 THE SIZE AND TYPE OF CONSTRUCTION FOR THE BUILDING  
 SHALL BE PERMITTED ON ONE SIDE AS A FIRE CODE.  
 ONE SIDE OF THE STREET FOR THE FIRE CODE.

72 HOURS  
 BEFORE THE  
 1-800-487-7111



UNPLATTED LAND  
 OWNER = ROBERT BENEVE TRUST  
 ADDRESS = 3008 SHAKER ROAD  
 CITY ZONING = I-1

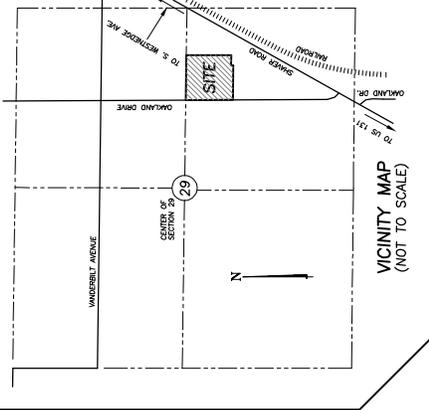
UNPLATTED LAND  
 OWNER = ROBERT BENEVE TRUST  
 ADDRESS = 3008 SHAKER ROAD  
 CITY ZONING = I-1

UNPLATTED LAND  
 OWNER = ROBERT BENEVE TRUST  
 ADDRESS = 3008 SHAKER ROAD  
 CITY ZONING = I-1

UNPLATTED LAND  
 OWNER = ROBERT BENEVE TRUST  
 ADDRESS = 3008 SHAKER ROAD  
 CITY ZONING = I-1

NOTE: EASEMENT SHALL BE PROVIDED TO THE CITY OF PORTAGE FOR NORTH/SOUTH SIDEWALK LOCATION AND LAYOUT IS SUBJECT TO CHANGE.

PARCEL DESCRIPTION PER CITY WEBSITE:  
 THE PARCEL IS 12.57 ACRES, 11.18 IN WIDTH AND 11.18 IN DEPTH. THE PARCEL IS BOUNDARY LINED BY OAKLAND DRIVE TO THE NORTH AND WEST, VAN DERBILT ROAD TO THE SOUTH AND EAST. THE PARCEL IS CURRENTLY ZONED I-1 (INDUSTRIAL) AND IS SUBJECT TO THE CITY OF PORTAGE ZONING ORDINANCE. THE PARCEL IS CURRENTLY UNPLATTED AND IS SUBJECT TO THE CITY OF PORTAGE PLATTING ACT. THE PARCEL IS CURRENTLY OWNED BY ROBERT BENEVE TRUST. THE PARCEL IS CURRENTLY UNPLATTED AND IS SUBJECT TO THE CITY OF PORTAGE PLATTING ACT. THE PARCEL IS CURRENTLY OWNED BY ROBERT BENEVE TRUST.



VICINITY MAP  
 (NOT TO SCALE)



AERIAL PHOTOGRAPH OF VICINITY  
 NO SCALE

- GENERAL NOTES:
- 1) PARCEL AREA = 12.57 ACRES.
  - 2) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 3) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 4) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 5) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 6) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 7) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 8) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 9) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 10) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 11) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.
  - 12) SETBACKS SHALL BE PROVIDED, SHOWN LANDSCAPING.

APPLICANT:  
**GREEN DEVELOPMENT VENTURES, LLC**  
 2186 EAST CENTRE STREET  
 PORTAGE, MI 49002  
 (269) 321-2610

DATE	5/16/2023
REVISION	08/04/2023
REVISION	06/29/2023
TENTATIVE PLAN	
PROPOSED "OAKLAND COMMONS"	
FORMERLY INTEGRATED, WALSON & MUEHLSTEIN, INC.	
1209 East Michigan Road, Suite B - Portage, Michigan 49002 - Phone 269 344-6065	
JOB No.	23-039
SHEET	1

NOTE: ON SITE WATER MAIN SYSTEM SHALL BE LOOPED. SYSTEM SHALL HAVE TWO CONNECTIONS TO EXISTING CITY WATER MAIN. WATER MAIN DESIGN IS PENDING.  
 NOTE: IN GENERAL, EXISTING IMPROVEMENTS HAVE BEEN SHOWN WITH A DASHED LINETYPE. PROPOSED IMPROVEMENTS HAVE BEEN SHOWN WITH A SOLID LINETYPE.  
 NOTE: SHOWN NUMBER OF DWELLINGS = 58.





(23)



(24)





(126)



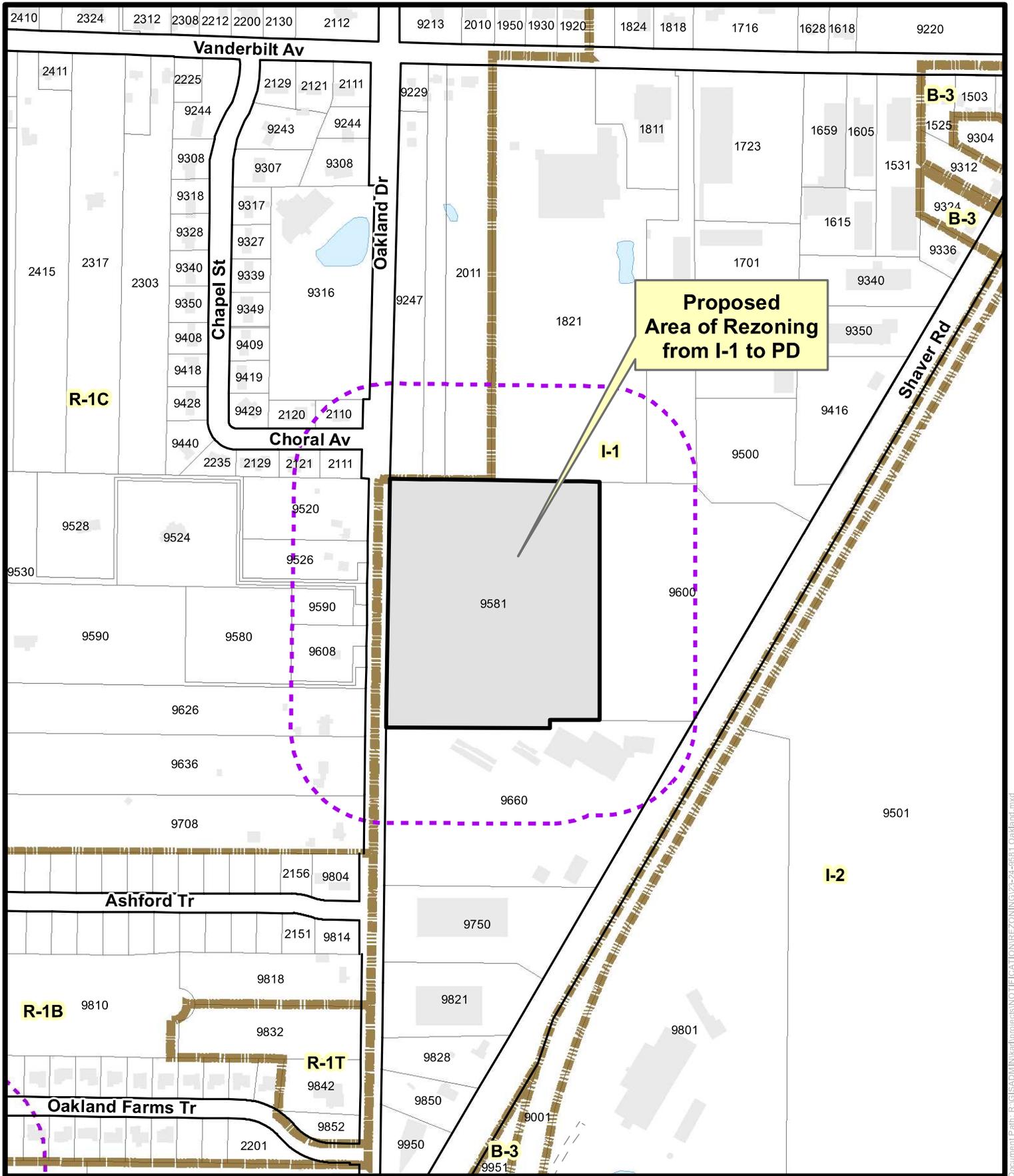
(27)



128



(29)



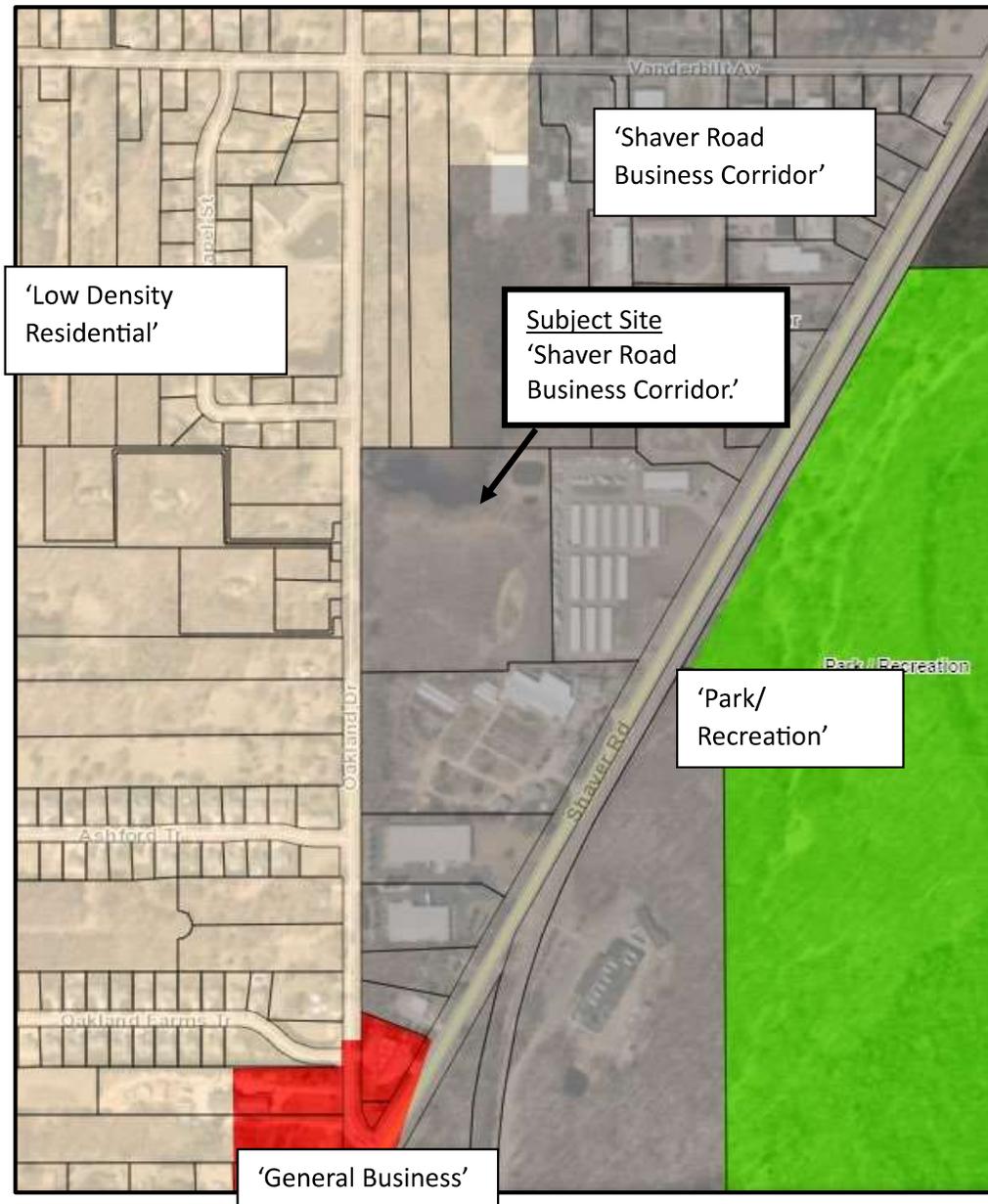
-  Zoning Boundary
-  300' Notification Area
-  Subject Parcel

## Notification Map Rezoning: 23/24 - 1 9581 Oakland Drive

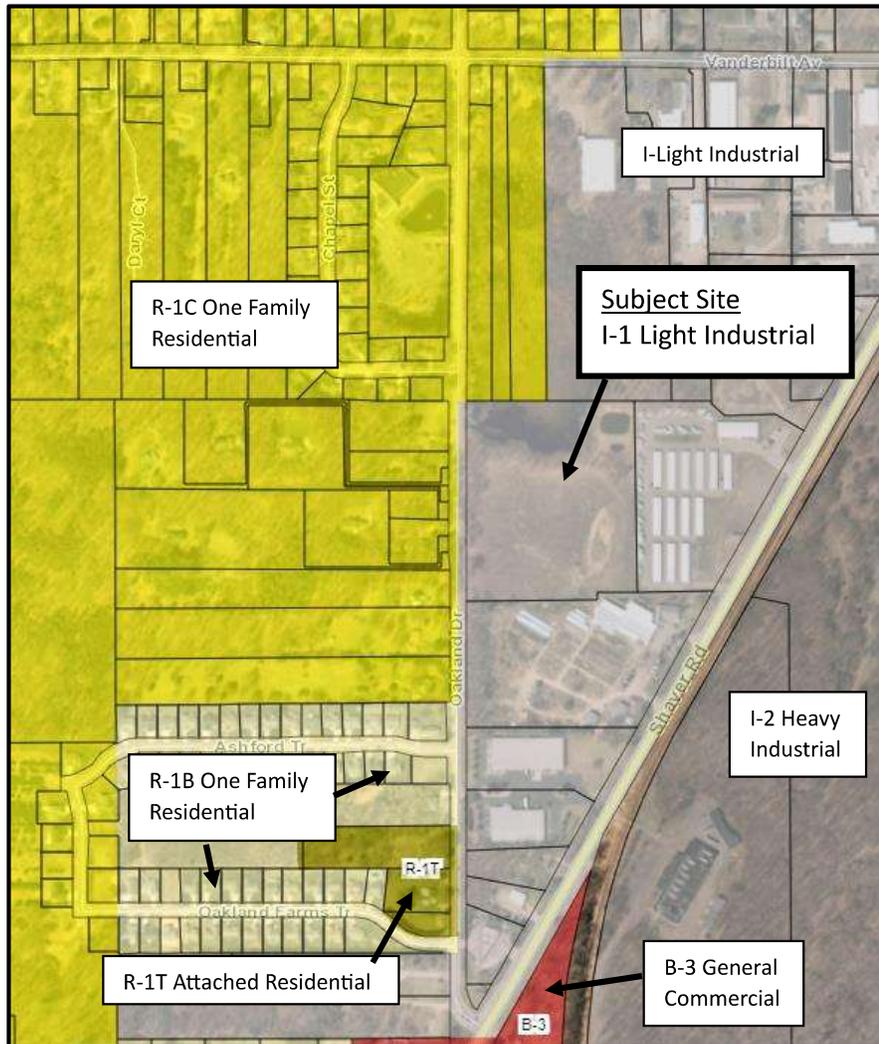


1 inch = 400 feet  
Date: 7/26/2023

City of Portage  
Comprehensive Plan  
Future Land Use Map



# City of Portage Existing Zoning Map



# Vicinity Map



# Staff's Site Pictures – August 9, 2023





Figure 1: Looking south along Oakland Drive. Subject site to the left (east) and driveways to multiple houses at 9590, 9530, 9528, 9526, 9524 Oakland Drive to the right (west).



Figure 2: Looking south along Oakland Drive. Subject site to the left (east) and drive to house at 9608 Oakland Drive to the right (west).



Figure 3: Looking north from within the site. Wetlands exist beyond tall tree line in background. Proposed development planned in low-growth area.



Figure 4: Looking east into the site from Oakland Drive. Clearing beyond trees in foreground is within subject site. Proposed development planned beyond treeline.



Figure 5: Looking east into the site from edge of wetlands. Site clearing created opening beyond trees in foreground. Structures in background are on Weathervane Self Storage and Romence Gardens along Shaver Road. Proposed development planned low-growth area.



Figure 6: Looking south into the site. Trees to the right are along Oakland Drive. Recent clearing show low-growing vegetation. Proposed development planned to the left of treeline.



Figure 7: Looking north into the site toward wetlands. Proposed development located south of wetlands.



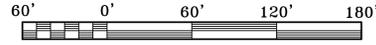
Figure 8: Looking west at 9520 and 9526 Oakland Drive (single family dwellings) located across from subject site.



Figure 9: Looking west through Weathervane Self Storage toward subject site in background. Romence Gardens is located to the left (south).

# FINAL PLAN for "OAKLAND COMMONS" in PORTAGE, MICHIGAN

N



ADDRESS = 9581 OAKLAND DRIVE  
 PARCEL ID = 00029-425-G  
 CURRENT ZONING = PD  
 SUBJECT PROPERTY = 12.57 ACRES  
 (AREA INCLUDES OAKLAND DRIVE RIGHT-OF-WAY AREA)

NOTE: REFER TO LANDSCAPE PLAN (BY OTHERS) FOR PROPOSED PLANTINGS ALONG OAKLAND DRIVE AND FOR ENTRYWAY LANDSCAPING.

NOTE: A PRELIMINARY 'STORM WATER BEST MANAGEMENT PRACTICES OPERATIONS AND MAINTENANCE AGREEMENT' SHALL BE PROVIDED TO THE CITY PRIOR TO THE CITY REQUIRED PRE-CONSTRUCTION MEETING. THE FINAL AGREEMENT SHALL BE PROVIDED TO THE CITY PRIOR TO COMPLETION OF THE PROJECT.

**SITE DATA:**

- PARCEL AREA = 12.57 ACRES
- # DWELLINGS = 58
- DENSITY = 4.6 DWELLINGS / ACRE

**BUILDING SEPARATION & SETBACKS**

- SINGLE FAMILY:**
- 20' SETBACK FROM BACK OF CURB
  - MINIMUM 12' BETWEEN BUILDINGS
  - MINIMUM 30' OFF OAKLAND DRIVE R.O.W.
- DUPLEXES:**
- 25' SETBACK OFF BACK OF CURB
  - MINIMUM 18' BETWEEN BUILDINGS
- THREE-PLEXES AND FOUR-PLEXES:**
- 25' SETBACK OFF BACK OF CURB
  - MINIMUM 30' BETWEEN BUILDINGS
  - MINIMUM 30' REAR SETBACK
  - MINIMUM 30' OFF OAKLAND R.O.W.

**FIRE DEPT. NOTES**

COMBUSTIBLE BUILDING CONSTRUCTION SHALL NOT COMMENCE UNTIL FIRE HYDRANTS ARE OPERABLE AND AN ALL WEATHER ROAD SURFACE IS IN PLACE.

THE FIRE FLOW FOR EACH BUILDING IS 1000 GPM FROM ONE HYDRANT WITHIN 400' OF ALL PORTIONS OF THE BUILDING. THIS FLOW IS BASED ON THE FLOOR AREA OF ALL FLOORS INCLUDING BASEMENT FOR A BUILDING OF TYPE V-B CONSTRUCTION. SHOULD A BUILDING EXCEED 3600 SQUARE FEET, THE MINIMUM FIRE FLOW WILL BE BASED ON THE SIZE AND TYPE OF CONSTRUCTION FOR THAT BUILDING.

FIRE APPARATUS ACCESS ROADS 26.1 TO 32 FEET WIDE SHALL BE POSTED ON ONE SIDE AS A FIRE LANE.

ALL STREETS WILL BE WIDE ENOUGH TO ALLOW PARKING ON ONE SIDE OF THE STREET PER THE FIRE CODE.

**PARCEL DESCRIPTION PER CITY WEBSITE:**

A PARCEL OF LAND SITUATED IN THE SE 1/4 OF SEC 29, 3S, 11W COMMENCING AT THE EAST 1/4 COR OF SEC 29, 3S, 11W; TH NORTH 89 DEG 50' 15" WEST (PREVIOUSLY RECORDED AS SOUTH 89 DEG 55' 07" WEST 331.47 FT ALG THE NORTH 1/4 OF THE SE 1/4 OF SEC 29 TO REFERENCE POINT "A"; TH SOUTH 90 DEG 50' 19" EAST (PREVIOUSLY RECORDED AS NORTH 90 DEG 55' 07" EAST) 231.47 FT ALG SD NORTH LI TO SD EAST 1/4 COR; TH NORTH 89 DEG 23' EAST (PREVIOUSLY RECORDED AS EAST) 116.85 FT ALG THE SOUTH LI OF THE NW 1/4 OF SEC 29, 3S, 11W TO THE NORTHWESTERLY ROW LI OF SHAVER RD; TH SOUTH 29 DEG 38' 07" WEST (PREVIOUSLY RECORDED AS SOUTH 29 DEG 38' WEST) 152.21 FT ALG SD ROW LI; TH NORTH 60 DEG 21' 53" WEST 145.41 FT; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS NORTH 89 DEG 55' 07" EAST) 198.90 FT PARALLEL; W SD SOUTH LI; TH NORTH 44 DEG 50' 19" WEST 67.78 FT TO A LI EXTENDING SOUTH 00 DEG 05' 30" EAST FROM SD REFERENCE POINT "A"; TH NORTH 00 DEG 05' 30" WEST 12.78 FT TO SD REFERENCE POINT "A"; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS SOUTH 89 DEG 55' 07" WEST) 278.54 FT ALG SD NORTH LI TO THE POB; TH SOUTH 00 DEG 09' 41" WEST 253.56 FT; TH NORTH 89 DEG 50' 19" WEST 163.82 FT (PREVIOUSLY RECORDED AS NORTH 89 DEG 55' 07" EAST) PARALLEL; W SD NORTH LI; TH SOUTH 00 DEG 09' 41" WEST 26 FT; TH NORTH 89 DEG 50' 19" WEST (PREVIOUSLY RECORDED AS SOUTH 89 DEG 55' 07" WEST) 543.56 FT PARALLEL; W SD NORTH LI TO THE WEST LI OF THE EAST 1/4 OF SD SE 1/4 AS ESTABLISHED BY "OAKLAND FARMS NORTH" SUBDIVISION; TH NORTH 00 DEG 04' 14" EAST (PREVIOUSLY RECORDED AS NORTH) 779.58 FT ALG SD WEST LI OF THE EAST 1/4 TO SD NORTH LI OF THE SE 1/4; TH SOUTH 89 DEG 50' 19" EAST (PREVIOUSLY RECORDED AS NORTH 89 DEG 55' 07" EAST) 708.61 FT ALG SD NORTH LI OF THE SE 1/4 TO THE POB. CONTAINING 12.57 ACRES OF LAND. THE WESTERLY PORTION BEING SUBJECT TO EASEMENT FOR OAKLAND DR. SPLIT/COMBINED ON 08/20/2018 FROM 00029-425-E, 00029-425-G.

**GENERAL NOTES:**

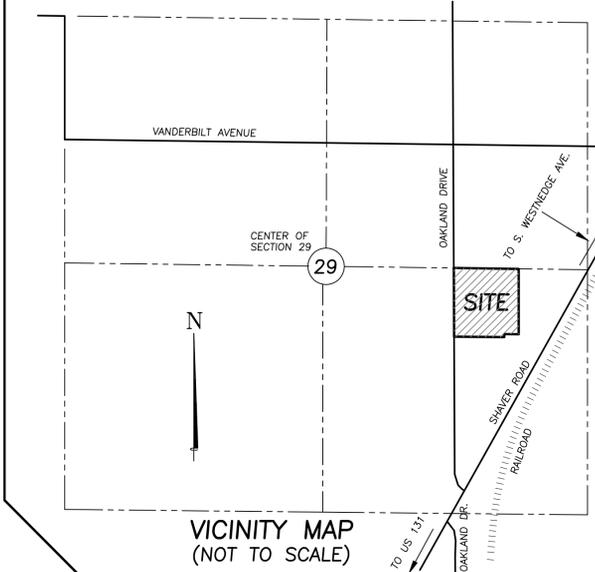
- 1) CURRENT CITY ZONING = PD.
- 2) PARCEL AREA = 12.57 ACRES.
- 3) SITE IS PARTIALLY COVERED WITH TREES AND BRUSH.
- 4) SITE LANDSCAPING SHALL BE PROVIDED. ANY SHOWN LANDSCAPING IS PRELIMINARY.
- 5) BUILDING PLANS (BY OTHERS) ARE PENDING.
- 6) ON-SITE FIRE HYDRANTS SHALL BE PROVIDED AS REQUIRED.
- 7) ALL ON-SITE WATER MAIN & SANITARY SEWER SHALL BE PUBLIC AND SHALL BE WITHIN AN EASEMENT TO THE CITY OF PORTAGE.
- 8) BUILDINGS SHALL CONNECT TO CITY WATER SYSTEM AND CITY SANITARY SEWER SYSTEM (EXCEPT GARAGES).
- 9) ALL NEW ROADWAYS SHALL BE PRIVATE.
- 10) EXISTING WATER MAINS AND SANITARY SEWERS ARE LOCATED WEST OF THE SUBJECT PROPERTY IN OAKLAND DRIVE.
- 11) STORM SEWER SYSTEM AND STORM WATER DISPOSAL SYSTEM SHALL BE PRIVATE.

NOTE: UNOBSTRUCTED ACCESS TO FIRE HYDRANTS SHALL BE MAINTAINED AT ALL TIMES. THE FIRE DEPARTMENT SHALL NOT BE PREVENTED FROM GAINING IMMEDIATE ACCESS TO FIRE HYDRANTS OR FIRE PROTECTION EQUIPMENT. THIS PROVISION INCLUDES SNOW REMOVAL TO ALLOW FOR PROPER ACCESS TO FIRE HYDRANTS.

NOTE: SHOWN NUMBER OF DWELLINGS = 58. ALL ON SITE IMPROVEMENTS AND ALL DWELLINGS SHALL BE UNDER SINGLE OWNERSHIP.



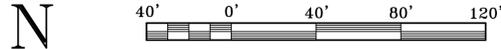
NOTE: ANY NEW STREET TREES SHALL NOT BE PLANTED IN FUTURE DRIVEWAY LOCATIONS AND SHALL NOT BE PLANTED ABOVE SEWER LINES AND WATER LINES. ALSO, STREET TREES SHALL BE PLANTED TO MAXIMIZE DISTANCE FROM UTILITY MAINS (SANITARY SEWER, STORM SEWER, AND WATER MAIN).



- SHEET INDEX:**
- 1) COVER SHEET
  - 2) LAYOUT PLAN
  - 3) GRADING & STORM SEWER PLAN
  - 4) OAKLAND DRIVE DETAIL
  - 5) PLAN & PROFILE SHEET (SOUTH PORTION)
  - 6) PLAN & PROFILE SHEET (NORTH PORTION)
  - 7) CONSTRUCTION DETAILS & NOTES
  - 8) MDT EROSION CONTROL DETAILS
  - 9) TRAFFIC CONTROL PLAN
  - 10) STORM WATER TREATMENT DEVICE
  - 11) WATER MAIN (ADJOINING PROPERTY)
- NOTE: LANDSCAPING PLAN SHALL BE PREPARED BY OTHERS.

**APPLICANT:**  
**GREEN DEVELOPMENT VENTURES, LLC**  
 2186 EAST CENTRE STREET  
 PORTAGE, MI 49002  
 (269) 321-2610

COVER SHEET		DATE:	11/28/2023
"OAKLAND COMMONS"		SHEET:	1
PART OF THE SE 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE		JOB No.:	23-039
MONUMENT ENGINEERING GROUP ASSOC., INC. Formerly Ingersoll, Watson & McMachen, Inc.		REVISIONS: 5/07/2024 4/25/2024 4/09/2024 2/28/2024	
1209 East Milham Road, Suite B • Portage, Michigan 49002 • Phone 269 344-6165			



NOTE: A S.E.S.C. PERMIT SHALL BE OBTAINED FROM THE CITY PRIOR TO COMMENCING CONSTRUCTION.

**BUILDING SEPARATION & SETBACKS**

SINGLE FAMILY:  
 -20' SETBACK FROM BACK OF CURB  
 -MINIMUM 12' BETWEEN BUILDINGS  
 -MINIMUM 30' OFF OAKLAND DRIVE R.O.W.

DUPLEXES:  
 -25' SETBACK OFF BACK OF CURB  
 -MINIMUM 18' BETWEEN BUILDINGS

THREE-PLEXES AND FOUR-PLEXES:  
 -25' SETBACK OFF BACK OF CURB  
 -MINIMUM 30' BETWEEN BUILDINGS  
 -MINIMUM 30' REAR SETBACK  
 -MINIMUM 30' OFF OAKLAND R.O.W.

SEE CROSS-SECTIONS FOR OAKLAND DRIVE ON SHEET 4

NOTE: FOR PROPOSED OAKLAND DRIVE CENTER TURN LANE, ALL PROPOSED LANES ARE 12.0' WIDE AND ALL PROPOSED PAVED SHOULDERS ARE 4.0' WIDE (TO MATCH EXISTING LANE WIDTHS AND SHOULDER WIDTHS). PROPOSED GRAVEL SHOULDERS SHALL BE 3.0' WIDE.

PROPOSED PAVEMENT MILLING / RESURFACING = 1.5" THICKNESS.

**FIRE DEPT. NOTES**

COMBUSTIBLE BUILDING CONSTRUCTION SHALL NOT COMMENCE UNTIL FIRE HYDRANTS ARE OPERABLE AND AN ALL WEATHER ROAD SURFACE IS IN PLACE.

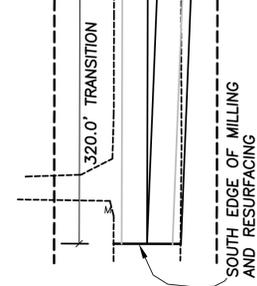
THE FIRE FLOW FOR EACH BUILDING IS 1000 GPM FROM ONE HYDRANT WITHIN 400' OF ALL PORTIONS OF THE BUILDING. THIS FLOW IS BASED ON THE FLOOR AREA OF ALL FLOORS INCLUDING BASEMENT FOR A BUILDING OF TYPE V-B CONSTRUCTION. SHOULD A BUILDING EXCEED 3600 SQUARE FEET, THE MINIMUM FIRE FLOW WILL BE BASED ON THE SIZE AND TYPE OF CONSTRUCTION FOR THAT BUILDING.

FIRE APPARATUS ACCESS ROADS 26.1 TO 32 FEET WIDE SHALL BE POSTED ON ONE SIDE AS A FIRE LANE.

ALL STREETS WILL BE WIDE ENOUGH TO ALLOW PARKING ON ONE SIDE OF THE STREET PER THE FIRE CODE.

LANE LINE STRIPING for OAKLAND DRIVE:  
 ALL NEW LANE LINES SHALL BE SINGLE 6" STRIPES (WHITE) EXCEPT LANES LINES BETWEEN OPPOSING TRAFFIC DIRECTIONS SHALL BE DOUBLE 6" STRIPES (YELLOW).

MATCH LINE (SEE BELOW RIGHT)



PROPOSED ENTRY / EXIT.  
 ENTRY LANE = 14.0' WIDE  
 EXIT LANE = 11.0' WIDE  
 TOTAL PAVEMENT WIDTH = 36.0'

**LEGEND**

- EXISTING PUBLIC SANITARY SEWER
- PROPOSED 8" PUBLIC SANITARY SEWER OR PRIVATE 6" SEWER LEAD
- EXISTING PRIVATE STORM SEWER
- PROPOSED PRIVATE STORM SEWER
- EXISTING PUBLIC WATER MAIN
- PROPOSED 8" PUBLIC WATER MAIN OR PRIVATE 1.25" COPPER WATER SERVICE
- PROPOSED GAS, ELECTRIC, TV, AND PHONE
- EDGE OF PROPOSED WATER MAIN & SANITARY SEWER EASEMENT TO THE CITY OF PORTAGE
- M EXISTING MAILBOX
- P EXISTING UTILITY PEDESTAL
- A EXISTING ADDRESS ON POST
- DW EXISTING DRYWELL

EXISTING WETLAND AREA SHALL NOT BE DISTURBED

NORTH EDGE OF MILLING AND RESURFACING

REGULATED WETLAND AREA

REGULATED WETLAND AREA

WETLAND BOUNDARY PER SITE INSPECTION AND EVALUATION BY ENVIRONMENTAL SCIENCE AND PLANNING, LLC (ALLAN HASSALL, PH.D., P.W.S.), FIELD INSPECTION DATED APRIL 2023.

EXISTING STORM WATER INFILTRATION BASIN FOR WEATHERVANE SELF STORAGE (NOT SHOWN)

EXISTING STORM WATER TREATMENT CELL FOR WEATHERVANE SELF STORAGE (NOT SHOWN)

EDGE EXISTING EASEMENT

STORM WATER DISPOSAL AREA (SEE SHEET 3)

PROPOSED CONCRETE PARKING AREA, TWO SPACES (9.0' x 20' EACH), 6" CONCRETE.

PROPOSED DUPLEX (TYP.) (TWO STORIES) ±2760 SQ.FT. FOOTPRINT

PROPOSED 6.0' WIDE PAVED WALKING TRAIL (TYP.)

PROPOSED 5.0' WIDE CONCRETE SIDEWALK (TYP.)

KINGFISHER CIRCLE

C.L. RADIUS = 60.0'  
L = 94.25'

C.L. RADIUS = 60.0'  
L = 94.25'

PROPOSED 3-PLEX (TYP.) (TWO-STORIES) 900 SQ.FT. FOOTPRINT

C.L. RADIUS = 300.0'  
L = 79.61'

C.L. RADIUS = 300.0'  
L = 79.61'

KINGFISHER CIRCLE

BOUNDARY LINE

PROPOSED R.O.W. LINE (±50.00' OFF PHYSICAL CENTER LINE)

PROPOSED 4-PLEX (TYP.) BUILDING AREA = ±3728 SQ.FT. FOOTPRINT (TWO STORIES)

PROPOSED 3-PLEX (TYP.) BUILDING AREA = ±2876 SQ.FT. FOOTPRINT (TWO STORIES)

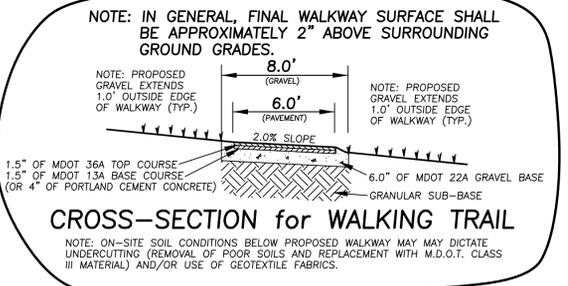
UNPLATTED LAND  
 OWNER = ROBERT ROMENCE TRUST  
 ADDRESS = 9660 SHAVER ROAD  
 CITY ZONING = I-1

NOTE: IN GENERAL, EXISTING IMPROVEMENTS HAVE BEEN SHOWN WITH A DASHED LINETYPE. PROPOSED IMPROVEMENTS HAVE BEEN SHOWN WITH A SOLID LINETYPE.

NOTE: SHOWN NUMBER OF DWELLINGS = 58.

**CONSTRUCTION NOTES:**

- 1) "NO PARKING THIS SIDE OF STREET" SIGNS SHALL BE INSTALLED ATOP THE FIRE HYDRANT SIGNS AND MIDWAY BETWEEN THE FIRE HYDRANTS ON THE HYDRANT SIDE OF THE STREETS.
- 2) STREET NAME SIGNS SHALL BE AT LEAST 9" FROM TOP TO BOTTOM OF ACTUAL SIGN (& PLACED ATOP STOP SIGNS).
- 3) ALL PAVEMENT MARKINGS AND ALL TRAFFIC SIGNAGE SHALL COMPLY WITH STANDARDS IN THE M.M.U.T.O. (CURRENT EDITION)
- 4) U-CHANNEL POSTS REQUIRED FOR ALL SIGNS.
- 5) SOIL EROSION & SEDIMENTATION CONTROL PERMIT SHALL BE OBTAINED AS REQUIRED BY STATE LAW.
- 6) PERMIT(S) SHALL BE OBTAINED FROM THE CITY OF PORTAGE AS REQUIRED.
- 7) E.G.L.E. CONSTRUCTION PERMITS REQUIRED (FOR BOTH PUBLIC WATER MAIN & PUBLIC SANITARY SEWER) PRIOR TO THE COMMENCEMENT OF INSTALLATION.



**SITE DATA:**

- PARCEL AREA = 12.57 ACRES
- # DWELLINGS = 58
- DENSITY = 4.6 DWELLINGS / ACRE

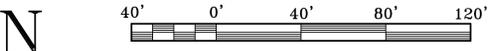
**GENERAL NOTES:**

- 1) CURRENT CITY ZONING = PD.
- 2) PARCEL AREA = 12.57 ACRES.
- 3) SITE IS PARTIALLY COVERED WITH TREES AND BRUSH.
- 4) SITE LANDSCAPING SHALL BE PROVIDED. ANY SHOWN LANDSCAPING IS PRELIMINARY.
- 5) BUILDING PLANS (BY OTHERS) ARE PENDING.
- 6) ON-SITE FIRE HYDRANTS SHALL BE PROVIDED AS REQUIRED.
- 7) ALL ON-SITE WATER MAIN & SANITARY SEWER SHALL BE PUBLIC AND SHALL BE WITHIN AN EASEMENT TO THE CITY OF PORTAGE.
- 8) BUILDINGS SHALL CONNECT TO CITY WATER SYSTEM AND CITY SANITARY SEWER SYSTEM (EXCEPT GARAGES).
- 9) ALL NEW ROADWAYS SHALL BE PRIVATE.
- 10) EXISTING WATER MAINS AND SANITARY SEWERS ARE LOCATED WEST OF THE SUBJECT PROPERTY IN OAKLAND DRIVE.
- 11) STORM SEWER SYSTEM AND STORM WATER DISPOSAL SYSTEM SHALL BE PRIVATE.
- 12) ALL NEW ROADWAYS SHALL BE PRIVATE.

NOTE: FIRE HYDRANT LOCATIONS SHOWN WITHIN THESE PLANS ARE SUBJECT TO FINAL ADJUSTMENT BASED ON FIRE DEPARTMENT & CITY ENGINEER REQUIREMENTS.

ADDRESS = 9581 OAKLAND DRIVE  
 PARCEL ID = 00029-425-G  
 CURRENT ZONING = PD  
 SUBJECT PROPERTY = 12.57 ACRES  
 (AREA INCLUDES OAKLAND DRIVE RIGHT-OF-WAY AREA)

LAYOUT PLAN <b>OAKLAND COMMONS</b>		DATE: 11/28/2023
PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN		SHEET: 2
<b>MONUMENT ENGINEERING GROUP ASSOC., INC.</b> Formerly Ingersoll, Watson & McMachen, Inc.		JOB No.: 23-039
1209 East Milham Road, Suite B • Portage, Michigan 49002 • Phone 269 344-6165		REVISIONS: 4/25/2024 4/08/2024 3/28/2024 2/28/2024



**NOTE: A S.E.S.C. PERMIT SHALL BE OBTAINED FROM THE CITY PRIOR TO COMMENCING CONSTRUCTION.**

NOTE: A S.E.S.C. PERMIT SHALL BE OBTAINED AS REQUIRED. THE PERMIT SHALL BE COORDINATED WITH THE CITY OF PORTAGE DEVELOPMENT REVIEW ADMINISTRATOR PRIOR TO COMMENCING CONSTRUCTION. THE S.E.S.C. PERMIT SHALL BE APPLIED FOR AFTER OBTAINING SITE PLAN APPROVAL.

NOTE: TEMPORARY SEEDING OF DISTURBED AREAS MAY BE REQUIRED TO REDUCE SEDIMENTATION DUE TO LOCATION OF THIS PROJECT AND THE NEARBY WETLAND AREA.

**NOTE: FOR THE FIRE HYDRANT EXTENSIONS ALONG OAKLAND DRIVE, IF A FLANGED AUXILIARY VALVE IS CONNECTED TO THE HYDRANT, OR THE JOINTS ARE NOT RESTRAINED, LINE STOP(S) SHALL BE REQUIRED TO COMPLETE THE HYDRANT EXTENSIONS AND FLANGED COMPONENTS SHALL BE REPLACED WITH MECHANICAL JOINTS.**

**NOTE: FIRE HYDRANT LOCATIONS SHOWN WITHIN THESE PLANS ARE SUBJECT TO FINAL ADJUSTMENT BASED ON FIRE DEPARTMENT & CITY ENGINEER REQUIREMENTS.**

**NOTE: CERTAIN BUILDING FOOTINGS AND/OR CONCRETE FLOORS MAY SIT ON ENGINEERED FILL MATERIAL. FOR THESE SITES, PLACEMENT OF FILL, INSPECTION, AND/OR TESTING SHALL BE DONE IN ACCORDANCE WITH CITY OF PORTAGE REQUIREMENTS.**

**STORM SEWER DATA (SOUTH & WEST SIDES)**

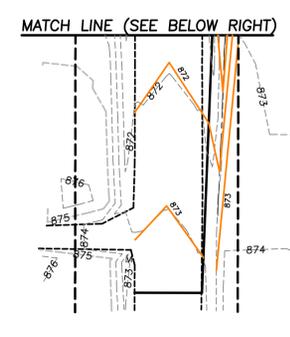
CB#36 24" DIAMETER RIM = 868.50 12" INV = 865.50	CB#35 TO CB#35: 142.0' - 12" STORM @ 1.20%
CB#35 24" DIAMETER RIM = 866.80 12" INV'S = 863.80	
CB#35 TO CB#34: 122.0' - 12" STORM @ 0.66%	
CB#34 48" DIAMETER RIM = 866.0 12" INV'S = 863.00	
CB#33 24" DIAMETER RIM = 866.80 12" INV = 863.80	
CB#33 TO CB#34: 122.0' - 12" STORM @ 0.66%	
CB#34 TO CB#8: 93.0' - 12" STORM @ 0.50%	
12" INV @ CB#8 = 862.54	
CB#32 24" DIAMETER RIM = 865.5 12" INV = 861.50	
CB#32 TO CB#31: 82.0' - 12" STORM @ 0.68%	
CB#31 24" DIAMETER RIM = 865.00 12" INV = 860.94	
CB#31 TO CB#30: 160.0' - 12" STORM @ 0.40%	
CB#30 24" DIAMETER RIM = 865.00 12" INV'S = 860.30	
CB#30 TO OUTLET @ BASIN: 77' - 12" STORM @ 0.40%	
12" INV @ BASIN = 860.00	

**OAKLAND DRIVE (PUBLIC R.O.W.)**

**NOTE: EXISTING MATURE TREES ON EAST SIDE OF OAKLAND DRIVE SHALL BE PRESERVED AS MUCH AS POSSIBLE.**

**NOTE: EXISTING MATURE TREES ON WEST SIDE OF OAKLAND DRIVE SHALL BE PRESERVED AND NOT DISTURBED.**

**ALSO SEE OAKLAND DRIVE DETAIL ON SHEET 4.**



72 HOURS BEFORE YOU DIG CALL MISS DIG 1-800-482-7171

**LEGEND**

- EXISTING PUBLIC SANITARY SEWER
- PROPOSED 8" PUBLIC SANITARY SEWER OR PRIVATE 6" SEWER LEAD
- EXISTING PRIVATE STORM SEWER
- PROPOSED PRIVATE STORM SEWER
- EXISTING PUBLIC WATER MAIN
- PROPOSED 8" PUBLIC WATER MAIN OR PRIVATE 1.25" COPPER WATER SERVICE
- PROPOSED GAS, ELECTRIC, TV, AND PHONE
- EDGE OF PROPOSED WATER MAIN & SANITARY SEWER EASEMENT TO THE CITY OF PORTAGE
- M EXISTING MAILBOX
- P EXISTING UTILITY PEDESTAL
- A EXISTING ADDRESS ON POST
- DW EXISTING DRYWELL

**STORM WATER TREATMENT DEVICE:**  
KS#1 = KENNEDY SOLUTIONS 3000 CHAMBER (298" LENGTH)  
(SEE DETAIL ON SHEET 10)

WETLAND BOUNDARY PER SITE INSPECTION AND EVALUATION BY ENVIRONMENTAL SCIENCE AND PLANNING, LLC (ALLAN HASCALL, Ph.D., P.W.S.), FIELD INSPECTION DATED APRIL 2023.

PROPOSED STORM WATER INFILTRATION BASIN.  
PROPOSED WET BOTTOM = 857'. REQ'D STORAGE VOLUME = 35,500 CF. PROVIDED STORAGE VOLUME (860 TO 864.5) = ±37,680 CF. OVERFLOW SPILLWAY @ ELEV 864.5. TYPICAL WATER LEVEL = ±860.

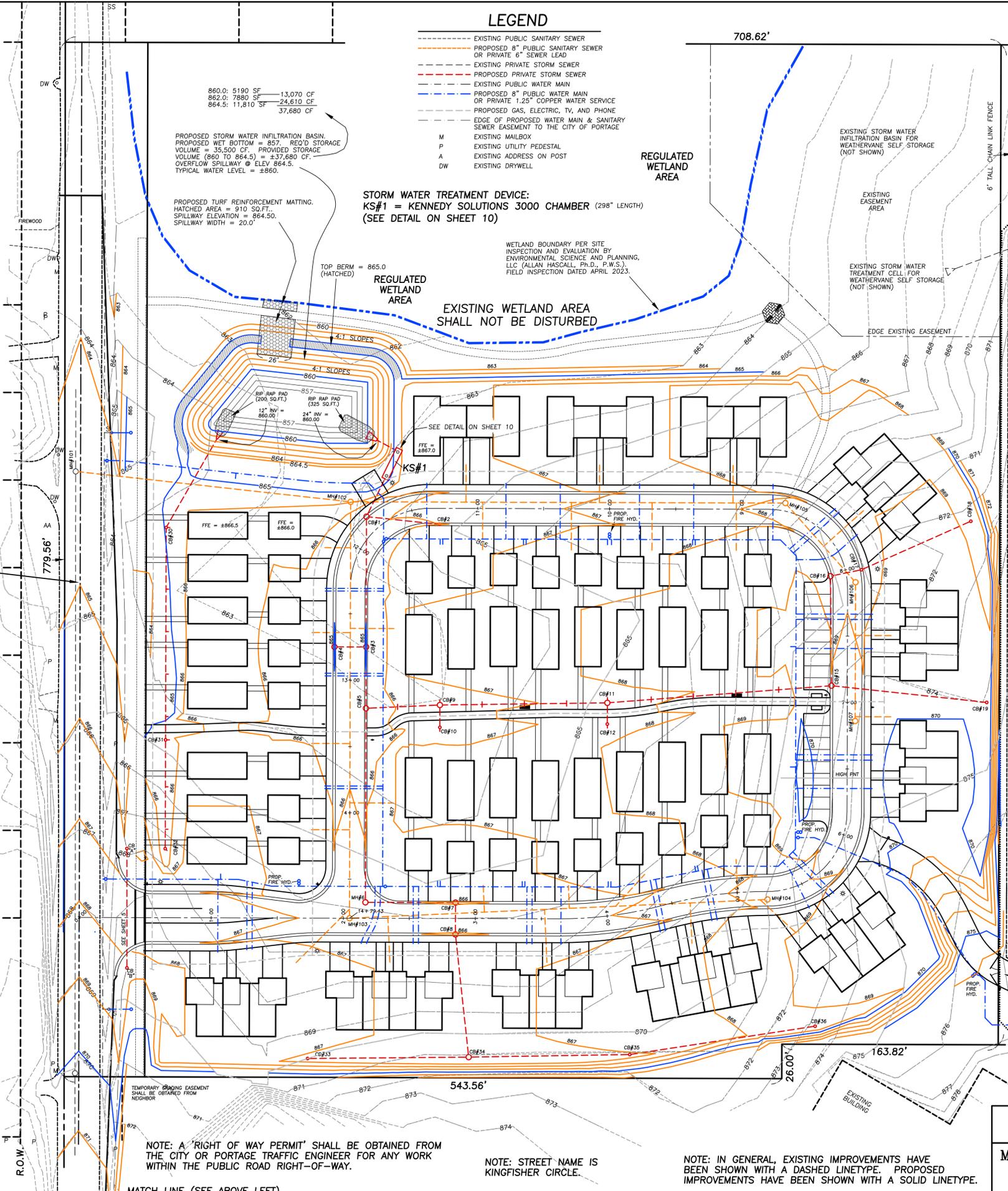
PROPOSED TURF REINFORCEMENT MATING.  
HATCHED AREA = 910 SQ.FT. SPILLWAY ELEVATION = 864.50. SPILLWAY WIDTH = 20.0'

REGULATED WETLAND AREA

EXISTING WETLAND AREA SHALL NOT BE DISTURBED

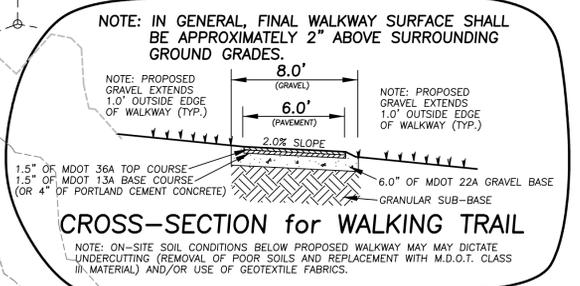
REGULATED WETLAND AREA

708.62'



**CONSTRUCTION NOTES:**

- NO PARKING THIS SIDE OF STREET SIGNS SHALL BE INSTALLED ATOP THE FIRE HYDRANT SIGNS AND MIDWAY BETWEEN THE FIRE HYDRANTS ON THE HYDRANT SIDE OF THE STREETS.
- STREET NAME SIGNS SHALL BE AT LEAST 9" FROM TOP TO BOTTOM OF ACTUAL SIGN (& PLACED ATOP STOP SIGNS).
- ALL PAVEMENT MARKINGS AND ALL TRAFFIC SIGNAGE SHALL COMPLY WITH STANDARDS IN THE M.M.U.T.C.D. (CURRENT EDITION)
- U-CHEMEL POSTS REQUIRED FOR ALL SIGNS.
- SOIL EROSION & SEDIMENTATION CONTROL PERMIT SHALL BE OBTAINED AS REQUIRED BY STATE LAW.
- PERMITS SHALL BE OBTAINED FROM THE CITY OF PORTAGE AS REQUIRED.
- E.G.L.E. CONSTRUCTION PERMITS REQUIRED (FOR BOTH PUBLIC WATER MAIN & PUBLIC SANITARY SEWER) PRIOR TO THE COMMENCEMENT OF INSTALLATION.



**GENERAL NOTES:**

- CURRENT CITY ZONING = PD.
- PARCEL AREA = 12.57 ACRES.
- SITE IS PARTIALLY COVERED WITH TREES AND BRUSH.
- SITE LANDSCAPING SHALL BE PROVIDED. ANY SHOWN LANDSCAPING IS PRELIMINARY.
- BUILDING PLANS (BY OTHERS) ARE PENDING.
- ON-SITE FIRE HYDRANTS SHALL BE PROVIDED AS REQUIRED.
- ALL ON-SITE WATER MAIN & SANITARY SEWER SHALL BE PUBLIC AND SHALL BE WITHIN AN EASEMENT TO THE CITY OF PORTAGE.
- BUILDINGS SHALL CONNECT TO CITY WATER MAIN AND CITY SANITARY SEWER SYSTEM (EXCEPT GARAGES).
- ALL NEW ROADWAYS SHALL BE PRIVATE.
- EXISTING WATER MAINS AND SANITARY SEWERS ARE LOCATED WEST OF THE SUBJECT PROPERTY IN OAKLAND DRIVE.
- STORM SEWER SYSTEM AND STORM WATER DISPOSAL SYSTEM SHALL BE PRIVATE.

**STORM SEWER DATA (EAST & NORTH SIDES)**

CB#19 24" DIAMETER RIM = 869.30 12" INV = 865.07	CB#19 TO CB#15: 118.2' - 12" STORM @ 0.30% (RCP)
CB#18 24" DIAMETER RIM = 868.60 12" INV = 865.26	
CB#18 TO CB#17: 90.5' - 12" STORM @ 0.25% (RCP)	
CB#17 24" DIAMETER GUTTER = 868.40 12" INV'S = 865.03	
CB#17 TO CB#16: 24.0' - 12" STORM @ 0.25% (RCP)	
CB#16 48" DIAMETER GUTTER = 868.40 12" INV'S = 864.97	
CB#16 TO CB#15: 82.5' - 12" STORM @ 0.30% (RCP)	
CB#15 48" DIAMETER GUTTER = 869.04 12" INV (N AND E) = 864.72 12" INV (W) = 864.72	
CB#15 TO CB#11: 170.2' - 12" STORM @ 1.00%	CB#12 TO CB#11: 16.0' - 12" STORM @ 1.00%
CB#11 48" DIAMETER RIM = 867.30 12" INV'S = 863.02	CB#12 24" DIAMETER RIM = 867.30 12" INV = 863.18
CB#11 TO CB#9: 126.8' - 12" STORM @ 0.57%	CB#10 TO CB#9: 16.8' - 12" STORM @ 1.00%
CB#9 48" DIAMETER RIM = 866.40 12" INV'S = 862.30	CB#10 24" DIAMETER RIM = 866.40 12" INV = 862.47
CB#9 TO CB#5: 56.0' - 12" STORM @ 0.91%	
CB#5 48" DIAMETER GUTTER = 865.18 12" INV (E) = 861.79	

**ALSO SEE SHEETS 5 & 6 FOR ADDITIONAL STORM SEWER INFORMATION**

**NOTE: CERTAIN PIPE RUNS MAY NEED TO BE REINFORCED CONCRETE PIPE.**

**NOTE: CB#1 TO KS#1:  
34' - 24" STORM @ 0.30%  
24" INV @ KS#1 = 860.40**

**GRADING & STORM SEWER PLAN**  
**OAKLAND COMMONS**  
PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN

**MONUMENT ENGINEERING GROUP ASSOC., INC.**  
Formerly Ingersoll, Watson & McMachen, Inc.  
1209 East Milham Road, Suite B • Portage, Michigan 49002 • Phone 269 344-6165

DATE: 11/28/2023  
SHEET: 3  
JOB No.: 23-039

REVISED: 4/25/2024  
REVISED: 4/08/2024  
REVISED: 2/28/2024

**NOTE: A 'RIGHT OF WAY PERMIT' SHALL BE OBTAINED FROM THE CITY OF PORTAGE TRAFFIC ENGINEER FOR ANY WORK WITHIN THE PUBLIC ROAD RIGHT-OF-WAY.**

**NOTE: STREET NAME IS KINGFISHER CIRCLE.**

**NOTE: IN GENERAL, EXISTING IMPROVEMENTS HAVE BEEN SHOWN WITH A DASHED LINETYPE. PROPOSED IMPROVEMENTS HAVE BEEN SHOWN WITH A SOLID LINETYPE.**

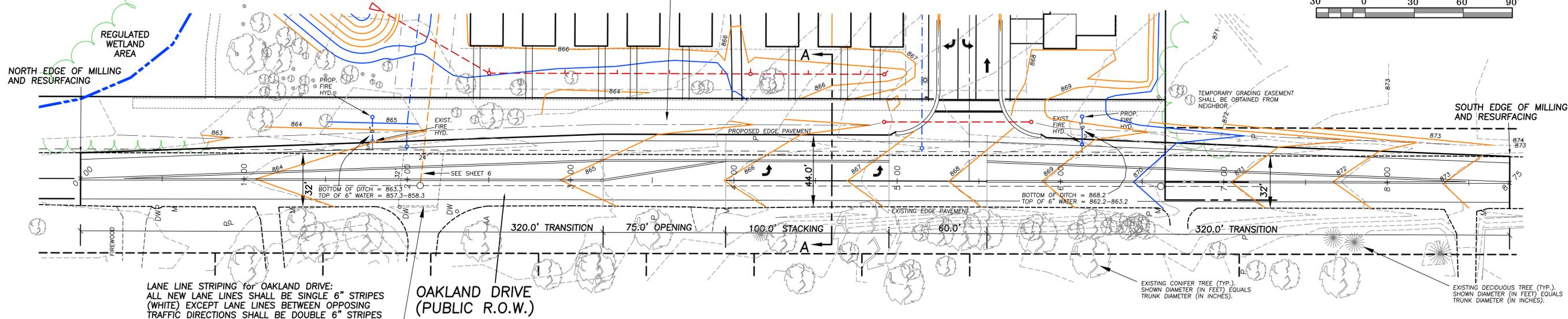
NOTE: FOR PROPOSED OAKLAND DRIVE CENTER TURN LANE, ALL PROPOSED LANES ARE 12.0' WIDE AND ALL PROPOSED PAVED SHOULDERS ARE 4.0' WIDE (TO MATCH EXISTING LANE WIDTHS AND SHOULDER WIDTHS). PROPOSED GRAVEL SHOULDER SHALL BE 3.0' WIDE.

NOTE: SHOWN TREES SHALL BE PRESERVED.

NOTE: MULCH BLANKETS SHALL BE PROVIDED FOR DITCH STABILIZATION & RESTORATION ALONG PROPOSED DITCH LINES.

NOTE: EXISTING MATURE TREES ALONG EACH SIDE OF OAKLAND DRIVE SHALL BE PRESERVED AS MUCH AS POSSIBLE.

NOTE: EXISTING MATURE TREES ON EAST SIDE OF OAKLAND DRIVE SHALL BE PRESERVED AS MUCH AS POSSIBLE.



LANE LINE STRIPING FOR OAKLAND DRIVE: ALL NEW LANE LINES SHALL BE SINGLE 6" STRIPES (WHITE) EXCEPT LANE LINES BETWEEN OPPOSING TRAFFIC DIRECTIONS SHALL BE DOUBLE 6" STRIPES (YELLOW).

OAKLAND DRIVE (PUBLIC R.O.W.)

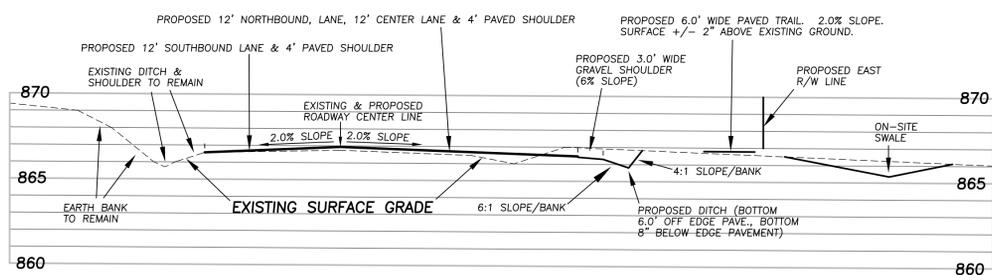
PROPOSED SAWCUT LINES FOR PAVEMENT REMOVAL. SEE CITY DETAIL SD-165. SAWCUT = ±32" x ±24" (±768 SQ.FT.)

PROPOSED PAVEMENT REPAIR:  
 3" OF MDOT 13A MODIFIED TOP COURSE (2 LAYERS)  
 5" OF MDOT 13A BASE COURSE (MIN. 2 LAYERS)  
 8" OF COMPACTED MDOT 22A GRAVEL BASE ON SUITABLE GRANULAR/SAND SUB-BASE. (12" SAND MIN.)

NOTE: EXISTING GRAVEL SHOULDER, DITCH, BANK, AND VEGETATION ON THE WEST SIDE OF OAKLAND DRIVE SHALL NOT BE DISTURBED.

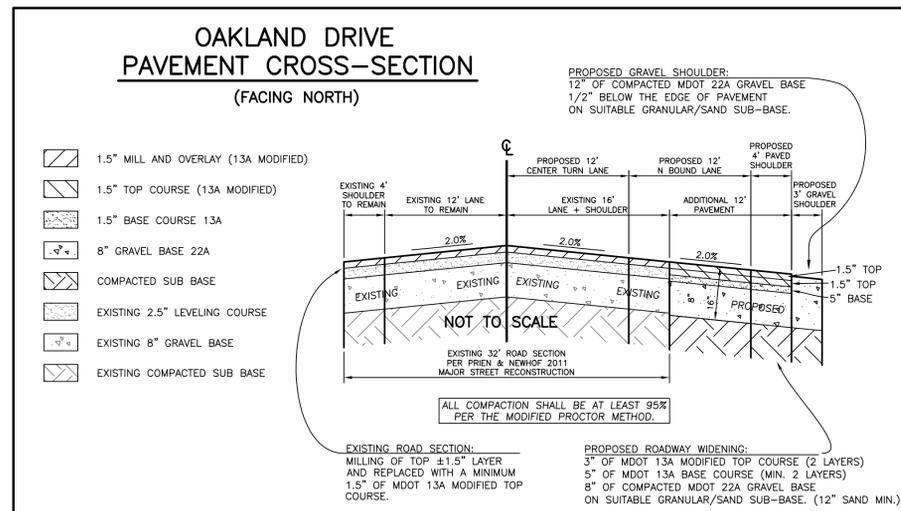
NOTE: FOR THE 6.0' WIDE PEDESTRIAN TRAIL WITHIN THE OAKLAND DRIVE RIGHT-OF-WAY, THE TRAIL SHALL SLOPE TOWARDS THE WEST. THE EAST EDGE OF THE TRAIL SHALL BE 0.12' (1.5") HIGHER THAN THE WEST EDGE OF THE TRAIL. (IF, IN ANY AREA, THE 6.0' TRAIL SLOPES TO THE EAST, A DRAINAGE AGREEMENT / EASEMENT SHALL BE PROVIDED TO THE CITY)

- LEGEND**
- EXISTING PUBLIC SANITARY SEWER
  - PROPOSED 8" PUBLIC SANITARY SEWER OR PRIVATE 6" SEWER LEAD
  - EXISTING PRIVATE STORM SEWER
  - PROPOSED PRIVATE STORM SEWER
  - EXISTING PUBLIC WATER MAIN
  - PROPOSED 8" PUBLIC WATER MAIN OR 6" HYDRANT LEAD
  - M EXISTING MAILBOX
  - P EXISTING UTILITY PEDESTAL
  - A EXISTING ADDRESS ON POST
  - DW EXISTING DRYWELL

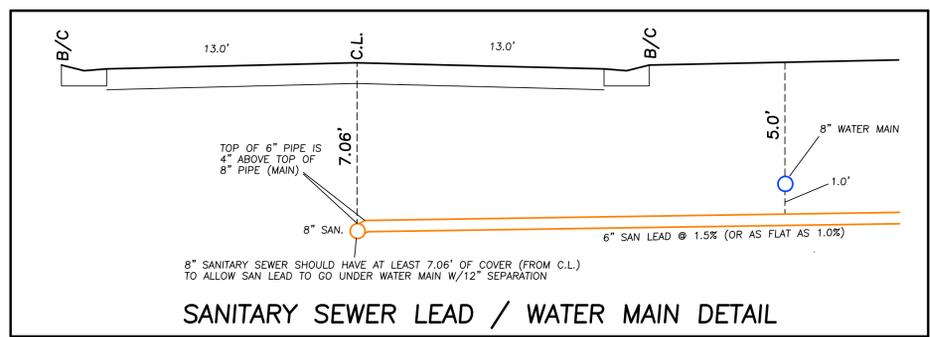


SECTION A-A (LOOKING NORTH)  
 (1" = 10' HORIZONTAL, 1" = 5' VERTICAL)

NOTE: FOR THE FIRE HYDRANT ALONG OAKLAND DRIVE, IF A FLANGED AUXILIARY VALVE IS CONNECTED TO THE HYDRANT, OR THE JOINTS ARE NOT RESTRAINED, LINE STOP(S) SHALL BE REQUIRED TO COMPLETE THE HYDRANT EXTENSIONS AND FLANGED COMPONENTS SHALL BE REPLACED WITH MECHANICAL JOINTS.  
 NOTE: FIRE HYDRANT LOCATIONS SHOWN WITHIN THESE PLANS ARE SUBJECT TO FINAL ADJUSTMENT BASED ON FIRE DEPARTMENT & CITY ENGINEER REQUIREMENTS.



ALSO SEE CITY OF PORTAGE STANDARD DETAIL SD-165.



OAKLAND DRIVE DETAIL <b>OAKLAND COMMONS</b> PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN		DATE: <b>2/28/2024</b>
<b>MONUMENT ENGINEERING GROUP ASSOC., INC.</b> Formerly Ingersoll, Watson & McMachen, Inc. 1209 East Milham Road, Suite B • Portage, Michigan 49002 • Phone 269 344-6165		SHEET: <b>4</b>
		JOB No.: <b>23-039</b>

**LEGEND**

- EXISTING PUBLIC SANITARY SEWER
- PROPOSED 8" PUBLIC SANITARY SEWER OR PRIVATE 6" SEWER LEAD
- EXISTING PRIVATE STORM SEWER
- PROPOSED PRIVATE STORM SEWER
- EXISTING PUBLIC WATER MAIN
- PROPOSED 8" PUBLIC WATER MAIN OR PRIVATE 1.25" COPPER WATER SERVICE
- SUGGESTED GAS & ELECTRIC LINES
- PROPOSED 4.0' WIDE CONCRETE SIDEWALK
- PROPOSED CROSSWALK WITH PEDESTRIAN CROSSING SIGNS (IF REQUIRED) & 1.0' WIDE PAINTED CROSSWALK STRIPES
- PROPOSED 2.0' WIDE CONCRETE ROLL CURB
- BUILDING SETBACK LINE
- EDGE OF EASEMENT
- PROPOSED FIRE HYDRANT ASSEMBLY (8" x 6" TEE, 6" GATE VALVE, & HYD.)
- PROPOSED SIGN
- \* SUGGESTED LOCATION OF PROPOSED STREET LIGHT

**SOIL/GROUNDWATER CONTAMINATION NOTES:**

- IF SOIL AND/OR GROUNDWATER CONTAMINATION IS ENCOUNTERED OR SUSPECTED DURING CONSTRUCTION THE FOLLOWING STEPS SHALL BE FOLLOWED:
- 1) CONTRACTOR SHALL STOP WORK IMMEDIATELY AND NOTIFY OWNER AND/OR OWNER'S REPRESENTATIVE.
  - 2) OWNER AND/OR OWNER'S REPRESENTATIVE SHALL NOTIFY THE CITY.
  - 3) OWNER SHALL ENGAGE THE SERVICES OF A QUALIFIED ENVIRONMENTAL CONSULTANT.
  - 4) CONSULTANT SHALL CONDUCT A SITE ASSESSMENT AND REPORT TO THE ABOVE PARTIES.
  - 5) CONSULTANT SHALL DEVELOP A PLAN TO ADDRESS ANY CONTAMINATION WHICH MAY BE FOUND DURING CONSTRUCTION.
  - 6) THE CONSULTANT TOGETHER WITH THE DESIGN ENGINEER SHALL COMPLETE A SITE EVALUATION PROCESS FOLLOWING THE DOCUMENT ENTITLED IMPLEMENTING STORMWATER INFILTRATION PRACTICES AT VACANT PARCELS AND BROWNFIELD SITES (EPA, 2013) TO DETERMINE IF THE STORMWATER MANAGEMENT SYSTEM REDESIGN AND RESUBMITTAL TO THE CITY FOR REVIEW AND APPROVAL.
  - 7) OWNER AND/OR OWNER'S REPRESENTATIVE SHALL SUBMIT TO THE CITY SUPPORTING DOCUMENTATION OF THE SITE EVALUATION PROCESS. A REVISED STORMWATER REVIEW PACKAGE MUST ALSO BE SUBMITTED IF IT BECOMES NECESSARY TO CHANGE THE DESIGN OF THE STORMWATER MANAGEMENT SYSTEM.
  - 8) MICHIGAN DEPARTMENT OF ENVIRONMENT, GREAT LAKES, AND ENERGY (EGLE) REMEDIATION OF REDEVELOPMENT DIVISION (RRD) STAFF SHALL BE NOTIFIED AS APPLICABLE.

**GENERAL NOTES:**

- 1) LIGHT POLES AND ANY UTILITY POLE/BOX SHALL NOT BE LOCATED WITHIN THE PUBLIC SEWER & WATER EASEMENT.
- 2) STREET NAME SIGNS SHALL BE AT LEAST 8" FROM TOP TO BOTTOM OF ACTUAL SIGN (& PLACED AT STOP SIGNS).
- 3) ALL PAVEMENT MARKINGS AND ALL TRAFFIC SIGNAGE SHALL COMPLY WITH STANDARDS IN THE M.M.U.T.C.D. (CURRENT EDITION).
- 4) U-CHANNEL POSTS REQUIRED FOR ALL SIGNS.
- 5) SOIL EROSION & SEDIMENTATION CONTROL PERMIT SHALL BE OBTAINED AS REQUIRED BY STATE LAW.
- 6) PERMIT(S) SHALL BE OBTAINED FROM THE CITY OF PORTAGE AS REQUIRED.
- 7) E.G.L.E. CONSTRUCTION PERMITS REQUIRED (FOR BOTH PUBLIC WATER MAIN & PUBLIC SANITARY SEWER) PRIOR TO THE COMMENCEMENT OF UTILITY INSTALLATION.
- 8) "NO PARKING THIS SIDE OF STREET" SIGNS SHALL BE INSTALLED ATOP THE FIRE HYDRANT SIGNS AND MIDWAY BETWEEN THE FIRE HYDRANTS ON THE HYDRANT SIDE OF THE STREETS.
- 9) FIVE INCH "STORZ" FITTINGS SHALL BE USED ON FIRE HYDRANTS.
- 10) FOR ON-SITE DWELLINGS, BASEMENT FOOTINGS AND FOUNDATIONS SHALL COMPLY WITH BUILDING CODE REQUIREMENTS BASED ON TYPE OF SOIL PRESENT AND STABILITY OF SOIL.
- 11) FOR ON-SITE DWELLINGS, REQUIRED RADON VENTING SYSTEM BELOW BASEMENT FLOOR SHALL AS A PRECAUTION, BE PLUMBED TO ALSO BE A DRAINAGE SYSTEM DURING PERIODS OF HIGH GROUNDWATER.
- 12) ALL PROPOSED STREET LIGHTS SHALL COMPLY WITH CITY OF PORTAGE STANDARDS AND CONSUMERS ENERGY STANDARDS WITH REGARDS TO LOCATION AND LIGHT SPECIFICATION.
- 13) AS-BUILT CONSTRUCTION PLANS SHALL BE SUBMITTED TO THE CITY UPON COMPLETION OF THE ROADWAY PROJECT.
- 14) UNDERGROUND GAS, ELECTRIC, TELEPHONE AND CABLE TV LINES SHALL BE DESIGNED BY UTILITY OWNERS AND SHALL NOT BE LOCATED IN PROPOSED STREET RIGHT-OF-WAYS UNLESS UTILITY IS CROSSING STREET (SQUARE TO CENTER LINE).
- 15) FIRE HYDRANT SIGNS SHALL BE LOCATED 4.0 FEET FROM THE CENTER OF THE FIRE HYDRANT.
- 16) PRIOR TO START OF CONSTRUCTION, PRE-CONSTRUCTION MEETING SHALL BE HELD AT CITY OF PORTAGE OFFICES.
- 17) IF A FIRE HYDRANT EXTENSION IS USED TO RAISE A HYDRANT, THE EXTENSION SHALL BE PAINTED THE SAME COLOR OF THE HYDRANT PER CITY OF PORTAGE SPECIFICATIONS.
- 18) A "STORM WATER BEST MANAGEMENT PRACTICES OPERATIONS & MAINTENANCE AGREEMENT" SHALL BE COMPLETED AND PREPARED PRIOR TO OBTAINING CITY FINAL APPROVAL.

**BARRIER FREE RAMP NOTES:**

- 1) SIDEWALK RAMP SHALL BE AT LEAST 5.0' WIDE
- 2) LANDINGS SHALL BE AT LEAST 5.0' BY 5.0'
- 3) IF A RAMP HAS A LONGITUDINAL SLOPE GREATER THAN 5.0%, THEN A LANDING IS REQUIRED AT THE TOP OF THE RAMP.
- 4) THE MAXIMUM SLOPE OF A RAMP SHALL BE 8.33% (OR 1" FALL OVER 1", OR 10" FALL OVER 10').
- 5) THE MAXIMUM CROSS SLOPE FOR ANY RAMP OR SIDEWALK SHALL BE 2.0%.
- 6) SIDEWALK RAMP ARE TO BE CONSTRUCTED IN ACCORDANCE WITH MDT DETAIL R-28-J WITH THE EXCEPTION THAT THE DETECTABLE WARNING PLATES MUST BE CAST IRON WITH BLACK ASPHALT COATING.
- 7) SEE MDT DETAIL FOR ANY ADDITIONAL REQUIREMENTS.
- 8) ANY RAMP AND LANDINGS THAT DO NOT COMPLY SHALL BE REMOVED AND REPLACED.

NOTE: ALL WATER MAINS & THEIR APPURTENANCES SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY OF PORTAGE SPECIFICATIONS AND A.W.W.A. STANDARD C600.

NOTE: WATER MAIN FLUSHING SHALL PROVIDE A MINIMUM WATER VELOCITY OF 3.0 FEET PER SECOND IN ACCORDANCE WITH A.W.W.A. STANDARD C651.

NOTE: ALL SIDEWALK RAMP ( @ CURB) TO BE CONSTRUCTED IN ACCORDANCE WITH MDT DETAIL R-28-J WITH THE EXCEPTION THAT THE DETECTABLE WARNING PLATES SHALL BE CAST IRON WITH A BLACK ASPHALT COATING.

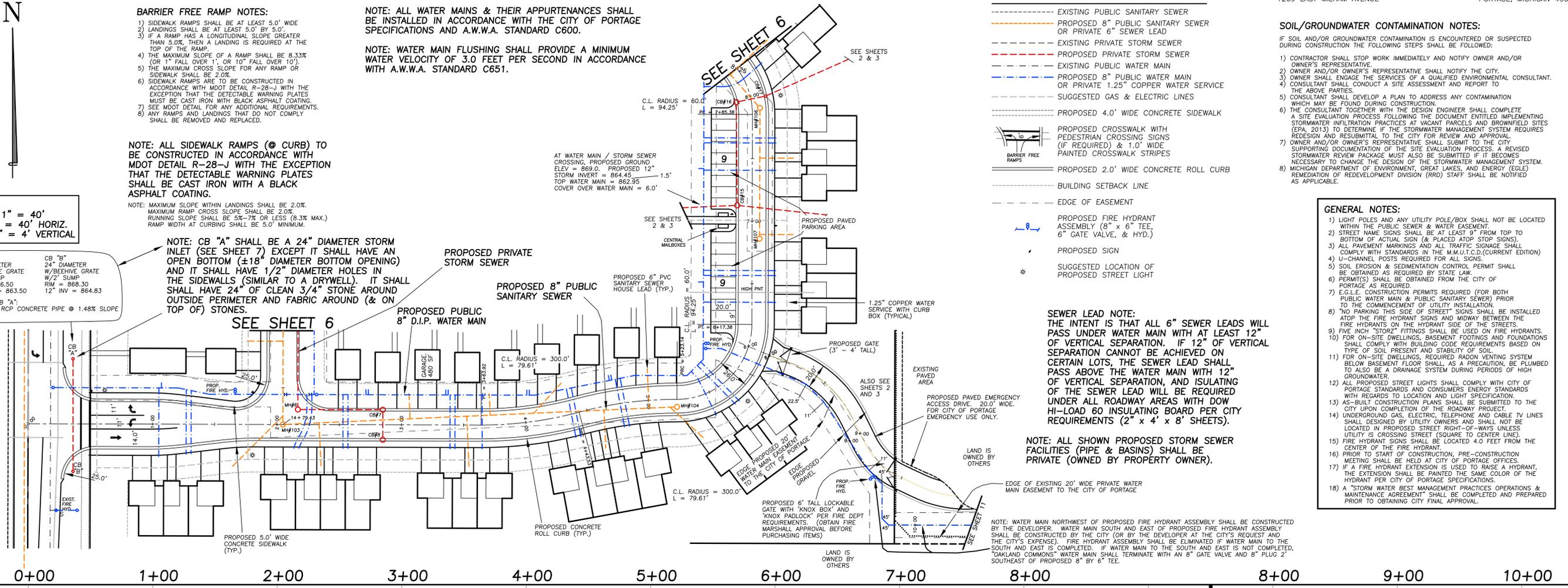
NOTE: MAXIMUM SLOPE WITHIN LANDINGS SHALL BE 2.0%. MAXIMUM RAMP CROSS SLOPE SHALL BE 2.0%. RUNNING SLOPE SHALL BE 5%-7% OR LESS (8.3% MAX.). RAMP WIDTH AT CURBING SHALL BE 5.0' MINIMUM.

NOTE: CB "A" SHALL BE A 24" DIAMETER STORM INLET (SEE SHEET 7) EXCEPT IT SHALL HAVE AN OPEN BOTTOM (±18" DIAMETER BOTTOM OPENING) AND IT SHALL HAVE 1/2" DIAMETER HOLES IN THE SIDEWALLS (SIMILAR TO A DRYWELL). IT SHALL HAVE 24" OF CLEAN 3/4" STONE AROUND OUTSIDE PERIMETER AND FABRIC AROUND (& ON TOP OF) STONES.

**SCALES:**  
 PLAN VIEW: 1" = 40'  
 PROFILE: 1" = 40' HORIZ.  
 1" = 4' VERTICAL

- CB "A" 24" DIAMETER W/BEEHIVE GRATE W/2" SUMP RIM = 866.50 12" INV = 863.50
- CB "B" 24" DIAMETER W/BEEHIVE GRATE W/2" SUMP RIM = 868.30 12" INV = 864.83
- CB "B" TO CB "A": 90.0' - 12" RCP CONCRETE PIPE @ 1.48% SLOPE

NOTE: STREET CUT PERMIT IS NEEDED PRIOR TO DOING ANY WORK IN OAKLAND DR.



**SEWER LEAD NOTE:**  
 THE INTENT IS THAT ALL 6" SEWER LEADS WILL PASS UNDER WATER MAIN WITH AT LEAST 12" OF VERTICAL SEPARATION. IF 12" OF VERTICAL SEPARATION CANNOT BE ACHIEVED ON CERTAIN LOTS, THE SEWER LEAD SHALL PASS ABOVE THE WATER MAIN WITH 12" OF VERTICAL SEPARATION, AND INSULATING OF THE SEWER LEAD WILL BE REQUIRED UNDER ALL ROADWAY AREAS WITH DOW HI-LOAD 60 INSULATING BOARD PER CITY REQUIREMENTS (2" x 4' x 8' SHEETS).

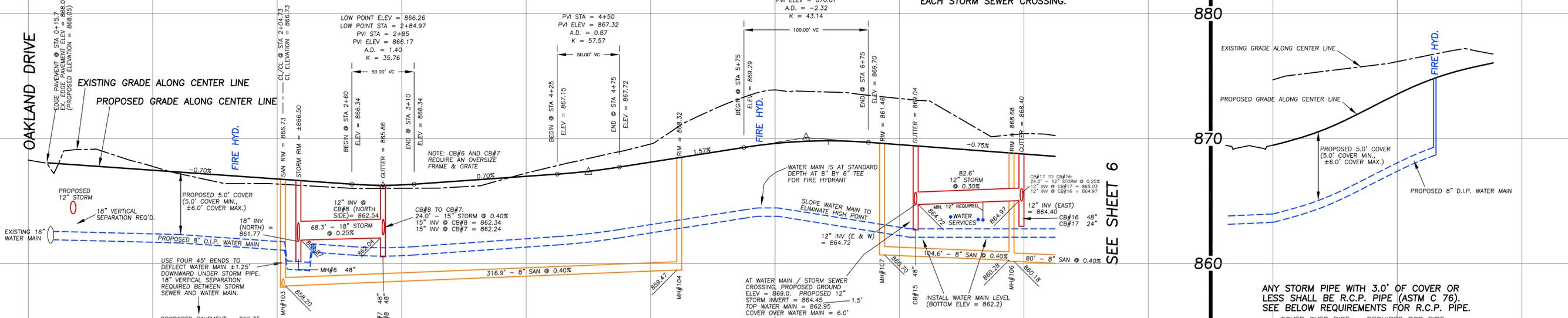
NOTE: ALL SHOWN PROPOSED STORM SEWER FACILITIES (PIPE & BASINS) SHALL BE PRIVATE (OWNED BY PROPERTY OWNER).

NOTE: WATER MAIN NORTHWEST OF PROPOSED FIRE HYDRANT ASSEMBLY SHALL BE CONSTRUCTED BY THE DEVELOPER. WATER MAIN SOUTH AND EAST OF PROPOSED FIRE HYDRANT ASSEMBLY SHALL BE CONSTRUCTED BY THE CITY (OR BY THE DEVELOPER AT THE CITY'S REQUEST AND THE CITY'S EXPENSE). FIRE HYDRANT ASSEMBLY SHALL BE ELIMINATED IF WATER MAIN TO THE SOUTH AND EAST IS COMPLETED. IF WATER MAIN TO THE SOUTH AND EAST IS NOT COMPLETED, "OAKLAND COMMONS" WATER MAIN SHALL TERMINATE WITH AN 8" GATE VALVE AND 8" PLUG 2' SOUTHWEST OF PROPOSED 8" BY 6" TEE.

**KINGFISHER CIRCLE  
 STA 0+00 TO STA 8+25**

NOTE: ALL WATER SERVICES SHALL HAVE 5.0' - 6.0' OF COVER, SHALL PASS OVER SANITARY SEWER WITH AT LEAST 1.0' OF VERTICAL SEPARATION, AND SHALL HAVE AT LEAST 1.0' OF VERTICAL SEPARATION AT EACH STORM SEWER CROSSING.

**EMERGENCY ACCESS DRIVE  
 STA 8+00 TO STA 9+38**



SEE SANITARY SEWER LEAD/  
 WATER MAIN DETAIL ON  
 SHEET 4

NOTE: SOME DEWATERING  
 MAY BE NECESSARY.

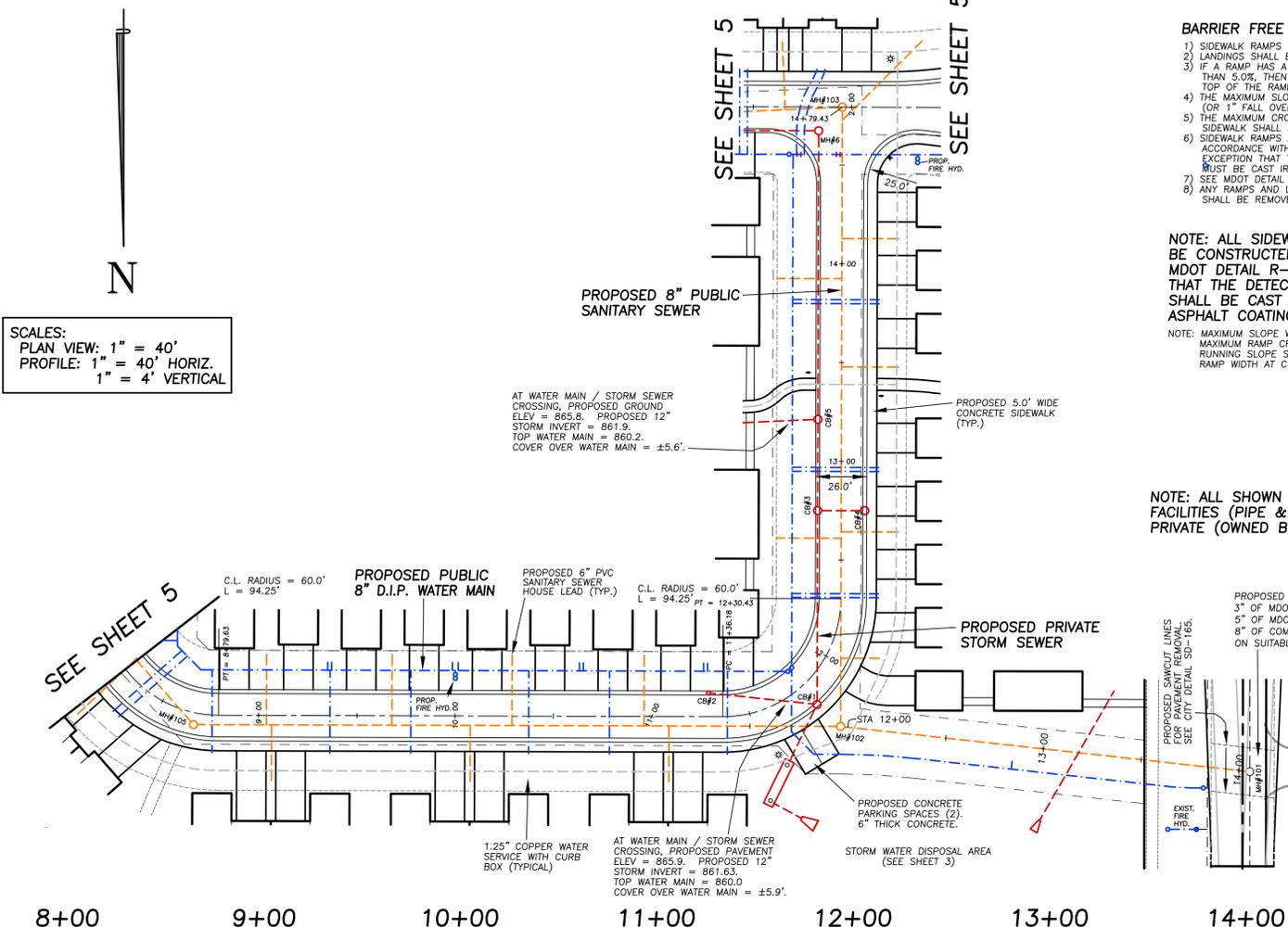
ANY STORM PIPE WITH 3.0' OF COVER OR LESS SHALL BE R.C.P. PIPE (ASTM C 76). SEE BELOW REQUIREMENTS FOR R.C.P. PIPE.

COVER OVER PIPE	REQUIRED RCP PIPE	REQUIRED RCP PIPE
0' - 3.0'	CLASS IV OR V	CLASS IV OR V
3.1' - 9.9'	CLASS III	CLASS III
10' - 15.9'	CLASS III	CLASS III
16' - 22.9'	CLASS IV	CLASS IV
23' - 33'	CLASS V	CLASS V



- SOIL/GROUNDWATER CONTAMINATION NOTES:**  
 IF SOIL AND/OR GROUNDWATER CONTAMINATION IS ENCOUNTERED OR SUSPECTED DURING CONSTRUCTION THE FOLLOWING STEPS SHALL BE FOLLOWED:
- 1) CONTRACTOR SHALL STOP WORK IMMEDIATELY AND NOTIFY OWNER AND/OR OWNER'S REPRESENTATIVE.
  - 2) OWNER AND/OR OWNER'S REPRESENTATIVE SHALL NOTIFY THE CITY.
  - 3) OWNER SHALL ENGAGE THE SERVICES OF A QUALIFIED ENVIRONMENTAL CONSULTANT.
  - 4) CONSULTANT SHALL CONDUCT A SITE ASSESSMENT AND REPORT TO THE ABOVE PARTIES.
  - 5) CONSULTANT SHALL DEVELOP A PLAN TO ADDRESS ANY CONTAMINATION WHICH MAY BE FOUND DURING CONSTRUCTION.
  - 6) THE CONSULTANT TOGETHER WITH THE DESIGN ENGINEER SHALL COMPLETE A SITE EVALUATION PROCESS FOLLOWING THE DOCUMENT ENTITLED IMPLEMENTING STORMWATER INFILTRATION PRACTICES AT VACANT PARCELS AND BROWNFIELD SITES (EPA, 2013) TO DETERMINE IF THE STORMWATER MANAGEMENT SYSTEM REQUIRES REDESIGN AND RESUBMITTAL TO THE CITY FOR REVIEW AND APPROVAL.
  - 7) OWNER AND/OR OWNER'S REPRESENTATIVE SHALL SUBMIT TO THE CITY SUPPORTING DOCUMENTATION OF THE SITE EVALUATION PROCESS. A REVISED STORMWATER REVIEW PACKAGE MUST ALSO BE SUBMITTED IF IT BECOMES NECESSARY TO CHANGE THE DESIGN OF THE STORMWATER MANAGEMENT SYSTEM.
  - 8) MICHIGAN DEPARTMENT OF ENVIRONMENT, GREAT LAKES, AND ENERGY (EGLE) REMEDIATION OF REDEVELOPMENT DIVISION (RD) STAFF SHALL BE NOTIFIED AS APPLICABLE.

**SCALES:**  
 PLAN VIEW: 1" = 40'  
 PROFILE: 1" = 40' HORIZ.  
 1" = 4' VERTICAL



- BARRIER FREE RAMP NOTES:**
- 1) SIDEWALK RAMP SHALL BE AT LEAST 5.0' WIDE
  - 2) LANDINGS SHALL BE AT LEAST 5.0' BY 5.0'
  - 3) IF A RAMP HAS A LONGITUDINAL SLOPE GREATER THAN 5.0%, THEN A LANDING IS REQUIRED AT THE TOP OF THE RAMP.
  - 4) THE MAXIMUM SLOPE OF A RAMP SHALL BE 8.33% (OR 1" FALL OVER 12', OR 10" FALL OVER 10')
  - 5) THE MAXIMUM CROSS SLOPE FOR ANY RAMP OR SIDEWALK SHALL BE 2.0%.
  - 6) SIDEWALK RAMP ARE TO BE CONSTRUCTED IN ACCORDANCE WITH MDOT DETAIL R-28-J WITH THE EXCEPTION THAT THE DETECTABLE WARNING PLATES MUST BE CAST IRON WITH BLACK ASPHALT COATING.
  - 7) SEE MDOT DETAIL FOR ANY ADDITIONAL REQUIREMENTS.
  - 8) ANY RAMP AND LANDINGS THAT DO NOT COMPLY SHALL BE REMOVED AND REPLACED.

**NOTE: ALL SIDEWALK RAMP(S) TO CURB) TO BE CONSTRUCTED IN ACCORDANCE WITH MDOT DETAIL R-28-J WITH THE EXCEPTION THAT THE DETECTABLE WARNING PLATES SHALL BE CAST IRON WITH A BLACK ASPHALT COATING.**

**NOTE: MAXIMUM SLOPE WITHIN LANDINGS SHALL BE 2.0%. MAXIMUM RAMP CROSS SLOPE SHALL BE 2.0%. RUNNING SLOPE SHALL BE 5%-7% OR LESS (8.33% MAX.). RAMP WIDTH AT CURBING SHALL BE 5.0' MINIMUM.**

**NOTE: ALL SHOWN PROPOSED STORM SEWER FACILITIES (PIPE & BASINS) SHALL BE PRIVATE (OWNED BY PROPERTY OWNER).**

**SEWER LEAD NOTE:**  
 THE INTENT IS THAT ALL 6" SEWER LEADS WILL PASS UNDER WATER MAIN WITH AT LEAST 12" OF VERTICAL SEPARATION. IF 12" OF VERTICAL SEPARATION CANNOT BE ACHIEVED ON CERTAIN LOTS, THE SEWER LEAD SHALL PASS ABOVE THE WATER MAIN WITH 12" OF VERTICAL SEPARATION, AND INSULATING UNDER ALL ROADWAY AREAS WITH DOW HI-LOAD 60 INSULATING BOARD PER CITY REQUIREMENTS (2' x 4' x 8' SHEETS).

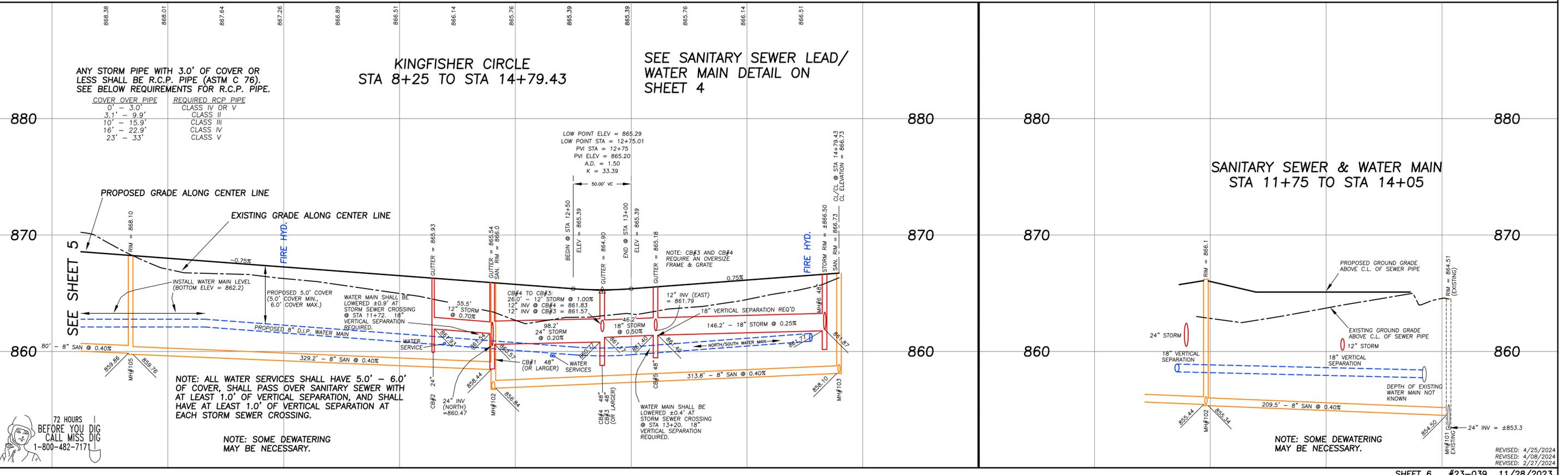
**NOTE: ALL WATER MAINS & THEIR APPURTENANCES SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY OF PORTAGE SPECIFICATIONS AND A.W.W.A. STANDARD C600.**

**NOTE: WATER MAIN FLUSHING SHALL PROVIDE A MINIMUM WATER VELOCITY OF 3.0 FEET PER SECOND IN ACCORDANCE WITH A.W.W.A. STANDARD C651.**

**LEGEND**

- EXISTING PUBLIC SANITARY SEWER
- PROPOSED 8" PUBLIC SANITARY SEWER OR PRIVATE 6" SEWER LEAD
- EXISTING PRIVATE STORM SEWER
- PROPOSED PRIVATE STORM SEWER
- EXISTING PUBLIC WATER MAIN
- PROPOSED 8" PUBLIC WATER MAIN OR PRIVATE 1.25" COPPER WATER SERVICE
- SUGGESTED GAS & ELECTRIC LINES
- PROPOSED 4.0' WIDE CONCRETE SIDEWALK
- PROPOSED 2.0' WIDE CONCRETE ROLL CURB
- BUILDING SETBACK LINE
- EDGE OF EASEMENT
- PROPOSED FIRE HYDRANT ASSEMBLY (8" x 6" TEE, 6" GATE VALVE, & HYD.)
- PROPOSED SIGN
- SUGGESTED LOCATION OF PROPOSED STREET LIGHT

- GENERAL NOTES:**
- 1) LIGHT POLES AND ANY UTILITY POLE/BOX SHALL NOT BE LOCATED WITHIN THE PUBLIC SEWER & WATER EASEMENT.
  - 2) STREET NAME SIGNS SHALL BE AT LEAST 9' FROM TOP TO BOTTOM OF ACTUAL SIGN (& PLACED ATOP STOP SIGNS).
  - 3) ALL PAVEMENT MARKINGS AND ALL TRAFFIC SIGNAGE SHALL COMPLY WITH STANDARDS IN THE M.M.U.T.O.D. (CURRENT EDITION)
  - 4) U-CHANNEL POSTS REQUIRED FOR ALL SIGNS.
  - 5) SOIL EROSION & SEDIMENTATION CONTROL PERMIT SHALL BE OBTAINED AS REQUIRED BY STATE LAW.
  - 6) PERMIT(S) SHALL BE OBTAINED FROM THE CITY OF PORTAGE AS REQUIRED.
  - 7) E.G.L.E. CONSTRUCTION PERMITS REQUIRED (FOR BOTH PUBLIC WATER MAIN & PUBLIC SANITARY SEWER) PRIOR TO THE COMMENCEMENT OF UTILITY INSTALLATION.
  - 8) "NO PARKING THIS SIDE OF STREET" SIGNS SHALL BE INSTALLED ATOP THE FIRE HYDRANT SIGNS AND MIDWAY BETWEEN THE FIRE HYDRANTS ON THE HYDRANT SIDE OF THE STREETS.
  - 9) FIVE INCH "STORZ" FITTINGS SHALL BE USED ON FIRE HYDRANTS.
  - 10) FOR ON-SITE DWELLINGS, BASEMENT FOOTINGS AND FOUNDATIONS SHALL COMPLY WITH BUILDING CODE REQUIREMENTS BASED ON TYPE OF SOIL PRESENT AND STABILITY OF SOIL.
  - 11) FOR ON-SITE DWELLINGS, REQUIRED RADON VENTING SYSTEM BELOW BASEMENT FLOOR SHALL, AS A PRECAUTION, BE PLUMBED TO ALSO BE A DRAINAGE SYSTEM DURING PERIODS OF HIGH GROUNDWATER.
  - 12) ALL PROPOSED STREET LIGHTS SHALL COMPLY WITH CITY OF PORTAGE STANDARDS AND CONSUMERS ENERGY STANDARDS WITH REGARDS TO LOCATION AND LIGHT SPECIFICATION.
  - 13) AS-BUILT CONSTRUCTION PLANS SHALL BE SUBMITTED TO THE CITY UPON COMPLETION OF THE ROADWAY PROJECT.
  - 14) UNDERGROUND GAS, ELECTRIC, TELEPHONE AND CABLE TV LINES SHALL BE DESIGNED BY UTILITY OWNERS AND SHALL NOT BE LOCATED IN PROPOSED STREET RIGHT-OF-WAYS UNLESS UTILITY IS CROSSING STREET (SQUARE TO CENTER LINE).
  - 15) FIRE HYDRANT SIGNS SHALL BE LOCATED 4.0 FEET FROM THE CENTER OF THE FIRE HYDRANT.
  - 16) PRIOR TO START OF CONSTRUCTION, PRE-CONSTRUCTION MEETING SHALL BE HELD AT CITY OF PORTAGE OFFICES.
  - 17) IF A FIRE HYDRANT EXTENSION IS USED TO RAISE A HYDRANT, THE EXTENSION SHALL BE PAINTED THE SAME COLOR OF THE HYDRANT PER CITY OF PORTAGE SPECIFICATIONS.
  - 18) A "STORM WATER BEST MANAGEMENT PRACTICES OPERATIONS & MAINTENANCE AGREEMENT" SHALL BE COMPLETED AND PREPARED PRIOR TO OBTAINING CITY FINAL APPROVAL.



REVISED: 4/25/2024  
 REVISED: 4/08/2024  
 REVISED: 2/27/2024

**GENERAL NOTES**

- 1) CONTRACTOR SHALL NOTIFY MISS DIG AT 1-800-482-7171 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- 2) LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. ADDITIONAL UNDERGROUND UTILITIES MAY EXIST ON THIS SITE WHICH ARE NOT SHOWN ON THESE PLANS. CONTRACTOR SHALL BE RESPONSIBLE TO LOCATE AND PROTECT ALL UTILITIES ON SITE, WHETHER SHOWN ON THESE PLANS OR NOT.
- 3) CONTRACTOR SHALL PROVIDE ALL SIGNS, FENCING, LIGHTS, BARRICADES, ETC. NECESSARY TO PROVIDE FOR PUBLIC SAFETY DURING CONSTRUCTION.
- 4) REQUESTS FOR CONSTRUCTION STAKING SHALL BE DIRECTED TO THE ENGINEER A MINIMUM OF 48 HOURS PRIOR TO THEIR ACTUAL NEED.
- 5) IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH ALL UTILITY COMPANIES FOR THE PROTECTION AND/OR RELOCATION OF EXISTING UTILITIES.
- 6) ALL MATERIALS PLACED DURING CONSTRUCTION SHALL BE IN NEW CONDITION, SHALL BE FREE FROM DEFECTS AND SHALL BE SUBJECT TO INSPECTION AND APPROVAL BY THE CITY AND THEIR INSPECTOR.
- 7) ALL CONSTRUCTION SHALL BE SUBJECT TO CONSTRUCTION INSPECTION BY OWNER, OWNER'S REPRESENTATIVE, AND THE CITY OF PORTAGE.
- 8) ELEVATIONS SHOWN ON THESE PLANS ARE BASED ON USGS DATUM AS PROMULGATED WITHIN THE CITY OF PORTAGE.
- 9) ALL WORK SHALL BE IN ACCORDANCE WITH THE CITY OF PORTAGE CONTRACT CONDITIONS AND SPECIFICATIONS.
- 10) A SOIL EROSION CONTROL PERMIT NEEDS TO BE OBTAINED PRIOR TO THE COMMENCEMENT OF ANY GRADING ACTIVITIES.

**CONSTRUCTION NOTES**

- 1) ALL PIPE LENGTH DISTANCES ARE FROM CENTER TO CENTER OF STRUCTURE.
- 2) ALL TRENCHING AND BACKFILL FOR UTILITIES SHALL COMPLY WITH REQUIREMENTS OF THE CITY OF PORTAGE STANDARD SPECIFICATIONS AND THE REQUIREMENTS INDICATED ON THE CONSTRUCTION DRAWINGS.
- 3) ALL CONSTRUCTION SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL RULES, REQUIREMENTS, AND LAWS.

**WATER MAIN NOTES**

- 1) ALL 8" OR 10" WATER MAIN SHALL BE AWWA C151, CLASS 52 DUCTILE IRON PIPE. ALL 12" WATER MAIN SHALL BE AWWA C151, CLASS 53 DUCTILE IRON PIPE. ALL PIPE SHALL HAVE CEMENT LINING AND SHALL BE INSTALLED ACCORDING TO THE CITY OF PORTAGE STANDARD SPECIFICATIONS (AVAILABLE AT CITY OF PORTAGE WEBSITE: www.portage.mi.gov).
- 2) ALL WATER MAIN CONSTRUCTION SHALL BE SUBJECT TO INSPECTION BY A CITY OF PORTAGE APPROVED INSPECTOR.
- 3) MINIMUM 5.0' AND MAXIMUM 6.0' OF COVER OVER NEW WATER MAIN PIPE IS REQUIRED ONCE FINAL SITE GRADING IS COMPLETE.
- 4) WATER SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH CITY OF PORTAGE STANDARD DETAIL SD-153, TYPICAL SERVICE LINE AND METER BOX INSTALLATIONS, PLUMBING PERMIT REQUIRED.
- 5) HYDRANT ASSEMBLIES SHALL BE INSTALLED IN ACCORDANCE WITH CITY OF PORTAGE STANDARD DETAIL SD-144, AUXILIARY VALVE LOCATION.
- 6) HYDRANT SIGNS SHALL BE INSTALLED IN ACCORDANCE WITH CITY OF PORTAGE STANDARD DETAIL SD-145, STANDARD HYDRANT SIGN. NOTE: FIRE HYDRANT SIGNS SHALL BE INSTALLED A MINIMUM 4.0' FROM THE CENTER OF THE INSTALLED HYDRANT PER THE CITY'S NEW REQUIREMENTS.

**SANITARY SEWER NOTES**

- 1) ALL SANITARY SEWER PIPE SHALL BE SDR26 PVC AND SHALL COMPLY WITH ASTM D 3033 OR ASTM D 3034. PIPE SHALL BE INSTALLED ACCORDING TO MANUFACTURER'S INSTRUCTIONS.
- 2) ALL SANITARY SEWER CONSTRUCTION, INCLUDING MAINLINES, LATERALS, MANHOLES, AND APPURTENANCES, SHALL CONFORM WITH THE CITY OF PORTAGE REQUIREMENTS AND SPECIFICATIONS AND SHALL BE SUBJECT TO THEIR INSPECTION.

**PRIVATE UTILITY NOTES**

- 1) UNDERGROUND ELECTRIC, NATURAL GAS, TELEPHONE, AND CABLE TV PRIVATE UTILITIES SHALL BE SIZED AND INSTALLED BY UTILITY OWNERS.

**STORM SEWER NOTES**

- 1) STORM SEWER PIPE SHALL BE EITHER CORRUGATED POLYETHYLENE (C.P.E.), REINFORCED CONCRETE (R.C.P.), OR OTHER TYPES MEETING MDOT AND CITY OF PORTAGE APPROVAL.
- 2) CORRUGATED POLYETHYLENE (C.P.E.) PIPE SHALL MEET THE REQUIREMENTS OF AASHTO M 294 TYPE 5 (DUAL WALL, SMOOTH INSIDE, CORRUGATED OUTSIDE, NO PERFORATIONS). THE JOINTS SHALL BE WATER TIGHT AND COMPLY WITH ASTM D 3212. (HANCOR HI-O, ADS N-12, OR EQUIVALENT)
- 3) REINFORCED CONCRETE PIPE (R.C.P.) SHALL MEET THE REQUIREMENTS OF ASTM C 76. THE JOINTS SHALL BE WATER TIGHT AND COMPLY WITH ASTM C 443.
- 4) ALL STORM SEWER PIPE AND INSTALLATION SHALL COMPLY WITH CITY OF PORTAGE "CONSTRUCTION SPECIFICATIONS AND STANDARDS"
- 5) PREMIUM WATER TIGHT JOINTS (RUBBER OR SLP SEAL) ARE REQUIRED AT EACH STORM SEWER PIPE JOINT IN ACCORDANCE WITH CITY OF PORTAGE REQUIREMENTS.
- 6) PIPE CONNECTIONS AT ALL SOLID WALLED STRUCTURES SHALL BE MADE WITH A WATER TIGHT RESILIENT CONNECTOR (RUBBER TYPE BOOT WITH STAINLESS STEEL BANDS, OR OTHER TYPE AS APPROVED BY CITY OF PORTAGE).
- 7) STORM SEWER PIPE SHALL HAVE A MINIMUM OF 24 INCHES OF COVER.
- 8) ALL STORM SEWER PIPE, JOINTS, AND GASKETS SHALL BE INSPECTED. ANY DEFECTIVE PRODUCTS SHALL NOT BE INSTALLED AND SHALL BE REMOVED FROM THE SITE.
- 9) ALL SOLID WALLED STRUCTURES SHALL HAVE CONCRETE BOTTOMS.
- 10) DEFLECTION TESTS SHALL BE PERFORMED ON ALL FLEXIBLE PIPE. THE TEST SHALL BE CONDUCTED AFTER THE BACKFILL HAS BEEN PLACED AND PRIOR TO INSTALLING ANY ASPHALT OR CONCRETE SURFACE. NO PIPE SHALL EXCEED A DEFLECTION OF FIVE (5) PERCENT. THE DEFLECTION TEST SHALL BE RUN USING A RIGID BALL OR MANDREL, HAVING A DIAMETER EQUAL TO 95% OF THE INSIDE DIAMETER OF THE PIPE. DEFLECTION SHALL BE CONDUCTED AFTER FINAL BACKFILL HAS BEEN IN PLACE FOR AT LEAST 30 DAYS.

**SOIL EROSION CONTROL NOTES**

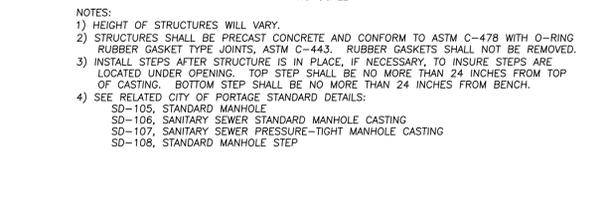
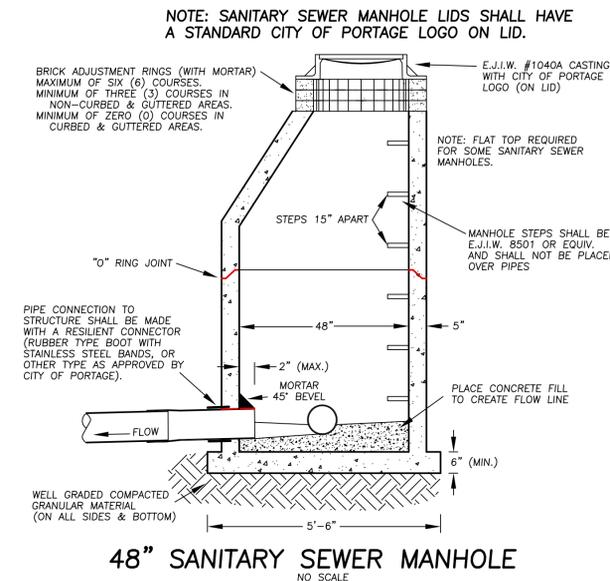
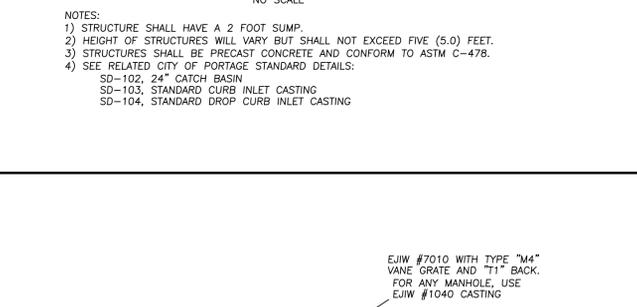
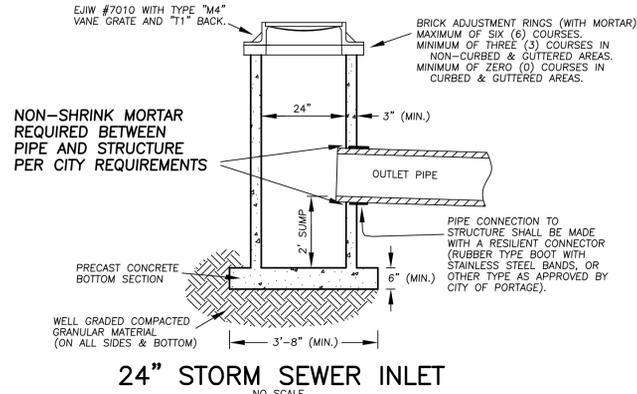
- 1) ALL CONSTRUCTION SHALL COMPLY WITH THE SOIL EROSION AND SEDIMENTATION CONTROL ACT (P.A. 451 OF 1994, AS AMENDED) AS ADMINISTERED BY THE CITY OF PORTAGE.
- 2) EARTHWORK CONTRACTORS SHALL BE RESPONSIBLE FOR THE FOLLOWING:
  - A) BERMING OR FILTERING OF ADJACENT STREETS WHERE EROSION COULD OCCUR.
  - B) PROTECTION OF ALL EXISTING AND NEWLY CONSTRUCTED STORM SEWER STRUCTURES, WHETHER ADJACENT TO THE SITE OR ON THE SITE.
  - C) MINIMIZATION OF TRACKING OF MATERIAL OFF-SITE.
  - D) SWEEPING AND CLEANING OF ADJACENT STREETS AS NECESSARY.
- 3) OWNER SHALL OBTAIN 'SOIL EROSION CONTROL PERMIT' FROM THE CITY OF PORTAGE.

**RESTORATION NOTES**

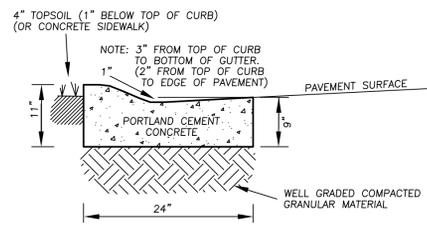
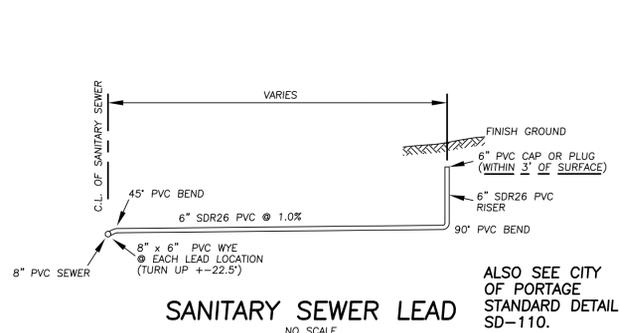
- 1) ALL DISTURBED AREAS SHALL BE RESTORED WITH MINIMUM 4" TOPSOIL AND GRASS SEED, OR OTHER REQUIRED LANDSCAPING.
- 2) EXISTING STREETS AND YARDS SHALL BE RESTORED TO A CONDITION AT LEAST EQUAL TO THAT WHICH EXISTED PRIOR TO CONSTRUCTION.

**STREET LIGHTS & STREET SIGNS**

- 1) DECIDUOUS TREES AT PLANTING SHALL HAVE A MINIMUM CALIPER OF 1.5" AT 6" ABOVE THE ROOT BALL; A CLEAR STEM OF 4 FEET; AND A BURLAP BALL SIZE OF AT LEAST 10 TIMES THE CALIPER. ALLOWED TREES INCLUDE: SARCENT CHERRY, AMUR CORKTREE, GOLDENRAIN TREE, MAPLES (HEDGE, PACIFIC SUNSET, TRIDENT, TATARIAN MAPLE), AMERICAN & EUROPEAN HORNBEEAM, KATSURA TREE, IVORY SILK JAPANESE TREE LILAC, & ORNAMENTAL PEARS (CLEVELAND AND REDSPIRE CALLERY PEAR).
- 2) STREET LIGHTS SHALL BE 8500 LUMEN, HIGH PRESSURE SODIUM VAPOR. DECORATIVE LIGHT POLES SHALL HAVE A HEIGHT OF 19 FEET. ALL LIGHT STANDARDS AND UTILITY POLES SHALL BE LOCATED AT LEAST 5 FEET OFF PROPOSED BACK OF CURB.
- 3) ALL PAVEMENT MARKINGS AND TRAFFIC SIGNS MUST CONFORM TO THE STANDARDS SET FORTH IN THE CURRENT EDITION OF THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES ('M.M.U.T.C.D.').
- 4) ALL STREET SIGNS SHALL CONFORM TO CITY OF PORTAGE PUBLISHED REQUIREMENTS. ALL SIGNS SHALL BE INSTALLED ON 3 POUND GALVANIZED U-CHANNEL POSTS AND SHALL HAVE A MINIMUM BURY OF 36 INCHES. DISTANCE FROM BOTTOM OF SIGN TO GROUND SHALL BE 7 FEET. ANYTHING NOT SPECIFIED IN THESE PLANS SHALL BE IN ACCORDANCE WITH THE M.M.U.T.C.D. STREET NAME SIGNS SHALL BE 9 INCHES TALL (VERTICAL DIMENSION OF ACTUAL SIGN).

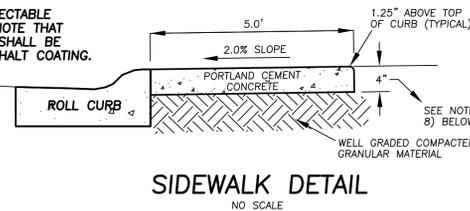


NOTE: ALL SANITARY SEWER SHALL BE SDR26 PIPE.

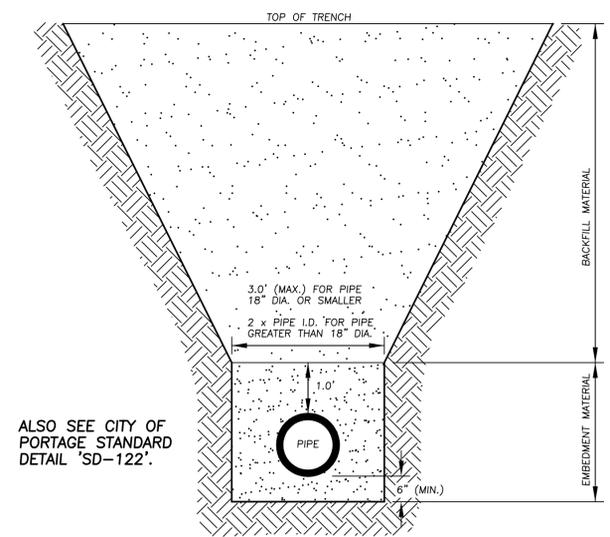
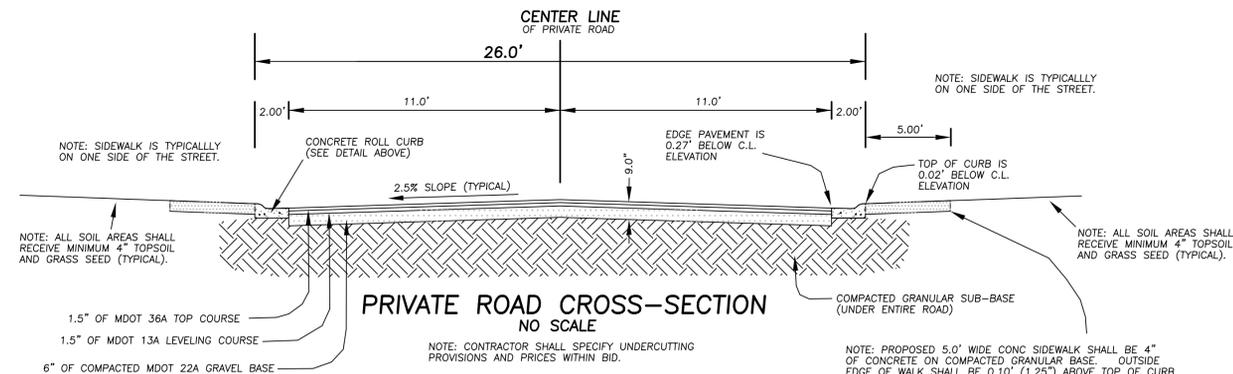


- NOTES:  
 1) CONCRETE SHALL BE MDOT GRADES 35P OR 35S, AND MEET THE FOLLOWING REQUIREMENTS:  
 • ALL AGGREGATES SHALL BE LIMESTONE OR SLAG MATERIAL.  
 • ENTRAINED AIR = 6 TO 8 PERCENT  
 • COMPRESSIVE STRENGTH AT 28 DAYS = 3500 P.S.I.  
 • SLUMP = 1.5 TO 3 INCHES  
 • MODULUS OF RUPTURE AT 28 DAYS = 650 P.S.I.  
 • MINIMUM PORTLAND CEMENT CONTENT = 5.50 SACKS PER CUBIC YARD  
 2) EXPANSION JOINTS SHALL BE PLACED AT SPRING POINTS AND UNIFORM INTERVALS OF NOT MORE THAN 100 FEET.  
 3) PLANE OF WEAKNESS JOINTS SHALL BE PLACED AT UNIFORM INTERVALS OF TEN (10) FEET. IRREGULAR SECTIONS SHALL NOT BE LESS THAN EIGHT (8) FEET.  
 4) CONCRETE SHALL BE CURED IN A MANNER APPROVED BY THE CITY OF PORTAGE.

SEE MDOT CURB RAMP & DETECTABLE WARNING DETAILS (R-28-J). NOTE THAT DETECTABLE WARNING PLATES SHALL BE CAST IRON WITH A BLACK ASPHALT COATING.



- NOTES:  
 1) CONCRETE SHALL BE MDOT GRADES 35P OR 35S, AND MEET THE FOLLOWING REQUIREMENTS:  
 • ALL AGGREGATES SHALL BE LIMESTONE OR SLAG MATERIAL.  
 • ENTRAINED AIR = 6 TO 8 PERCENT  
 • COMPRESSIVE STRENGTH AT 28 DAYS = 3500 P.S.I.  
 • SLUMP = 1.5 TO 3 INCHES  
 • MODULUS OF RUPTURE AT 28 DAYS = 650 P.S.I.  
 • MINIMUM PORTLAND CEMENT CONTENT = 5.50 SACKS PER CUBIC YARD  
 2) ONE (1) INCH TRANSVERSE EXPANSION JOINTS SHALL BE PLACED AT UNIFORM INTERVALS OF NOT MORE THAN 50 FEET.  
 3) PLANE OF WEAKNESS JOINTS SHALL BE PLACED AT UNIFORM INTERVALS OF FOUR (4) FEET.  
 4) ALL JOINTS SHALL BE CONSTRUCTED AT RIGHT ANGLES TO THE CENTERLINE OF THE WALK.  
 5) ALL EDGES AND JOINTS SHALL BE PROPERLY TOOLED.  
 6) THE SURFACE SHALL BE FINISHED WITH A WOOD FLOOT OR BRUSH TO PROVIDE A NON-SLIP SURFACE.  
 7) CONCRETE SHALL BE CURED IN A MANNER APPROVED BY THE OWNER.  
 8) WALK WITHIN DRIVEWAY AREAS SHALL BE 6" THICK.



- NOTES:  
 1) LARGE ROCKS, BOULDERS, AND STONES OVER ONE (1.0) INCH IN DIAMETER SHALL BE REMOVED TO PROVIDE A CLEARANCE OF SIX (6) INCHES (MIN.) FROM ANY PART OF THE PIPE.  
 2) ALL EMBEDMENT AND BACKFILL MATERIALS SHALL BE FREE FROM FROZEN EARTH, ICE, SNOW, TOPSOIL, MUCK, SANDERS, ASHES, REFUSE, ORGANIC MATTER, BOULDERS, ROCKS, OR OTHER MATERIALS WHICH, IN THE OPINION OF THE ENGINEER, ARE UNSUITABLE.  
 3) EMBEDMENT MATERIAL SHALL BE GRANULAR MATERIAL WITH MAXIMUM PARTICLE SIZE OF 1.0" COMPACTED TO 95% OF MAXIMUM UNIT WEIGHT AS DETERMINED BY THE MODIFIED PROCTOR METHOD.  
 4) BACKFILL MATERIAL UNDER PAVEMENT AREAS AND AREAS WITHIN TRENCH INFLUENCE OF BACK OF CURB OR EDGE OF PAVEMENT SHALL BE GRANULAR MATERIAL COMPACTED TO 95% OF MAXIMUM UNIT WEIGHT AS DETERMINED BY THE MODIFIED PROCTOR METHOD.  
 5) BACKFILL MATERIAL FOR PIPE TRENCHES NOT UNDER PAVEMENT AREAS AND AREAS OUTSIDE OF TRENCH INFLUENCE MAY BE NONGRANULAR NATIVE MATERIAL WHICH, IN THE OPINION OF THE ENGINEER, IS ACCEPTABLE MATERIAL AND IS ADEQUATELY COMPACTED TO PREVENT SETTLEMENT.  
 6) BACKFILL MATERIAL MAY BE PLACED, SPREAD, AND COMPACTED WITH EQUIPMENT OPERATED LONGITUDINALLY ALONG THE PIPE LINE. PUSHING MATERIAL INTO THE TRENCH FROM THE SIDE WITHOUT LONGITUDINAL SPREADING AND COMPACTION SHALL NOT BE ALLOWED.  
 7) SEE RELATED CITY OF PORTAGE STANDARD DETAIL:  
 SD-122, STANDARD TRENCH DETAIL

CONSTRUCTION DETAILS & NOTES  
**OAKLAND COMMONS**  
 PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN

REVISOR: 4/09/2024  
 REVISOR: 4/03/2024  
 REVISOR: 2/28/2024

DATE: 11/28/2023  
 SHEET: 7  
 JOB No.: 23-039

MONUMENT ENGINEERING GROUP ASSOC., INC.  
 Formerly Ingersoll, Watson & McMachen, Inc.  
 1209 East Milham Road, Suite B • Portage, Michigan 49002 • Phone 269 344-6165



**APPLICABLE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES**  
(COMPREHENSIVE DETAILS ARE LOCATED IN SECTION 6 OF THE SOIL EROSION & SEDIMENTATION CONTROL MANUAL)

A = SLOPES  
B = STREAMS AND WATERWAYS  
C = SURFACE DRAINAGEWAYS  
D = ENCLOSED DRAINAGE (INLET & OUTFALL CONTROL)  
E = LARGE FLAT SURFACE AREAS  
F = BORROW AND STOCKPILE AREAS  
G = DNRE PERMIT MAY BE REQUIRED

KEY	DETAIL	CHARACTERISTICS	A	B	C	D	E	F	G
1	TURBIDITY CURTAIN	A Turbidity Curtain is used when slack water area is necessary to isolate construction activities from the watercourse. The still water area contains the sediments within the construction limits.							
2	GRUBBING OMITTED	Retains existing root mat which assists in stabilizing slopes. Assists in the revegetation process by providing sprout growth. Reduces sheet flow velocities preventing rilling and gullying. Discourages off-road vehicle use.							
3	PERMANENT/TEMPORARY SEEDING	Inexpensive but effective erosion control measure to stabilize flat areas and mild slopes. Permits runoff to infiltrate soil, reducing runoff volumes. Proper preparation of the seed bed, fertilizing, mulching and watering is critical to its success.							
4	DUST CONTROL	Dust control can be accomplished by watering, and/or applying calcium chloride. The disturbed areas should be kept to a minimum. PERMANENT/TEMPORARY SEEDING (KEY 3) should be applied as soon as possible.							
5	SODDING	Provides immediate vegetative cover such as at spillways and ditch bottoms. Proper preparation of the topsoil, placement of the sod, and watering is critical to its success.							
6	VEGETATED BUFFER STRIPS	Reduces sheet flow velocities preventing rilling and gullying. Assists in the collection of sediments by filtering runoff. Assists in the establishment of a permanent vegetative cover.							

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR  
**SOIL EROSION & SEDIMENTATION CONTROL MEASURES**

9-10-2010 F.T.E.L. APPROVAL 6-3-2010 PLAN DATE R-96-E SHEET 1 OF 6

KEY	DETAIL	CHARACTERISTICS	A	B	C	D	E	F	G
7	RIPRAP	Used where vegetation cannot be established. Very effective in protecting against high velocity flows. Should be placed over a protective liner.							
8	AGGREGATE COVER	Can be used in any area where a stable condition is needed for construction operations, equipment storage or in heavy traffic areas. Reduces potential soil erosion and fugitive dust by stabilizing raw areas.							
9	BENCHES	Reduces sheet flow velocities preventing rilling and gullying. Assists in the collection and filtering of sediments. Provides access for stabilizing slopes.							
10	DIVERSION DIKE	Assists in the diversion of runoff to a stable outlet or sediment control device. Reduces sheet flow velocities preventing rilling and gullying. Collects and diverts runoff to properly stabilized drainage ways. Works well with INTERCEPTING DITCH (KEY 11)							
11	INTERCEPTING DITCH	Assists in the diversion of runoff to a stable outlet or sediment control device. Reduces sheet flow velocities preventing rilling and gullying. Works well with DIVERSION DIKE (KEY 10)							
12	INTERCEPTING DITCH AND DIVERSION DIKE	Assists in the diversion of runoff to a stable outlet or sediment control device. Reduces sheet flow velocities preventing rilling and gullying.							
13	GRAVEL FILTER BERM	Useful in filtering flow prior to its reentry into a lake, stream or wetland. Works well with SEDIMENT TRAP (KEY 20) and TEMPORARY BYPASS CHANNEL (KEY 35). Not to be used in lieu of a CHECK DAM (KEY 37) in a ditch.							
14	GRAVEL ACCESS APPROACH	Provides a stable access to roadways minimizing fugitive dust and tracking of materials onto public streets and highways.							

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR  
**SOIL EROSION & SEDIMENTATION CONTROL MEASURES**

9-10-2010 F.T.E.L. APPROVAL 6-3-2010 PLAN DATE R-96-E SHEET 2 OF 6

KEY	DETAIL	CHARACTERISTICS	A	B	C	D	E	F	G
15	SLOPE DRAIN SURFACE	Excellent device for carrying water down slopes without creating an erosive condition. Generally used in conjunction with DIVERSION DIKE (KEY 10), INTERCEPTING DITCH (KEY 11) and INTERCEPTING DITCH AND DIVERSION DIKE (KEY 12) to direct flow to a stable discharge area or SEDIMENT TRAP (KEY 20).							
16	TREES, SHRUBS AND PERENNIALS	Trees, shrubs and perennials can provide low maintenance long term erosion protection. These plants may be particularly useful where site aesthetics are important along the roadside slopes.							
17	PIPE DROP	Effective way to allow water to drop in elevation very rapidly without causing an erosive condition. Generally used in conjunction with a sediment collector device. May be left in place as a permanent erosion control device.							
18	DEWATERING WITH FILTER BAG	It may be necessary to dewater from behind a cofferdam or construction dam to create a dry work site. Discharged water must be pumped to a filter bag. A GRAVEL FILTER BERM (KEY 13) may be placed downslope of the filter bag to provide additional filtration prior to entering any stream or wetland.							
19	ENERGY DISSIPATORS	A device to prevent the erosive force of water from eroding soils. Used at outlets of culverts, drainage pipes or other conduits to reduce the velocity of the water. Prevents structure scouring and undermining.							
20	SEDIMENT TRAP	Used to intercept concentrated flows and prevent sediments from being transported off site or into a watercourse or wetland. The size of a Sediment Trap is 5 cubic yards or less. Works well when used with CHECK DAM (KEY 37).							
21	SEDIMENT BASIN	A Sediment Basin is used to trap sediments from an upstream construction site. Requires periodic inspections, repairs, and maintenance. Where practical, sediments should be contained on site. A Sediment Basin should be the last choice of sediment control. The size of a Sediment Basin is greater than 5 cubic yards.							
22	VEGETATIVE BUFFER AT WATERCOURSE	This practice is used to maintain a vegetative buffer adjacent to a watercourse. When utilized with SILT FENCE (KEY 26) it will, under normal circumstances, prevent sediment from leaving the construction site.							

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR  
**SOIL EROSION & SEDIMENTATION CONTROL MEASURES**

9-10-2010 F.T.E.L. APPROVAL 6-3-2010 PLAN DATE R-96-E SHEET 3 OF 6

KEY	DETAIL	CHARACTERISTICS	A	B	C	D	E	F	G
23	STREAM RELOCATION	A detail depicting the proper procedures for stream relocation. Maintains same width, depth, and flow velocity as the natural stream. Revegetate banks with PERMANENT/TEMPORARY SEEDING (KEY 3), MULCHING AND MULCH ANCHORING (KEY 28), MULCH BLANKETS AND HIGH VELOCITY MULCH BLANKETS (KEY 33) and woody plants to shade the stream.							
24	SAND AND STONE BAGS	Sand and stone bags are a useful tool in the prevention of erosion. Can be used to divert water around a construction site by creating a DIVERSION DIKE (KEY 10). Works well for creating a CONSTRUCTION DAM (KEY 36) and temporary culvert end fill.							
25	SAND FENCE AND DUNE STABILIZATION	A Sand Fence traps blowing sand by reducing wind velocities. Can be used to prevent sand from blowing onto roads. Must be maintained until sand source is stabilized.							
26	SILT FENCE	A permeable barrier erected behind disturbed areas to capture sediments from sheet flow. Can be used to divert small volumes of water to stable outlets. Ineffective as a filter and should never be placed across streams or ditches where flow is concentrated.							
27	PLASTIC SHEETS OR GEOTEXTILE COVER	Plastic Sheets can be used to create a liner in temporary channels. Can also be used to create a temporary cover to prevent erosion of stockpiled materials.							
28	MULCHING AND MULCH ANCHORING	Anchored mulch provides erosion protection against rain and wind. Mulch must be used on seeded areas to promote water retention and growth. Should be inspected after every rainstorm and repaired as necessary until vegetation is well established.							
29	INLET PROTECTION FABRIC DROP	Provides settling and filtering of silt laden water prior to its entry into the drainage system. Can be used in median and side ditches where vegetation will be disturbed. Allows for early use of drainage systems prior to project completion.							
30	INLET PROTECTION GEOTEXTILE AND STONE	Provides settling and filtering of silt laden water prior to its entry into the drainage system. Should be used in paved areas where drainage structures are existing or proposed. Allows for early use of drainage systems prior to project completion.							

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR  
**SOIL EROSION & SEDIMENTATION CONTROL MEASURES**

9-10-2010 F.T.E.L. APPROVAL 6-3-2010 PLAN DATE R-96-E SHEET 4 OF 6

KEY	DETAIL	CHARACTERISTICS	A	B	C	D	E	F	G
31	INLET PROTECTION SEDIMENT TRAP	An Inlet Protection Sediment Trap is a temporary device that can be used in areas where medium flows are anticipated. Effective in trapping small quantities of sediments prior to water entering the drainage system. Can be used in areas such as median and side ditches.							
32	SLOPE ROUGHENING AND SCARIFICATION	A simple and economical way to reduce soil erosion by wind and water. Can be accomplished by harrowing with a disk, back blading, or tracking with a dozer perpendicular to the slope.							
33	MULCH BLANKETS AND HIGH VELOCITY MULCH BLANKETS	Mulch blankets provide an immediate and effective cover over raw erodible slopes affording excellent protection against rain and wind erosion. High velocity mulch blankets work well for stabilizing the bottom of ditches in waterways.							
34	COFFERDAM	Used to create a dry construction area and protect the stream from raw erodible areas. Must be pumped dry or dewatered according to DEWATERING WITH FILTER BAG (KEY 18).							
35	TEMPORARY BYPASS CHANNEL	Utilized when a dry construction area is needed. Isolates stream flows from raw erodible areas minimizing erosion and subsequent siltation. Can incorporate SEDIMENT BASIN (KEY 21), CHECK DAM (KEY 37), and GRAVEL FILTER BERM (KEY 13) to remove sediments from water. Construction sequence of events may be necessary.							
36	CONSTRUCTION DAM	Used to create a dry or slack water area for construction. Isolates the stream from raw erodible areas. Can be created out of any non-erodible materials such as SAND AND STONE BAGS (KEY 24), a gravel dike with clay core or plastic liner, steel plates or plywood.							
37	CHECK DAM	Can be constructed across ditches or any area of concentrated flow. Protects vegetation in early stages of growth. A Check Dam is intended to reduce water velocities and capture sediment. A Check Dam is not a filtering device.							

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR  
**SOIL EROSION & SEDIMENTATION CONTROL MEASURES**

9-10-2010 F.T.E.L. APPROVAL 6-3-2010 PLAN DATE R-96-E SHEET 5 OF 6

KEY	DETAIL	CHARACTERISTICS	A	B	C	D	E	F	G
38	SILT FENCE	A permeable barrier erected behind disturbed areas to capture sediments from sheet flow. Can be used to divert small volumes of water to stable outlets. Ineffective as a filter and should never be placed across streams or ditches where flow is concentrated.							
39	VEGETATIVE BUFFER AT WATERCOURSE	This practice is used to maintain a vegetative buffer adjacent to a watercourse. When utilized with SILT FENCE (KEY 26) it will, under normal circumstances, prevent sediment from leaving the construction site.							

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR  
**SOIL EROSION & SEDIMENTATION CONTROL MEASURES**

9-10-2010 F.T.E.L. APPROVAL 6-3-2010 PLAN DATE R-96-E SHEET 6 OF 6

NOTES:  
THIS STANDARD PLAN WILL SERVE AS A KEY IN THE SELECTION OF THE APPROPRIATE SOIL EROSION AND SEDIMENTATION CONTROL DETAILS. THIS PLAN ALSO PROVIDES THE KEY TO THE NAMED EROSION CONTROL ITEMS SPECIFIED ON THE CONSTRUCTION PLANS. REFER TO THE MOST SOIL EROSION & SEDIMENTATION CONTROL MANUAL, SECTION 6 FOR SPECIFIC DETAILS, CONTRACT ITEMS (PAY ITEMS), AND PAY UNITS.  
COLLECTED SILT AND SEDIMENT SHALL BE REMOVED PERIODICALLY TO MAINTAIN THE EFFECTIVENESS OF THE SEDIMENT TRAP, SEDIMENT BASIN, AND SILT FENCE. AGGREGATES PLACED IN STREAMS SHOULD CONTAIN A MINIMUM OF FINESS.  
TEMPORARY EROSION AND SEDIMENTATION CONTROL PROVISIONS SHALL BE COORDINATED WITH THE PERMANENT CONTROL MEASURES TO ASSURE EFFECTIVE CONTROL OF SEDIMENTS DURING CONSTRUCTION OF THE PROJECT.  
ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE REMOVED AFTER VEGETATION ESTABLISHMENT OR AT THE DISCRETION OF THE ENGINEER. CARE SHALL BE TAKEN DURING REMOVAL TO MINIMIZE SILTATION IN NEARBY DRAINAGE COURSES.

**NOTE: ALL SESC MEASURES AND PROVISIONS SHALL COMPLY WITH DETAILS ON THIS SHEET.**

EROSION CONTROL DETAILS  
**OAKLAND COMMONS**  
PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN

MONUMENT ENGINEERING GROUP ASSOC., INC.  
Formerly Ingersoll, Watson & McEachen, Inc.  
1209 East Milham Road, Suite B - Portage, Michigan 49002 • Phone 269 344-6165

REVISED: 4/03/2024  
DATE: 11/28/2023  
SHEET: 8  
JOB No.: 23-039



NOTE: ALL SIGNS & PANELS SHALL BE IN PLACE WHILE THE CONTRACTOR IS WORKING IN THE ROAD RIGHT-OF-WAY.

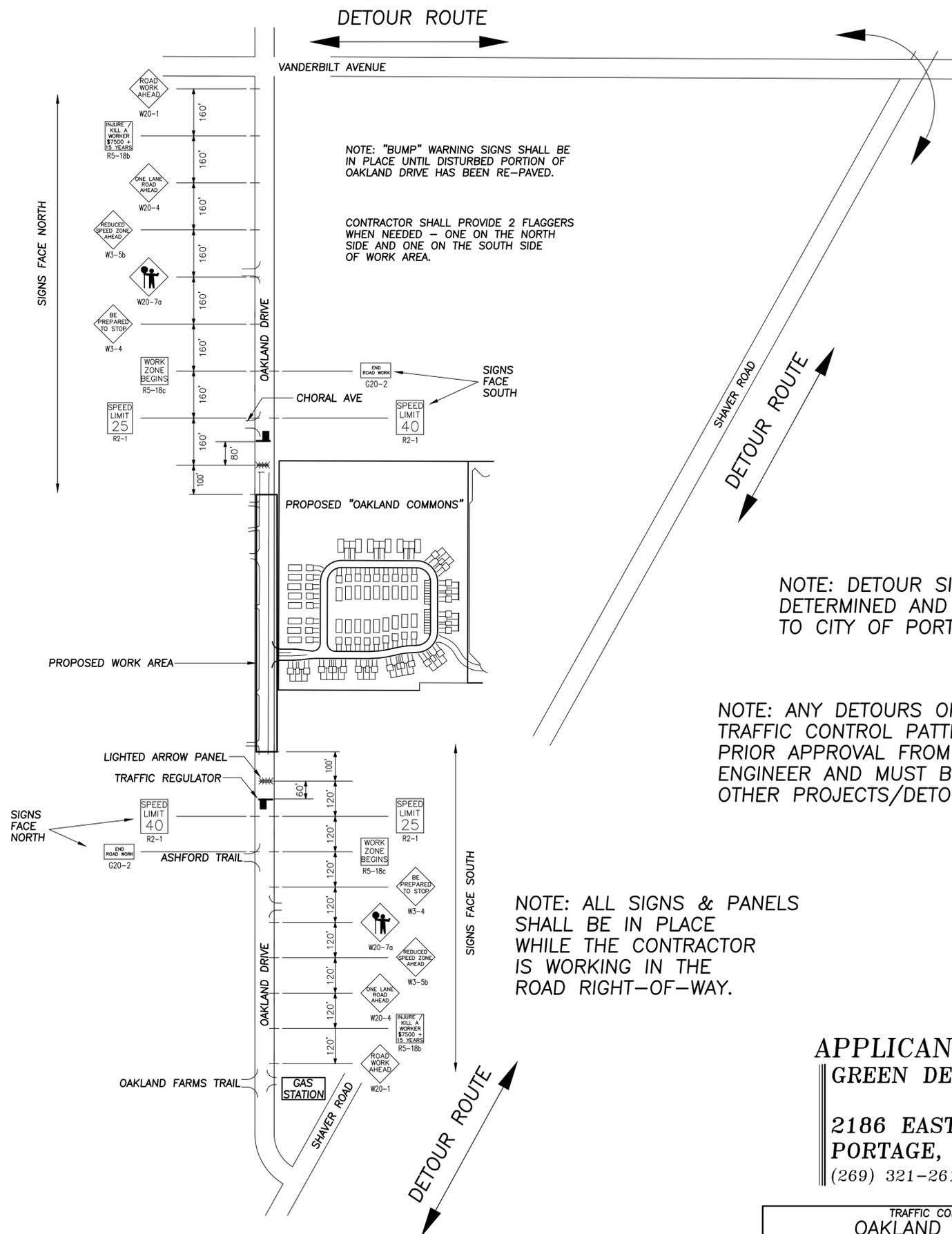
NOTE: POSTED SPEED LIMIT ON OAKLAND DRIVE IS 40 M.P.H.

NOTE: ONE LANE SHALL REMAIN OPEN FOR TRAFFIC AT ALL TIMES, EXCEPT FOR SANITARY SEWER INSTALLATION. TWO LANES SHALL REMAIN OPEN OVERNIGHT. FOR SANITARY SEWER INSTALLATION, OAKLAND DRIVE MAY BE CLOSED BETWEEN 9:00 AM AND 8:00 PM UNLESS THE CITY DICTATES OTHERWISE.

NOTE: REFERENCE MDOT TRANSPORTATION STANDARD 110-TR-NFW-2L FOR ADDITIONAL DETAILS.

**NOTES:**

- 1) OAKLAND DRIVE IS UNDER THE JURISDICTION OF THE CITY OF PORTAGE.
- 2) ALL ROADWAY CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE CITY OF PORTAGE STANDARD SPECIFICATIONS AND THE 2020 MDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION.
- 3) PERMIT(S) SHALL BE OBTAINED FROM THE CITY AS REQUIRED.
- 4) LANE CLOSURES WILL BE PULLED BACK WHEN WORK IS NOT BEING PERFORMED IN ORDER TO MAINTAIN TWO-WAY TRAFFIC. TRAFFIC MUST BE ON AN ADEQUATE SURFACE AT ALL TIMES.
- 5) TWO FLAGGERS SHALL BE PROVIDED WHEN OAKLAND DRIVE HAS ONLY ONE LANE OPEN.
- 6) ALL PAVEMENT MARKINGS AND SIGNAGE SHALL COMPLY WITH THE CURRENT EDITION OF THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.



NOTE: DETOUR SIGNAGE IS YET TO BE DETERMINED AND SHALL BE SUBJECT TO CITY OF PORTAGE APPROVAL.

NOTE: ANY DETOURS OR CHANGES IN TRAFFIC CONTROL PATTERNS WILL REQUIRE PRIOR APPROVAL FROM THE CITY TRAFFIC ENGINEER AND MUST BE COORDINATED WITH OTHER PROJECTS/DETOURS IN THE AREA.

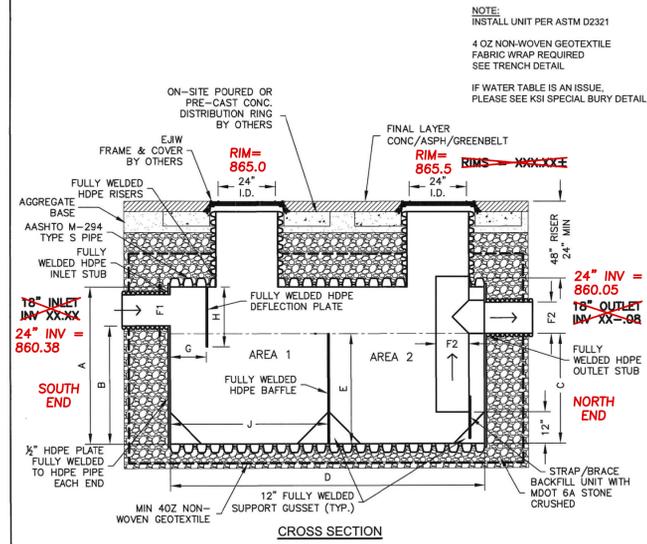
NOTE: ALL SIGNS & PANELS SHALL BE IN PLACE WHILE THE CONTRACTOR IS WORKING IN THE ROAD RIGHT-OF-WAY.

**APPLICANT:**  
**GREEN DEVELOPMENT VENTURES, LLC**  
 2186 EAST CENTRE STREET  
 PORTAGE, MI 29002  
 (269) 321-2610

TRAFFIC CONTROL PLAN <b>OAKLAND COMMONS</b>		DATE: <b>2/28/2024</b>
<small>PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN</small>		SHEET: <b>9</b>
<b>MONUMENT ENGINEERING GROUP ASSOC., INC.</b> <small>Formerly Ingersoll, Watson &amp; McMachen, Inc.</small>		JOB No.: <b>23-039</b>
<small>1209 East Milham Road, Suite B - Portage, Michigan 49002 - Phone 269 344-6165</small>		

REVISED: 4/9/2024  
REVISED: 4/3/2024

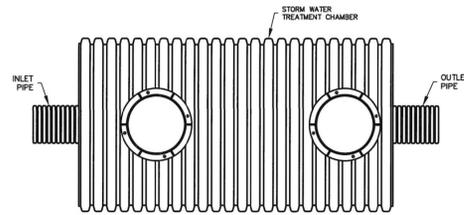
## STORM WATER TREATMENT CHAMBER KSI SERIES 3000 HDPE CHAMBER



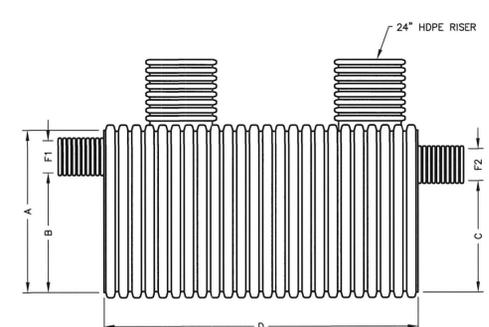
STORM WATER TREATMENT CHAMBER DIMENSIONS AND CAPACITIES

MODEL NO.	A	B	C	D	E	F1	F2	G	H	J	AREA 1	AREA 2
3000-60-18	60"	39"	38"	298"	38"	18"	18"	18"	24"	149"	162 CF	274 CF

DESIGN BY: **AG / RK** REV. 10-21-22 DATE: 10-10-08 **2 OF 2**  
 MANUF. APPROVAL BY: SCALE: **NTS** SHEET NO. **KSI** KENNEDY SOLUTIONS, INC.  
 10051 Bergh Road Howell, MI 48843 Ph: 800-899-4046



PLAN VIEW



SIDE VIEW

DESIGN BY: **AG / RK** REV. 10-21-22 DATE: 10-10-08 **1 OF 2**  
 MANUF. APPROVAL BY: SCALE: **NTS** SHEET NO. **KSI** KENNEDY SOLUTIONS, INC.  
 10051 Bergh Road Howell, MI 48843 Ph: 800-899-4046

### KSI Storm Water Treatment Sizing Chart

KSI Model	Wayne County 75 Micron	75 Micron	110 Micron
350	1.8 cfs	2.2 cfs	2.3 cfs
500	2.3 cfs	2.9 cfs	3.0 cfs
750	3.3 cfs	4.1 cfs	4.2 cfs
1000	4.1 cfs	5.1 cfs	5.3 cfs
1250	5.0 cfs	6.3 cfs	6.5 cfs
1500	6.2 cfs	7.8 cfs	8.1 cfs
1750	7.4 cfs	9.2 cfs	9.5 cfs
2000	8.3 cfs	10.4 cfs	10.7 cfs
2500	9.8 cfs	12.3 cfs	12.6 cfs
<b>3000</b>	<b>11.3 cfs</b>	<b>14.1 cfs</b>	<b>14.5 cfs</b>
3500	13.8 cfs	17.2 cfs	17.7 cfs
3750	16.3 cfs	20.4 cfs	21.0 cfs
4000	18.8 cfs	23.5 cfs	24.3 cfs
4500	20.0 cfs	25.2 cfs	25.8 cfs

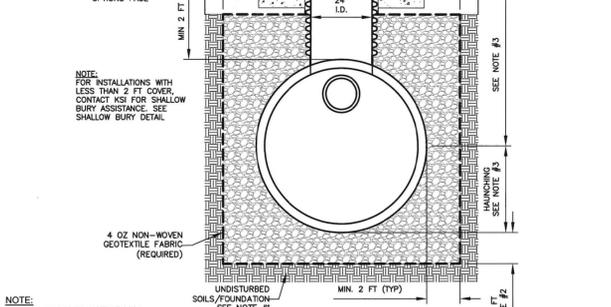
### STORM WATER TREATMENT CHAMBER TRENCH DETAIL

- NOTES:**
- FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH A FOUNDATION OF MDOT 6A CRUSHED STONE AS DEFINED IN ASTM D2321, LATEST EDITION; AS AN ALTERNATIVE TRENCH BOTTOM MAY BE STABILIZED USING A WOVEN GEOTEXTILE FABRIC AND OR A GEOGRID.
  - BEDDING: SUITABLE MATERIAL SHALL BE MDOT 6A CRUSHED STONE AS REQUIRED IN ASTM D2321, LATEST EDITION, UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, MINIMUM BEDDING THICKNESS SHALL BE 12" FOR 36"-120" SWTC.
  - HAUNCHING AND INITIAL BACKFILL: SUITABLE MATERIAL SHALL BE MDOT 6A CRUSHED STONE AND INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION, MAXIMUM TEN INCH (10") LIFTS.
  - UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, MIN. TRENCH WIDTHS ARE AS FOLLOWS.

NOMINAL I.D.	NOMINAL O.D.	MINIMUM RECOMM. TRENCH WIDTH
36"	42"	90"
48"	54"	102"
60"	66"	114"
72"	80.5"	129"
96"	105.9"	154"
120"	131.3"	190"

- MINIMUM COVER: MINIMUM RECOMMENDED COVER FOR VARIOUS LIVE LOADING CONDITIONS ARE AS SUMMARIZED IN THE FOLLOWING TABLE. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TAKEN FROM THE TOP OF THE SWTC TANK TO THE GROUND SURFACE.

SURFACE LIVE LOAD	MINIMUM REQUIRED COVER FOR WAYNE COUNTY (ALL SIZES)
HS-25/H25 (FLEXIBLE PAVEMENT)	MIN. 24" AS MEASURED FROM THE BOTTOM OF THE FLEXIBLE PAVEMENT
HS-25/H25 (RIGID PAVEMENT)	MIN. 24" AS MEASURED FROM THE TOP OF THE RIGID PAVEMENT
E80 RAILWAY	24"
HEAVY CONSTRUCTION	48"



DESIGN BY: **AG / RK** REV. 11-17-23 DATE: 10-10-08 **1 OF 1**  
 MANUF. APPROVAL BY: SCALE: **NTS** SHEET NO. **KSI** KENNEDY SOLUTIONS, INC.  
 10051 Bergh Road Howell, MI 48843 Ph: 800-899-4046

### STORM WATER TREATMENT CHAMBER INSTALLATION AND MAINTENANCE GUIDELINES

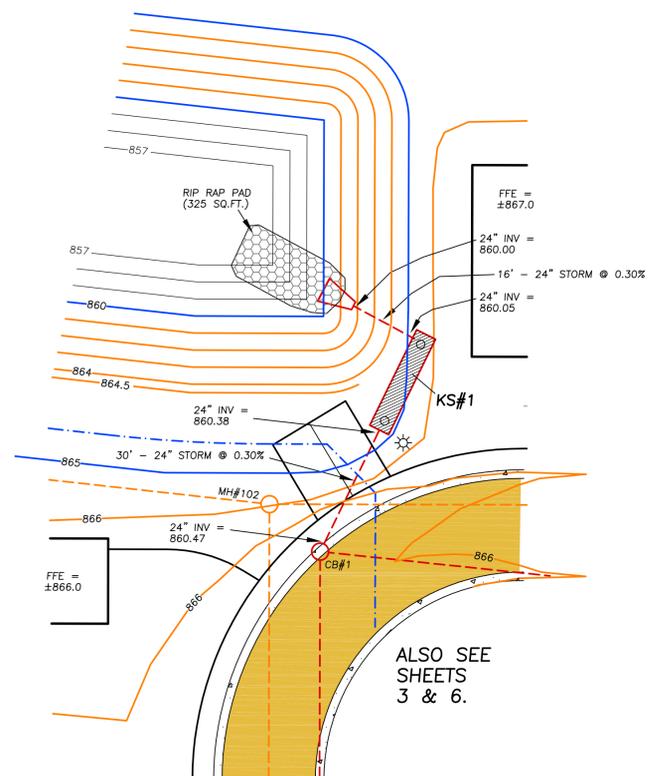
#### INSTALLATION GUIDELINES

- EXCAVATE AREA FOR KSI SWTC AND PREPARE TRENCH BOTTOM PER ASTM D2321, SECTIONS 6 & 7.
- THE KSI SWTC SHALL BE INSTALLED ON A BED OF NO LESS THAN 12" MDOT 6A CRUSHED STONE MATERIALS COMPACTED TO 95% PROCTOR DENSITY. COMPACTED TO 95% (90% MIN. FOR MDOT 6A CRUSHED STONE MATERIAL) OF THE BACKFILL MATERIAL'S MAXIMUM WEIGHT AT A MOISTURE CONTENT NOT GREATER THAN THE OPTIMUM.
- INSTALL KSI SWTC UNIT. HIGH FLOW BYPASS LINE (IF APPLICABLE), DIVERSION STRUCTURE AND EXITING STRUCTURE AT ELEVATIONS INDICATED ON SITE PLAN. COUPLE INLET AND OUTLET HDPE SPLIT COUPLERS TO CONVEYANCE PIPE.
- BACKFILL UNIT WITH MDOT 6A CRUSHED STONE MATERIALS PER ASTM D2321. BACKFILL SHALL BE COMPACTED TO 95% PROCTOR DENSITY.
- THE HDPE ACCESS RISERS SHALL BE FIELD CUT TO FINISH GRADE BY THE CONTRACTOR. SEE RISER INSTALLATION OPTIONS PAGE.
- KSI RECOMMENDS FILLING THE UNIT WITH WATER UPON COMPLETION OF INSTALLATION UP TO THE BAFFLE HEIGHT.
- ONCE THE UNIT IS INSTALLED, PLACE A ORANGE SAFETY FENCE 4-5 FT HIGH WITH TEE POST, AROUND THE SYSTEM. PLACE FENCE 5 FT BEYOND GRID FOOTPRINT. ANY DAMAGE TO THE SYSTEM AS A RESULT OF NOT FOLLOWING THESE INSTRUCTIONS AND THE BLUEPRINT DETAILS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR THE SYSTEM TO KSI'S SATISFACTION. IF GIVEN IN WRITING, A 3-4 DAY LEAD TIME, KSI WILL HAVE A REPRESENTATIVE AVAILABLE ON SITE DURING THE INSTALLATION.

#### MAINTENANCE GUIDELINES

- ALL STORM WATER TREATMENT CHAMBERS WILL REQUIRE PERIODIC MAINTENANCE DEPENDING ON SPECIFIC SITE CONDITIONS.
  - KSI RECOMMENDS CLEANING THE SWTC QUARTERLY AND AFTER HEAVY RAIN STORMS. SEDIMENT IS EASIER TO REMOVE WHEN IT IS REMOVED ON A REGULAR BASIS.
  - DISPOSAL OF MATERIAL FROM THE KSI SWTC ARE SIMILAR TO THAT OF ANY OTHER BEST MANAGEMENT PRACTICES (BMP). LOCAL GUIDELINES SHOULD BE CONSULTED PRIOR TO DISPOSAL OF THE SWTC CONTENTS. PETROLEUM WASTE PRODUCTS SHOULD BE REMOVED BY A LICENSED WASTE MANAGEMENT COMPANY.
  - IF A HYDROCARBON REMOVAL SYSTEM WAS INSTALLED - REPLACE IT WHEN IT TURNS BLACK. UNIT CAN BE DISPOSED OF VIA NORMAL REFUSE REMOVAL. SPENT UNIT DOES NOT LEACH CAPTURED CONTAMINATES.
- WHITE = NEW  
 GRAY = WORKING  
 BLACK = SPENT - NEEDS REPLACEMENT - CONTACT KSI FOR REPLACEMENT PARTS
- AFTER CLEANING THE UNIT - KSI RECOMMENDS REFILLING THE UNIT WITH WATER.

DESIGN BY: **AG / RK** REV. 4-1-22 DATE: 10-10-08 **1 OF 1**  
 MANUF. APPROVAL BY: SCALE: **NTS** SHEET NO. **KSI** KENNEDY SOLUTIONS, INC.  
 10051 Bergh Road Howell, MI 48843 Ph: 800-899-4046



KSI STORM WATER TREATMENT DETAIL  
SCALE: 1" = 20'

STORM WATER TREATMENT DEVICE DETAIL <b>OAKLAND COMMONS</b> PART OF THE SE. 1/4 OF SEC. 29, T. 3 S., R. 11 W., CITY OF PORTAGE, MICHIGAN		DATE: <b>4/08/2024</b>
<b>MONUMENT ENGINEERING GROUP ASSOC., INC.</b> Formerly Ingersoll, Watson & McMachen, Inc. 1209 East Milham Road, Suite B • Portage, Michigan 49002 • Phone 269 344-6165		SHEET: <b>10</b>
		JOB No.: <b>23-039</b>

**LEGEND**

- EXISTING PUBLIC SANITARY SEWER
- - - PROPOSED 8" PUBLIC SANITARY SEWER OR PRIVATE 6" SEWER LEAD
- EXISTING PRIVATE STORM SEWER
- - - PROPOSED PRIVATE STORM SEWER
- EXISTING PUBLIC WATER MAIN
- - - PROPOSED 8" PUBLIC WATER MAIN OR PRIVATE 1.25" COPPER WATER SERVICE
- PROPOSED 4.0' WIDE CONCRETE SIDEWALK
- PROPOSED 2.0' WIDE CONCRETE ROLL CURB
- BUILDING SETBACK LINE
- EDGE OF EASEMENT
- PROPOSED FIRE HYDRANT ASSEMBLY (8" x 6" TEE, 6" GATE VALVE, & HYD.)
- \* SUGGESTED LOCATION OF PROPOSED STREET LIGHT

**SOIL/GROUNDWATER CONTAMINATION NOTES:**

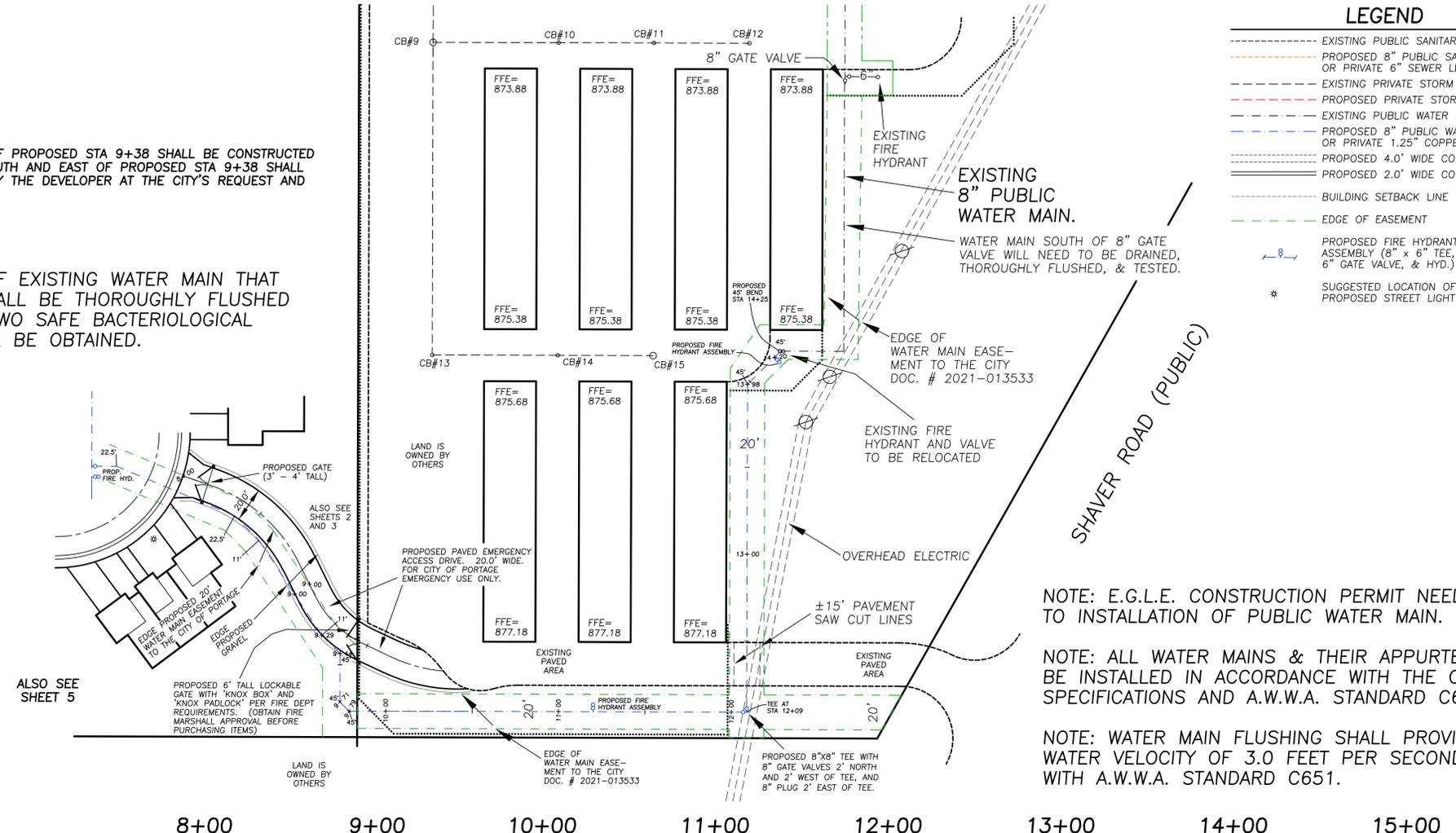
- IF SOIL AND/OR GROUNDWATER CONTAMINATION IS ENCOUNTERED OR SUSPECTED DURING CONSTRUCTION THE FOLLOWING STEPS SHALL BE FOLLOWED:
- 1) CONTRACTOR SHALL STOP WORK IMMEDIATELY AND NOTIFY OWNER AND/OR OWNER'S REPRESENTATIVE.
  - 2) OWNER AND/OR OWNER'S REPRESENTATIVE SHALL NOTIFY THE CITY.
  - 3) OWNER SHALL ENGAGE THE SERVICES OF A QUALIFIED ENVIRONMENTAL CONSULTANT.
  - 4) CONSULTANT SHALL CONDUCT A SITE ASSESSMENT AND REPORT TO THE ABOVE PARTIES.
  - 5) CONSULTANT SHALL DEVELOP A PLAN TO ADDRESS ANY CONTAMINATION WHICH MAY BE FOUND DURING CONSTRUCTION.
  - 6) THE CONSULTANT TOGETHER WITH THE DESIGN ENGINEER SHALL COMPLETE A SITE EVALUATION PROCESS FOLLOWING THE DOCUMENT ENTITLED IMPLEMENTING STORMWATER INFILTRATION PRACTICES AT VACANT PARCELS AND BROWNFIELD SITES (EPA, 2013) TO DETERMINE IF THE STORMWATER MANAGEMENT SYSTEM REQUIRES REDESIGN AND RESUBMITAL TO THE CITY FOR REVIEW AND APPROVAL.
  - 7) OWNER AND/OR OWNER'S REPRESENTATIVE SHALL SUBMIT TO THE CITY SUPPORTING DOCUMENTATION OF THE SITE EVALUATION PROCESS. A REVISED STORMWATER REVIEW PACKAGE MUST ALSO BE SUBMITTED IF IT BECOMES NECESSARY TO CHANGE THE DESIGN OF THE STORMWATER MANAGEMENT SYSTEM.
  - 8) MICHIGAN DEPARTMENT OF ENVIRONMENT, GREAT LAKES, AND ENERGY (EGLE) REMEDIATION OF REDEVELOPMENT DIVISION (RRD) STAFF SHALL BE NOTIFIED AS APPLICABLE.

**GENERAL NOTES:**

- 1) LIGHT POLES AND ANY UTILITY POLE/BOX SHALL NOT BE LOCATED WITHIN THE PUBLIC SEWER & WATER EASEMENT.
- 2) STREET NAME SIGNS SHALL BE AT LEAST 9" FROM TOP TO BOTTOM OF ACTUAL SIGN (& PLACED ATOP STOP SIGNS).
- 3) ALL PAVEMENT MARKINGS AND ALL TRAFFIC SIGNAGE SHALL COMPLY WITH STANDARDS IN THE M.M.U.T.C.D. (CURRENT EDITION).
- 4) U-CHANNEL POSTS REQUIRED FOR ALL SIGNS.
- 5) SOIL EROSION & SEDIMENTATION CONTROL PERMIT SHALL BE OBTAINED AS REQUIRED BY STATE LAW.
- 6) PERMIT(S) SHALL BE OBTAINED FROM THE CITY OF PORTAGE AS REQUIRED.
- 7) E.G.L.E. CONSTRUCTION PERMITS REQUIRED (FOR BOTH PUBLIC WATER MAIN & PUBLIC SANITARY SEWER) PRIOR TO THE COMMENCEMENT OF UTILITY INSTALLATION.
- 8) "NO PARKING THIS SIDE OF STREET" SIGNS SHALL BE INSTALLED ATOP THE FIRE HYDRANT SIGNS AND MIDWAY BETWEEN THE FIRE HYDRANTS ON THE HYDRANT SIDE OF THE STREETS.
- 9) FIVE INCH "STORZ" FITTINGS SHALL BE USED ON FIRE HYDRANTS.
- 10) FOR ON-SITE DWELLINGS, BASEMENT FOOTINGS AND FOUNDATIONS SHALL COMPLY WITH BUILDING CODE REQUIREMENTS BASED ON TYPE OF SOIL PRESENT AND STABILITY OF SOIL.
- 11) FOR ON-SITE DWELLINGS, REQUIRED RADON VENTING SYSTEM BELOW BASEMENT FLOOR SHALL, AS A PRECAUTION, BE PLUMBED TO ALSO BE A DRAINAGE SYSTEM DURING PERIODS OF HIGH GROUNDWATER.
- 12) ALL PROPOSED STREET LIGHTS SHALL COMPLY WITH CITY OF PORTAGE STANDARDS AND CONSUMERS ENERGY STANDARDS WITH REGARDS TO LOCATION AND LIGHT SPECIFICATION.
- 13) AS-BUILT CONSTRUCTION PLANS SHALL BE SUBMITTED TO THE CITY UPON COMPLETION OF THE ROADWAY PROJECT.
- 14) UNDERGROUND GAS, ELECTRIC, TELEPHONE AND CABLE TV LINES SHALL BE DESIGNED BY UTILITY OWNERS AND SHALL NOT BE LOCATED IN PROPOSED STREET RIGHT-OF-WAYS UNLESS UTILITY IS CROSSING STREET (SQUARE TO CENTER LINE).
- 15) FIRE HYDRANT SIGNS SHALL BE LOCATED 4.0 FEET FROM THE CENTER OF THE FIRE HYDRANT.
- 16) PRIOR TO START OF CONSTRUCTION, PRE-CONSTRUCTION MEETING SHALL BE HELD AT CITY OF PORTAGE OFFICES.
- 17) IF A FIRE HYDRANT EXTENSION IS USED TO RAISE A HYDRANT, THE EXTENSION SHALL BE PAINTED THE SAME COLOR OF THE HYDRANT PER CITY OF PORTAGE SPECIFICATIONS.
- 18) A "STORM WATER BEST MANAGEMENT PRACTICES OPERATIONS & MAINTENANCE AGREEMENT" SHALL BE COMPLETED AND PREPARED PRIOR TO OBTAINING CITY FINAL APPROVAL.

NOTE: PROPOSED WATER MAIN NORTHWEST OF PROPOSED STA 9+38 SHALL BE CONSTRUCTED BY THE DEVELOPER. WATER MAIN SOUTH AND EAST OF PROPOSED STA 9+38 SHALL BE CONSTRUCTED BY THE CITY (OR BY THE DEVELOPER AT THE CITY'S REQUEST AND THE CITY'S EXPENSE).

NOTE: ANY PORTION OF EXISTING WATER MAIN THAT IS DEPRESSURIZED SHALL BE THOROUGHLY FLUSHED AND, AT A MINIMUM, TWO SAFE BACTERIOLOGICAL WATER SAMPLES SHALL BE OBTAINED.



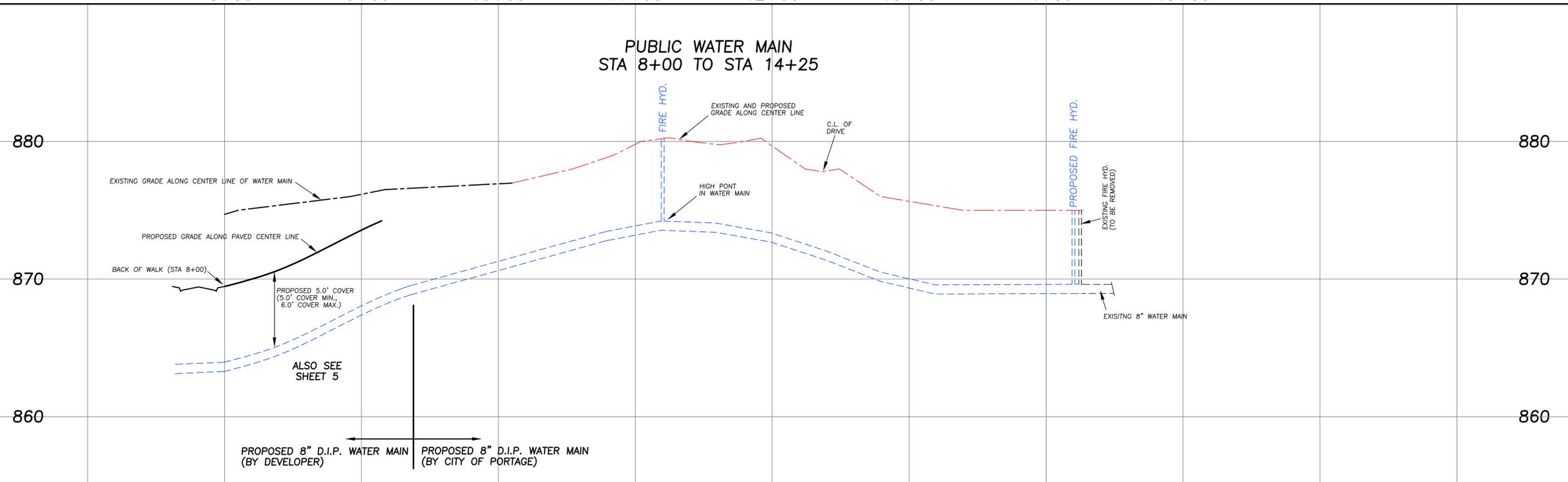
NOTE: E.G.L.E. CONSTRUCTION PERMIT NEEDED PRIOR TO INSTALLATION OF PUBLIC WATER MAIN.

NOTE: ALL WATER MAINS & THEIR APPURTENANCES SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY OF PORTAGE SPECIFICATIONS AND A.W.W.A. STANDARD C600.

NOTE: WATER MAIN FLUSHING SHALL PROVIDE A MINIMUM WATER VELOCITY OF 3.0 FEET PER SECOND IN ACCORDANCE WITH A.W.W.A. STANDARD C651.

SCALES:  
 PLAN VIEW: 1" = 40'  
 PROFILE: 1" = 40' HORIZ.  
 1" = 4' VERTICAL

**PUBLIC WATER MAIN  
 STA 8+00 TO STA 14+25**



# PROPOSED LANDSCAPE PLAN "OAKLAND COMMONS"



**PLANT SCHEDULE - ENTRANCE BEDS**

Figure	Qty	Botanical Name	Common Name	Size
<b>Evergreens</b>				
12	2	<i>Thuja occidentalis</i> Bobozerri	11R BOWLING BALL ARBORVITAE	#3
<b>Ornamental Grasses</b>				
12	2	<i>Miscanthus sinensis</i> Variegatus	VAREGATED SILVER GRASS	#1
<b>Perennials</b>				
12	8	<i>Artemisia</i> Silver Mound	SILVER MOUND ARTEMISIA	#1
12	12	<i>Echinacea purpurea</i> PawNaw Wild Berry	POW-NOW WILD BERRY CONEFLOWER	#1
14	14	<i>Nepeta x</i> Cats Pajamas	CATS PAJAMAS CATMINT	#1

**PLANT SCHEDULE - ROADSIDE**

Figure	Qty	Botanical Name	Common Name	Size
<b>Evergreens</b>				
6	6	<i>Juniperus scopulorum</i> Moonglow	MOONGLOW JUNIFER	6-8'
5	5	<i>Picea glauca</i> Democrat	BLACK HILLS SPRUCE	6-8'
<b>Trees</b>				
5	5	<i>Acer rubrum</i> October Glory	OCTOBER GLORY RED MAPLE	15-25'
		<i>Syringa reticulata</i> Ivory Silk	IVORY SILK LILAC TREE	15-25'

**"OAKLAND COMMONS"**  
GREEN DEVELOPMENT VENTURES

client:	date:	revision:
scale: 1"=20' / 24x36	18 NOV 2023	10 APRIL 2024
drawn by: SOFUL DESIGNS	checked by:	drawing #:



















# PROPOSED LANDSCAPE PLAN "OAKLAND COMMONS"



**PLANT SCHEDULE - ENTRANCE BEDS**

Figure	Qty	Botanical Name	Common Name	Size
<b>Evergreens</b>				
12	2	<i>Thuja occidentalis</i> Bobazari	11R BOWLING BALL ARBORVITAE	#3
<b>Ornamental Grasses</b>				
12	2	<i>Miscanthus sinensis</i> Variegatus	VAREGATED SILVER GRASS	#1
<b>Perennials</b>				
12	8	<i>Artemisia</i> Silver Mound	SILVER MOUND ARTEMISIA	#1
12	2	<i>Echinacea purpurea</i> PawNaw Wild Berry	POW-NOW WILD BERRY CONEFLOWER	#1
14	14	<i>Nepeta x</i> Cats Pajamas	CATS PAJAMAS CATMINT	#1

**PLANT SCHEDULE - ROADSIDE**

Figure	Qty	Botanical Name	Common Name	Size
<b>Evergreens</b>				
9	9	<i>Juniperus scopulorum</i> Moonglow	MOONGLOW JUNIPER	5-6'
2	2	<i>Picea glauca</i> Densata	BLACK HILLS SPRUCE	5-6'
<b>Trees</b>				
5	5	<i>Acer rubrum</i> October Glory	OCTOBER GLORY RED MAPLE	2-2 1/2'
5	5	<i>Syringa reticulata</i> Ivory Silk	IVORY SILK LILAC TREE	2-2 1/2'



1"=20'

**"OAKLAND COMMONS"**  
GREEN DEVELOPMENT VENTURES

client:	date:	revision:
scale: 1"=20' / 24x36	NOV 18 2023	6 OCT 2023
drawn by: SOFUL DESIGNS	checked by:	drawing #:

















# Staff's Site Pictures – August 9, 2023





Figure 1: Looking south along Oakland Drive. Subject site to the left (east) and driveways to multiple houses at 9590, 9530, 9528, 9526, 9524 Oakland Drive to the right (west).



Figure 2: Looking south along Oakland Drive. Subject site to the left (east) and drive to house at 9608 Oakland Drive to the right (west).



Figure 3: Looking north from within the site. Wetlands exist beyond tall tree line in background. Proposed development planned in low-growth area.



Figure 4: Looking east into the site from Oakland Drive. Clearing beyond trees in foreground is within subject site. Proposed development planned beyond treeline.



Figure 5: Looking east into the site from edge of wetlands. Site clearing created opening beyond trees in foreground. Structures in background are on Weathervane Self Storage and Romence Gardens along Shaver Road. Proposed development planned low-growth area.



Figure 6: Looking south into the site. Trees to the right are along Oakland Drive. Recent clearing show low-growing vegetation. Proposed development planned to the left of treeline.



Figure 7: Looking north into the site toward wetlands. Proposed development located south of wetlands.



Figure 8: Looking west at 9520 and 9526 Oakland Drive (single family dwellings) located across from subject site.



Figure 9: Looking west through Weathervane Self Storage toward subject site in background. Romence Gardens is located to the left (south).

**TO:** Planning Commission

**DATE:** May 16, 2024

**FROM:** Peter Dame, Chief Development Officer

**SUBJECT:** Draft Portage Forward Together 2045 Master Plan

## **I. INTRODUCTION:**

The City of Portage is updating its Master Plan for the first time since 2014. The Master Plan, also known as a Comprehensive Plan, is a policy document outlining a community's vision for the future. It provides the basis for and influences the community's future as a guide to quality of life, land use, economic development, zoning, and other regulatory ordinances.

## **II. BACKGROUND INFORMATION:**

The Michigan Planning Enabling Act 33 of 2008, requires local governments in Michigan to have and adopted Master Plan and to regularly assess and update it. The City of Portage Comprehensive Plan (Master Plan) was last updated in 2014, and in need of updating. The draft Portage Forward Together 2045 Master Plan (the draft Master Plan) includes an updated future land use plan, among other planning elements, all intended to respond to the current and future needs of the city.

### **BACKGROUND OF PLAN UPDATE**

On March 8, 2022, the City Council approved a contract with Houseal Lavigne, experts in community planning, urban design, and economic development, to guide the City of Portage through a Master Plan update process. The Master Plan update started in the fall of 2022, and community engagement has been a key piece of the Master Plan update process. With nearly 800 points of engagement across both in-person and online outreach forms, there have been ample opportunities to collect feedback. The draft Master Plan is developed based on current conditions, feedback received from the community, key stakeholders, and the planning commission through various working sessions, surveys, and open houses. The draft Master Plan is an expression of what the Portage community wants to become in the future, and it should be a dependable policy guide for decision-making and action.

The draft Master Plan is developed consistent with Michigan state standards, Michigan Economic Development Corporation (MDEC) Redevelopment Ready Communities (RRC) requirements, and best practices. It provides a comprehensive, data-driven analysis and user-friendly, graphic communication with maps and graphics, and this plan will be delivered virtually on an interactive project website which can be accessed via: <https://city-of-portage-mi-master-plan-hlplanning.hub.arcgis.com/>. Community and stakeholder engagement opportunities are available throughout the Master Plan update process. In addition to in-person community and stakeholder engagement opportunities, the project website also includes use of map.social, a web-based community issues mapping tool and online surveys.

The draft Master Plan includes the following elements:

1. About the Plan
2. Planning Context
3. Community Outreach
4. Visions and Goals
5. Land Use and Development
6. Housing and Neighborhoods
7. Economic Development
8. Transportation and Mobility
9. City Parks, Open Space, and the Environment
10. Livability and Sustainability
11. Subareas
12. Implementation

### **III. ANALYSIS:**

#### Draft Portage Forward Together 2045 Master Plan Review; Public Notification/ Commenting

Updating Master Plan is currently underway per the requirements of the Michigan Planning Enabling Act. The process will consist of upcoming public notification and public comment periods over the next several months. The planning process is being led by the Department of Community Development.

Per the Michigan Planning Enabling Act, the Planning Commission must formally take action at two key points:

1. After preparing a draft Master Plan, the Planning Commission votes to submit the draft Master Plan to the City Council requesting that the City Council approve the release of the draft Master Plan to the public and specific public agencies for a 63 day public comment period.
2. At the end of the 63 day public comment period, the Planning Commission will hold a hearing and subsequently vote to provide a formal approval of the draft Master Plan by resolution. Approval of the Master Plan by the Planning Commission at that point is the final step for adoption.

The draft master plan was presented for the Planning Commission's review at the May 2, 2024, meeting. When the Planning Commission has determined the draft Master Plan is at a stage that is ready to start the 63-day public comment period, it should vote to send the draft to the Council with the recommendation that the City Council approve it for distribution. Technical updates/corrections, if needed, can be made prior to the Plan distribution. The 63-day public comment period does not start until the City Council grants its approval to distribute the draft Master Plan.

After the City Council approves the distribution of the draft Master Plan with the Letter of Intent to Municipal Agencies, a 63-day commenting period begins. Within this period, the City will hold a community open house (Master Plan Public Meeting) scheduled on June 24, 2024, to help familiarize the public with the contents of the draft Plan and to further collect public comments. Once the comment period

closes, the Planning Commission will have a public hearing for the Master Plan approval (step 2). Adjustments to the draft plan can continue to be made to the draft Plan as a result of the comments heard during the 63-day public comment period and after the Planning Commission's formal hearing at the end of the 63-day comment period.

Draft Portage Forward Together 2045 Master Plan - beyond 2014 Comprehensive Plan

The draft Master Plan is attempting to meet the following objectives, based on the earlier stages of input into the plan, in addition to being consistent with Michigan state standards and MEDC RRC requirements:

1. Development of a collective vision statement utilizing public and stakeholder input, reaching a broad and representative cross-section of Portage residents and stakeholders through accessible and creative engagement strategies.
2. Consolidation and reclassification of the future land use categories to reflect the community's vision and create more housing opportunities for people of all life stages having the ability to live in Portage.
3. Addition of a new subarea plan to assess and plan for the future of Crossroads Mall area.
4. Refinement and update of the City Centre subarea plan, encapsulating the City's recent investments in the area, potential for expanding residential and commercial opportunities, and improving connectivity and accessibility.
5. Incorporation of the key findings of the adopted Lake Center Corridor & Placemaking Plan into the Lake Center subarea section of the plan. [The Lake Center](#) subarea objectives include creating a more people-oriented corridor design to help spur investment, increase vitality, and create an attractive and safe place.
6. Synthesis of recent citywide studies and data into Master Plan updates policies and implementation strategies to reflect current and future needs of the City.
7. Expanded emphasis on improving housing strategies by increasing the variety of housing types and attainable housing supply, and land uses and policies to support this objective.
8. Coordination of the City's Parks and Open Space Plan update, by incorporating that update into the draft Master Plan.
9. Establishment of more inclusive plan elements (e.g., a Mobility chapter in lieu of a Transportation chapter) to include and address a multi-modal environment and new means of travel, accessibility issues, right-of-way rebalancing, consideration of work from home impacts.
10. The existing Community and Quality of Life chapter and sub-elements were broken out into new sections and strategies for City Parks, Open Space, and the Environment; and Livability and Sustainability elements.
11. Ensuring the Plan update and related elements, maps, and deliverables are expressed in an accessible, interactive digital format consistent with the City's ESRI toolkit, so that it can be placed on the City's website and utilized with its GIS system.
12. Identification of key issues and policies that warrant review for a forthcoming Zoning Ordinance. Master Plan update will set clear expectations for public engagement and identify necessary or proposed revisions for consideration towards the City's Zoning Ordinance Rewrite that will follow in the upcoming fiscal years.

13. The implementation element will be an actively utilized mechanism to identify performance metrics, tracking mechanisms, integration with the internal City process (CIP, Development Review) and associated City Departments charged with implementation.
14. The draft Master Plan will assist the City in achieving Redevelopment Ready Community certification.

#### **IV. RECOMMENDATION:**

After reviewing the draft Master Plan, vote to initiate the required public notification steps for the draft Portage Forward Together 2045 Master Plan by recommending that the City Council approve the distribution of the draft Portage Forward Together 2045 Master Plan, with technical updates/corrections if needed prior to the distribution.

**Attachments:** None